

**CITY OF WINTER PARK
PLANNING AND ZONING BOARD**

**Staff Report
November 4, 2014**

REQUEST OF UNICORP NATIONAL DEVELOPMENT INC. FOR: AN ORDINANCE AMENDING THE "COMPREHENSIVE PLAN" AND THE FUTURE LAND USE MAP SO AS CHANGE THE FUTURE LAND USE MAP DESIGNATION OF COMMERCIAL TO MEDIUM DENSITY PLANNED DEVELOPMENT (PD-1) ON THE PROPERTY AT 110 S. ORLANDO AVENUE.

REQUEST OF FOR UNICORP NATIONAL DEVELOPMENT INC.: AN ORDINANCE AMENDING THE OFFICIAL ZONING MAP SO AS CHANGE THE EXISTING ZONING DESIGNATION OF COMMERCIAL (C-3) DISTRICT TO PLANNED DEVELOPMENT -1 (PD-1) DISTRICT ON THE PROPERTY AT 110 S. ORLANDO AVENUE.

REQUEST OF UNICORP NATIONAL DEVELOPMENT INC. FOR: CONDITIONAL USE APPROVAL TO REDEVELOP THE PROPERTY LOCATED AT 110 S. ORLANDO AVENUE INTO 31,960 SQUARE FEET OF RETAIL/RESTAURANT GROUND FLOOR DEVELOPMENT WITH 53 MULTI-FAMILY UNITS LOCATED ON THE SECOND THROUGH FOURTH STORIES OF THE PROJECT FOR A TOTAL MAXIMUM SQUARE FOOTAGE OF 232,570 FEET .

This public hearing is at the request of Unicorp National Development Inc. for the redevelopment of the Mt. Vernon Inn site located at 110 S. Orlando Avenue. The applicant is requesting to redevelop site with a mixed use project including commercial and multi-family residential development. The project highlights include:

Project Site:	3.58 acres
Existing Future Land Use Category:	Commercial
Existing Zoning District:	C-3
Proposed Future Land Use Category:	Medium Density Planned Development (PD-1) on 3 acres/Commercial on .58 acre
Proposed Zoning District:	PD-1 Zoning on 3 acres/C-3 zoning on .58 acre

The proposed development consists of 31,980 square feet of retail/restaurant ground floor development with 53 multi-family units located on the 2nd through fourth stories of the project. Based on the requirements that Planned Developments can be no larger than 3 acres in size, the developer is blending both the PD-1 zoning and the C-3 zoning districts together to set the minimum and maximum thresholds of the project. This project is also the first PD requested within the city.

The overall site area is 155, 945 square feet. Since this project combines two zoning districts, PD1 and C-3, the developer has blended the requirements of both districts to

obtain the development profile of the site. The blended profile allows up to 155% floor area ratio (FAR) with a maximum lot coverage of 46.69%. The open space/pervious surface coverage is 23.43%. This means that at least 23.43% of the site must be open space and the buildings, including the parking garage, must not be more than 155% of the total lot coverage. The developer is proposing a FAR of 149.74%. The developer is proposing 408 parking spaces which includes 40 spaces required by the City Commission for parking at the Lakeside development. Access to the project will include a driveway on Orlando Avenue and two driveways on Harper Avenue, all of them accessing the parking garage and surface parking lot.

The developer is asking for two variances from the rear setback requirements of 20' for the first and second stories and 25' for the third and fourth stories. The developer is requesting a 10' variance on the first and second stories facing Harper Street, making the setback 10' instead of the required 20'. He is also requesting a 5' variance for the 3rd and 4th stories facing Harper Street.

For purposes of comparison, the following table outlines the C-3 zoning requirements, the PD-1 zoning requirements and the blended requirements of this project.

	C-3 Requirements	PD-1 Requirements	Blended PD-1/ C-3 Requirements	Project Proposal
Property Size		Limited to 3 acres		3 acres PD-1/.58 acres C-3
Floor Area Ratio	Max. 45%	Max. 175%	Max. 155%	149.74%
Lot Coverage	Max. 45%	Max 47%	46.69%	46.34%
Min. Open Space	Min. 15%	Min. 25%	Min. 23.43%	23.70%
Primary 4 lane road/1 story		20 feet		30 feet
Primary 4 lane road/3+ story		30 ft		44.5 ft
Secondary 2 lane road/1 story		20 ft		10 ft
Secondary 2 lane road/3+ story		25 ft		20 ft
Landscape Protection Zone		Along primarily four lane roads 7 feet		7 ft
Density		17 units/acre	17 units/acre	14.89 units/acre (53 units)
Bldg. Height	55' (4 story max)	55' (4 story max)	55' (4 story max)	55' (4 story max)

Additional criteria is found in the Code Compliance matrix provided by the applicant as part of their site development package.

Staff Analysis of the Applicant's Requests:

Representatives from Planning & Community Development, Public Works, Electric Utility, Water and Wastewater Utilities, Fire, Urban Forestry, Parks and Recreation and City Administration Departments met on two occasions, once with the developer and comprised a list of conditions that the developer must meet to be consistent with both city policies and expectations of a project of this size and scale.

As required by the Land Development Code, the developer provided a traffic study to determine the impacts of this project on the intersections at Morse Blvd and Orlando Avenue and Fairbanks Avenue and Orlando Avenue as well as traffic along Orlando Avenue from Webster Avenue to Minnesota Avenue, Morse Boulevard from Orlando Avenue to Pennsylvania Avenue and Fairbanks Avenue from Clay Street to Pennsylvania Avenue. The city staff asked Christopher Simoneaux with CES Engineering to review the traffic study and make comments about the findings. The city's traffic consultant had concerns about the intersection of Morse Boulevard and Orlando Avenue and suggested that the developer provide new signals and do a complete signal warrant study to include restricted lefts in each direction. The consultant also recommends that the driveway along Orlando Avenue be restricted to a right in/right out only. The developer may also want to reach out to the Italo's restaurant and consider some type of cross-access to allow better flow from each site.

City staff also asked John Cunningham with ACI, under the city's continuing services contract for architectural services, to review the architectural aspects of this project since it is a gateway into downtown Winter Park and the first Planned Development. Mr. Cunningham's report is attached. In summary, he feels that the architectural detail proposed is relatively to scale with other projects in Winter Park and represents Winter Park's architectural style. He advises that the developer pay attention to details such as landscaping, trellises, windows and roof tiles.

Staff has analyzed the impact of an estimated additional population of 117 people based on 2.2 people/unit x 53 units. The developer has set aside approximately ¼ acre of land as part of the required open space for the PD-1 designation as a park on the southeast side of the project and pay the parks and recreation impact fees to mitigate any additional impact. While not a city park, the park space, which will be constructed and maintained by the developer will add to the overall appearance of the Martin Luther King Park by visually expanding it to the west side of Harper Street and provide on-site facilities for the residents as well as soften the impact of the surface parking along Harper Street. Staff has discussed with the developer imposing a public access easement so that this open space can be utilized by the general public.

Comprehensive Plan Determination:

There are a number of policies in the Comprehensive Plan that address the Planned Development Land Use designation. Those Policies are attached as an addendum to this staff report.

Summary and Conclusion:

After significant review, staff has analyzed the request for PD Future Land Use and PD-1 zoning. The applicant has provided a plan that meets the requirements of the land use and zoning as outlined in the Comprehensive Plan and Land Development Code subject to the following special conditions:

1. The Luxe Development entitlements comprise 31,980 square feet of commercial development which includes restaurants and retail space and 53 multi-family units. Exhibit "B" reflects the final approved site plan for the project.

2. The project is required to have a minimum of 404 parking spaces to meet the anticipated needs of the development plan, which includes a parking garage and surface parking lot.

3. The project may not change or modify the development plan as it affects parking spaces without approval of the Planning and Building Departments. Any development change over 10% of the total project shall be required to go through a public hearing process including the Planning & Zoning Board and City Commission.

4. The applicant will keep the landscaped area on the southeast side of the project as a bucolic enclave, to be constructed and maintained by the developer as a park site. The developer will provide the city with a public access easement for this area prior to the issuance of the first residential building permit. The developer will mitigate the additional parks and recreation impacts by payment of the parks and recreation impact fees.

5. The entrance to the project along Orlando Avenue will be restricted to right in/right out only.

6. The applicant will take care to ensure that architectural features of the project stay well proportioned, particularly columns and arbors fronting along Orlando Avenue.

7. The developer commits to installing and maintaining landscaping, both on street and in containers along the frontage of Orlando Avenue, Morse Boulevard and Harper Street to soften the effect of fast moving traffic.

8. The setback requirements along Harper Avenue are waived from 20 feet for the first and second stories to 10 feet and from 25 feet for the third and fourth stories to 20 feet.

9. The developer commits to undergrounding the power lines along Orlando Avenue for the entire length of this property.

10. The developer will coordinate and install intersection improvement for the full Morse Boulevard/Orlando Avenue intersection to include restricted left turn signals in all directions subject to FDOT and city approval.

11. The developer will pay and install the smart signal technology improvements at the Morse Boulevard/Orlando Avenue intersection.

12. The developer will contribute \$100,000 towards decorative mast arms along Orlando Avenue and Morse Boulevard to beautify the intersection.

13. The developer will install and maintain decorative paving along the entire Morse Boulevard/Orlando Avenue intersection for safer pedestrian crossing.

14. The developer will be responsible for the installation and maintenance of 10 feet sidewalks along the Orlando Avenue frontage of this property.

15. The developer commits to using 5" caliper street trees along Orlando Avenue for landscaping.

16. The developer will work with the city's Arborist regarding any tree replacement and maintenance along Orlando Avenue, Morse Avenue and Harper Avenue as it affects his property.

17. The developer will revise the site plan to show the required 8 feet landscaped setback along Harper Avenue.

18. The developer will provide bike parking per Land Development Code requirements on site.

19. The developer will coordinate with the City and the Lynx for a bus stop location on site.

20. The developer will be required to comply with all existing Life Safety and Building Codes adopted by the city at the time of submittal.

STAFF RECOMMENDATION IS FOR APPROVAL of the request for Medium Density Planned Development (PD-1) FLU and PD-1 Zoning for three acres of property located at 110 S. Orlando Avenue and APPROVAL of the Preliminary Conditional Use on 3.58 acres of property located at 110 E. Orlando Avenue for a four story, 53 units, 31,980 square feet retail/restaurant site with a parking garage with 333 spaces and surface parking of 75 spaces, as presented.

COMPREHENSIVE PLAN POLICIES RELATED TO PD DEVELOPMENT

Policy 1-2.3.4: Medium Density Planned Development (PD 1). This land use designation includes retail businesses, restaurants, offices and residential uses within building projects permitting higher density mixed and single use. This land use designation is restricted to geographic areas of the City identified in the Future Land Use Map Series – Map 1-6a, and 1-6b.

The maximum floor area ratio shall not exceed the percentages listed in the Maximum Future Land Use Density/ Intensity Table and as governed by the maximum number of stories permitted in the Maximum Height Map within this Future Land Use Element.

Policy 1-2.3.6: Limitation on the Use of Planned Development Future Land Use. This element includes maps within the Map Series (1-6a-1-6d) which indicate the only geographic areas within the City where properties may be granted Medium Density Planned Development and High Density Planned Development Future Land Use designations and corresponding zoning. Such action shall require an extraordinary vote (four votes) for approval by the City Commission. This element prohibits the use of the Planned Development designations, in areas of the City not identified on the Planned Development Candidate map series (1-6a – 1-6d).

Policy 1-3.2.1: Enhance the Ambiance and Quality of Winter Park's Wide Ranging

Business Climate. The City of Winter Park shall maintain the charm of the City's Central Business District's "village" ambiance comprised of compact offices and one of the premier shopping districts in Florida by preserving the eclectic mix of architectural styles that allows offices, unique retail shops and upstairs residents to coexist in a charming manner that is emulated by cities everywhere. The vitality of Winter Park's existing premiere retail shopping districts, with small unique shops, is essential to the continued success of our historic downtown area. To ensure economic sustainability of our premier shopping districts, the City shall encourage centralization of architecturally compatible, quality retail establishments into existing retail areas. In order to maintain the city's village character, in any new planned development project, single tenant retail developments over 65,000 square feet are not permitted.

Policy 1-3.2.5: Redevelopment of Areas Designated Commercial and Office/Professional. The City shall encourage the redevelopment of areas designated Commercial and Office/Professional at a floor area ratio up to the maximum permitted by those future land use designations to sustain the City's employment base and fiscal condition. At grade or elevated parking areas shall be included in floor area ratio calculations. However, the City shall ensure that such redevelopment contributes to the aesthetic character of the City by replacing or refurbishing older buildings to comply with the City's design standards which shall address the quality of architectural design to achieve more compatible relationships in the design of buildings, avoid unsightly appearance, and avoid structural incompatibilities. In evaluating development proposals the City shall ensure that compatible land use relationships occur, particularly between land use within perimeter areas of land designated commercial and

office and less intense development located within five-hundred (500) feet outside such areas. Regulatory measures shall ensure that issues pertaining to land use density and intensity as well as building height, mass, articulation, and fenestration reinforce a smooth and compatible land use transition. In addition, the City shall ensure that redevelopment proposals within Commercial and Office/Professional designated areas do not adversely impact the City's traffic flow and level-of-service for public facilities and comply with the City's landscaping, storm water management, and sign regulations.

Policy 1-3.2.6: Planned Development Land Use. The City recognizes that Planned

Developments, on specific properties identified as Planned Development Candidates (Map 6a-6d), may be appropriate if restricted to height, intensity and density compatible with the surrounding Commercial areas. The purpose of this Land Use Designation is to create zoning that will:

- a. Increase the City's tax revenues by providing Class A Office, Commercial or Residential that satisfies demonstrated space needs within the City. The City encourages development of Class A Office.
- b. Enhance the City's gateways by providing incentive for redevelopment.
- c. Substantially increase landscaping, trees and setbacks obtainable from existing Commercial/Office designations by providing not less than 25% natural pervious green space.
- d. Create inviting pedestrian oriented public spaces.
- e. Provide increased flexibility to the City in the consideration of unique development opportunities.

The City shall develop standards for the new Planned Development zoning code districts within the restraints of the maximum density, intensity of use and height limits specified in this Comprehensive Plan for the Planned Development Future Land Use Designations. The total building lot coverage allowed for the sum of all buildings within a Planned Development is limited to 47% in Medium Density Planned Developments and 55% in High Density Planned Developments. Single tenant retail in excess of 65,000 square feet is prohibited. The objective of these development codes is to provide meaningful guidance for the introduction of limited increased density without adversely impacting the existing pedestrian scale and low-density attributes of our historic core or nearby residential areas. Identification as Candidates for Planned Development does not create an entitlement to be approved for this Land Use Designation. All redesignations are subject to Public Hearings and Commission approval. This Planned Development Candidate designation is a test program and will be reassessed after 5 years. It is the prerogative of the City to reject a candidate or to remove a property from the list of properties to be considered, if changes in conditions warrant.

Policy 1-2.3.7: Creation of Two Planned Development Districts. Within one year after adoption of this Comprehensive Plan, the City Commission shall adopt two new Planned Development Zoning Districts (PD1 and PD2). Prior to adoption, the Planning and Zoning Commission shall evaluate Planned

Development Zoning Districts and provide draft detailed regulations to regulate the new zoning districts. Regulations shall be proposed which address at a minimum:

- a. At least two levels of density/intensity for Planned Developments based on proximity to the historic core of Winter Park.
- b. Retail, office, and residential use standards
- c. Open space and permeable space requirements
- d. Appropriate height, scale, mass, setbacks, lot coverage and density restrictions
- e. Long term maintenance of such facilities
- f. Parking
- g. Residential protection from noise, exhaust and lighting
- h. Landscaping
- i. Public spaces (courtyards/gathering places)
- j. Transportation connections to bus transit and bike trails
- k. Egress/ingress requirements including on-site stacking
- l. Consideration of inclusionary affordable housing
- m. Sidewalk and streetscape enhancement
- n. Proximity to residential zoning protections
- o. Restrictions on use of Planned Development zoning for single tenant use
- p. Minimum distance between Planned Developments
- q. Transportation issues
- r. Waste management and emergency vehicle access.
- s. Tools for limiting mass of Planned Development Individual buildings to ensure compatibility with the historic Village Character of Winter Park. At a minimum, consideration will be given to the following tools: Separation of large buildings into separate buildings, restricting wall heights at side yard setbacks, reducing heights along sensitive edges, establishing maximum wall plane lengths that reflect the traditional width of buildings along the street, and alignment of front setbacks.

The following minimum standards are hereby adopted to provide meaningful guidance to the future development of Land Use Development regulations governing Planned Development districts:

- a. Minimum property size eligible for PD zoning is 80,000 sq ft
- b. Maximum project size eligible for PD zoning is 3 acres. (Designated candidates in excess of 3 acres shall be eligible for approval as adjacent PD or adjacent Commercial or Office designations.)
- c. Not less than seventy-five feet of separation between any two principal PD buildings on sites larger than three acres
- d. Not less than 25% natural pervious green space
- e. Preference for Class A Office
- f. Prohibition of single tenant retail in excess of 65,000 sq ft
- g. Minimum setbacks to 20-30 feet from principal arterial road
- h. Landscaping standards superior to Commercial/Office code requirements
- i. Public spaces (including public courtyards) and 5-10 ft wide sidewalks
- j. Parking garages minimally visible from street or architecturally compatible with principal buildings

Policy 1-3.2.8: Future Land Use Amendments to Planned Development. The City may approve, at its discretion, amendments or changes to the planned development future land use designation when convinced that the location is compatible to be granted additional density and intensity given the character of surrounding properties. In granting or approving the planned development designation, the City may limit the intensity below the maximums permitted by this element for any reason determined advisable. The adoption of planned development intensity shall only occur in locations where redevelopment is to be encouraged, where the scale and character of the resultant project will be compatible with the commercial/ office location where the parcel size is sufficient and where there are minimal impacts on nearby residential properties.

OCPA Web Map

Florida Turnpike	Major Roads	Proposed Road	Block Line	Commercial/Institutional	Hydro	Golf Course
Interstate 4	Public Roads	Brick Road	Lot Line	Governmental/Institutional/Misc	Waste Land	Lakes and Rivers
Toll Road	Galad Roads	Rail Road	Residential	Commercial/Industrial	County Boundary	Building
Road Under Construction	Proposed SunRail	Agriculture	Agricultural Curtilage	Parke	Hospital	

Courtesy Rick Singh, CFA, Orange County Property Appraiser



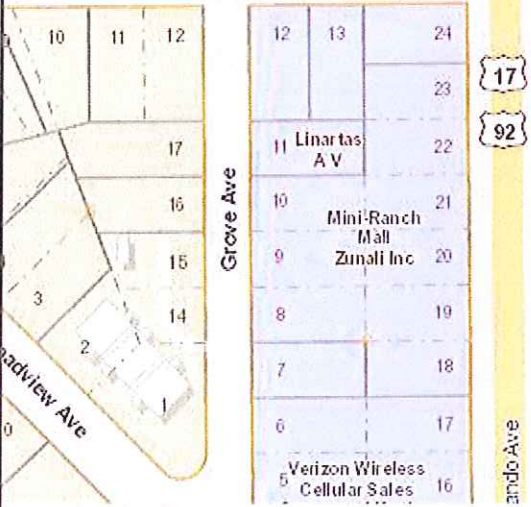
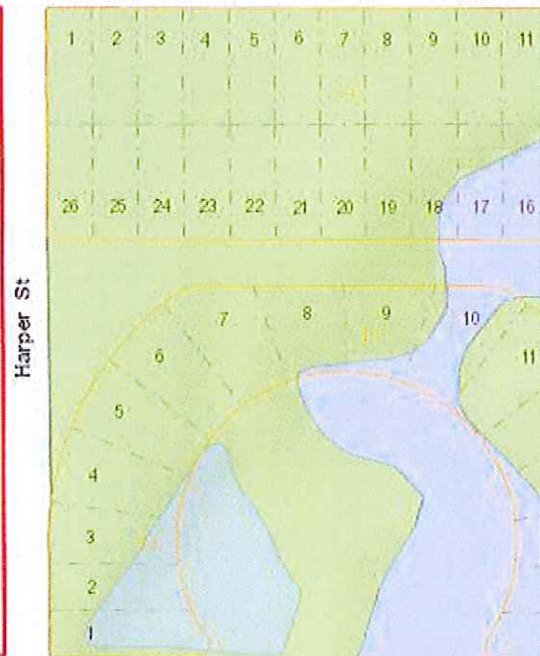
12	20
13 Carmel Cafe' and Wine Bar	30
14 Pjs Carmel Land Holdings II LLC	31
15	Sesco Lighting
16	1133 W Morse Blvd LLC
17	32 33 34

4	8
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	8 9 10

W Morse Blvd

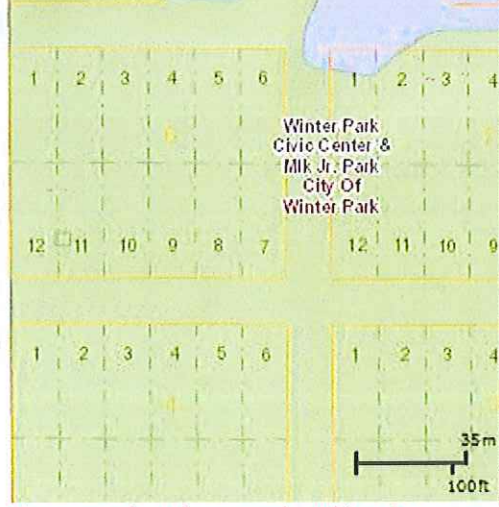
Daily Traffic
7000

14	15
13	16
12	17
11	18
10 Best Western Mt Vernon Inn	19
9 Motorlodge Of Winter Park Inc	20
8 [147 Rooms]	21
7	22
6	23
5	24
4 276 S Orlando Avenue LLC	25
3	26
2	27
1	28



W New England Ave

11	New England	12
10	Wharf Shopping	13
9	New England Courtyard LLC	14
8		15
7		16
6		17
5	Copytronics Sss Investments Of Jacksonville Inc	18
4		19
3	Burger King	20
2	Burger King Corp	21



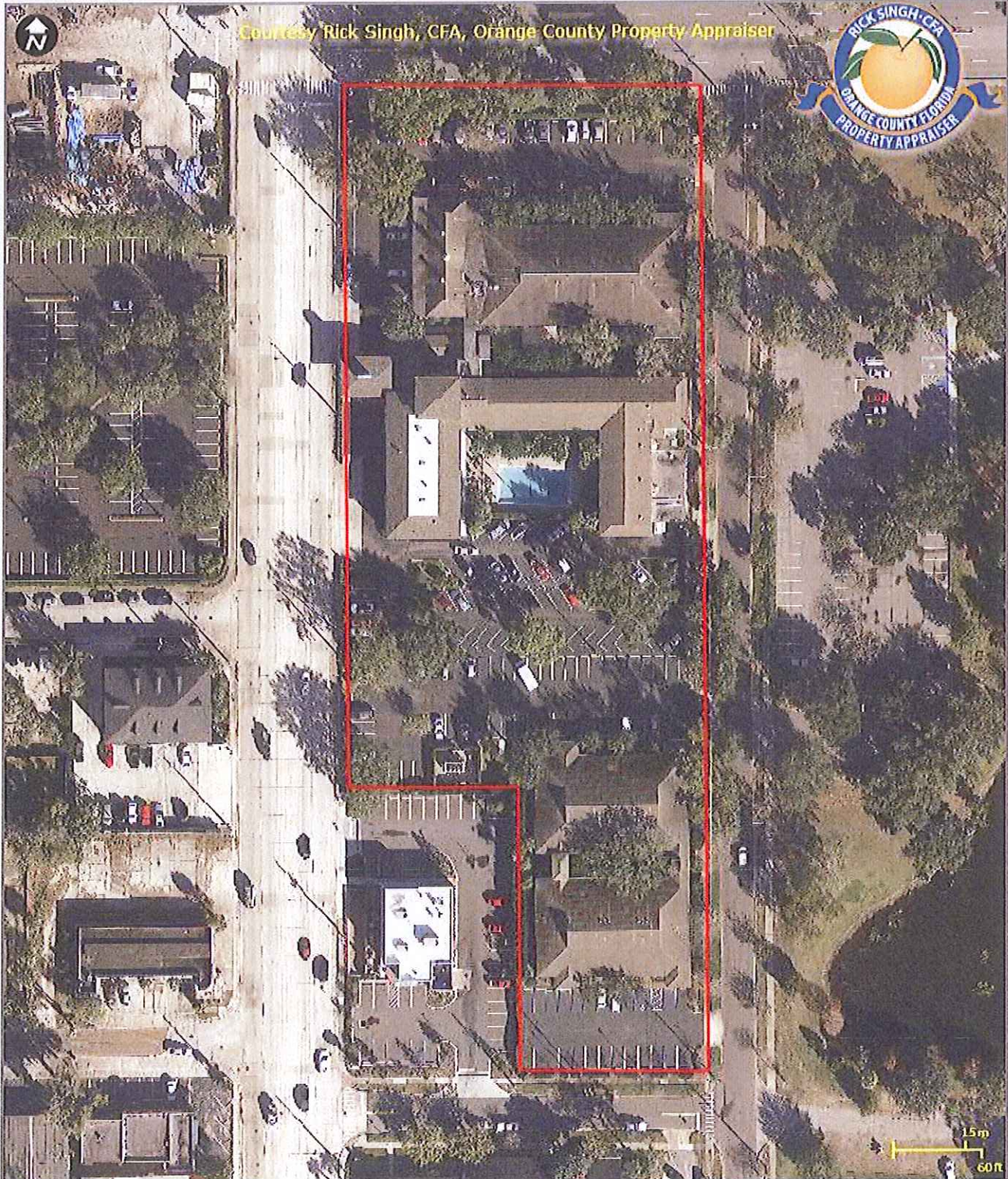
Created: 10/28/2014

This map is for reference only and is not a survey.

OCPA Web Map

	Major Roads		Proposed Road		Residential		Commercial/Industrial		Parks	6	Lot Number	
	Florida Turnpike		Public Road		Brick Road		Agriculture		Vacant Land		06060	Parcel Number
	Interstate 4		Local Roads		Block Line		Commercial/Institutional		Hydro		3106	Parcel Address
	Toll Road		Road Under Construction		Lot Line		Governmental/Institutional/Misc.		Waste Land	E	111.9	Parcel Dimension

Courtesy Rick Singh, CFA, Orange County Property Appraiser



Created: 10/28/2014

This map is for reference only and is not a survey.

Prepared by and return to:

JOINDER AND CONSENT

The undersigned, _____, in his capacity as the _____ of _____, hereby certifies that it is the holder of the hereafter described mortgage lien, encumbering the following described real property located in _____ County, Florida, to wit:

[INSERT LEGAL]

FOR VALUE RECEIVED, the undersigned hereby joins in, consents to the Development Order to be recorded on the lands described above by the owner thereof and agrees that its mortgage, lien, or other encumbrance, which is recorded in Official Records Book _____, Page _____, of the Public Records of _____ County, Florida, shall be subordinated to the Development Order as recorded in O.R. Book _____, Page _____, Public Records of _____ County, Florida.

IN WITNESS WHEREOF, the undersigned has caused these presents to be executed by its undersigned officer this _____ day of _____, 2014.

Signed, sealed and delivered
in the presence of:

[MORTGAGEE]

Print name: _____

By: _____
Print Name: _____
Title: _____

Print name: _____

STATE OF FLORIDA
COUNTY OF ORANGE

The foregoing instrument was acknowledged before me this _____ day of _____, 2012, by _____ the _____ of _____, (check one) who is personally known to me or who produced _____ as identification.

Notary Public
Print Name: _____
My Commission expires: _____

Prepared by:
Dori Stone, Director of Planning
City of Winter Park
401 Park Avenue South
Winter Park, FL 32789

Return to:
City Clerk
City of Winter Park
401 Park Avenue South
Winter Park, FL 32789

DEVELOPMENT ORDER FOR THE LUXE PD

THIS DEVELOPMENT ORDER (the "Order") is made and entered into this ____ day of _____, 2014, by and between the **City of Winter Park, Florida**, a political subdivision of the State of Florida (the "City"), 401 Park Avenue South, Winter Park, Florida 32789 and **Charles Whittall**, (referred to as "Developer" and/or "Owner") 7940 Via Dellagio Way, Suite 200 Orlando, FL 32819.

WITNESSETH:

WHEREAS, the Owner owns fee simple title to the property generally located at Morse Boulevard and Orlando Avenue in Winter Park, Florida, and described in attached **Exhibit A**, and the development to be built on the property is to be known as "The Luxe" or "Project"; and

WHEREAS, the City designated 3 acres of the Property described in **Exhibit B**, (the "Property"), with a Future Land Use Designation and Zoning of Planned Development (PD-1) in accordance with Sec. 58-82; and

WHEREAS, the intent of PD-1 is to increase the City's tax revenues by providing commercial, office or residential development that satisfies demonstrated space needs within the city, enhance the city's gateways by providing incentives for redevelopment, substantially increase landscaping, trees and setbacks, create inviting pedestrian oriented public spaces and provide increased flexibility to the City in the consideration of unique development opportunities; and

WHEREAS, other objectives of PD-1 are to provide meaningful guidance for the introduction of limited increased density and intensity without adversely impacting the existing pedestrian scale and low density attributes of the City's historic core or nearby residential areas; and

WHEREAS, the City Commission finds that the PD-1 future land use and zoning designation for the Property meets the intent of the City's Comprehensive Plan and the requirements of the City's Land Development Code; and

WHEREAS, the 3 acres in Exhibit B are designated with a PD-1 Future Land Use classification, and PD-1 zoning, and the remaining portion of the Project has a Commercial land use designation and C-3 zoning district; and

WHEREAS, this Order governs the 3 acres described in Exhibit B and referred to as the Property, and the balance of the Project zoned C-3; and

WHEREAS, the City finds that this Order is consistent with the City Comprehensive Plan, the City of Winter Park Zoning Code, and is in the best interests of the citizens of Winter Park.

NOW, THEREFORE, for and in consideration of the terms and conditions of this Development Order, the mutual covenants set forth herein, and for other good and valuable consideration, the City and Developer agree to the following conditions:

Section 1. Recitals. The above recitals are true and correct and form a material part of this Development Order.

Section 2. Development of the Property.

A. The Luxe Development entitlements comprise 31,980 square feet of commercial development, which includes restaurants and retail space and 53 multi-family units. The Owner hereby agrees to develop the Project in accordance with that certain development plan for The Luxe prepared by Antunovich Associates, Inc., date stamped as received by the City on October 26, 2014 (hereinafter referred to as the "Development Plan"), attached as Exhibit C. The Development Plan is hereby incorporated herein by reference as if fully set forth herein.

B. Conditions of Approval. The Owner hereby agrees that the Property shall be developed in accordance with and is made subject to the following Conditions of Approval (the "Conditions of Approval"). The Owner further agrees to comply with all of the terms and provisions of the Conditions of Approval, which are also set forth in the Development Plan.

1. The Project is required to have a minimum of 404 parking spaces to meet the anticipated needs of the development. The Developer may not change or modify the parking configuration in the Development Plan in any way without the express consent of the Planning and Building Departments.

2. The Developer must keep the landscaped area on the southeast side of the Project as a bucolic enclave, to be constructed and maintained by the Developer as a park site. The developer will provide the city with a public access easement to allow the public access to this area prior to the issuance of the first residential building permit.

3. The entrance to the Project along Orlando Avenue will be restricted to right in/right out only.

4. The Developer will take care to ensure that architectural features of the Project stay well proportioned, particularly columns and arbors fronting along Orlando Avenue.

5. The Developer commits to installing and maintaining landscaping containers along the frontage of Orlando Avenue, Harper Street and Morse Boulevard to soften the effect of fast moving traffic.

6. The Developer commits to undergrounding the power lines along Orlando Avenue for the entire length of this property.

7. The Developer will coordinate and install intersection improvement for the full Morse Boulevard/Orlando Avenue intersection to include restricted left turn signals in all directions subject to FDOT and city approval.

8. The Developer will pay and install the smart signal technology improvements at the Morse Boulevard/Orlando Avenue intersection.

9. The Developer will contribute \$100,000 towards decorative mast arms along Orlando Avenue and Morse Boulevard to beautify the intersection.

10. The Developer will install and maintain decorative paving along Orlando Avenue for safer pedestrian crossing.

11. The Developer will be responsible for the installation and maintenance of 10 feet sidewalks along the Orlando Avenue frontage of the Project.

12. The Developer commits to using 5" caliper street trees along Orlando Avenue for landscaping.

13. The Developer will work with the City's Arborist regarding any tree replacement and maintenance along Orlando Avenue, Morse Avenue and Harper Avenue as it affects the Property.

14. The Developer will revise the Development Plan to show the required 8 feet landscaped setback along Harper Avenue.

15. The Developer will provide bike parking per Land Development Code requirements on site.

16. The Developer will coordinate with the City and the Lynx for a bus stop location on site.

17. The Developer will be required to comply with all existing Life Safety and Building Codes adopted by the city at the time of submittal.

C. Except as otherwise expressly set forth in this Order and the Development Plan, it is agreed that (1) the Owner shall comply with the zoning and subdivision regulation of the City as set forth in the Winter Park Land Development Code, as it may from time to time be amended, the (2) all preliminary subdivision plans, final subdivision plans, and final site plans for the Property or any portion thereof shall conform to the Winter Park Land Development Code

requirements in effect at the time of approval of any such plans. In the event of any conflict between the provisions of the Winter Park Land Development Code, as it may from time to time be amended, and this Order, it is agreed that the provisions of this Order shall control.

Section 3. Waivers from the Winter Park Land Development Code. Pursuant to City of Winter Park Land Development Code Section 58-82, Developer has been granted waivers from the requirements of the Winter Park Land Development Code, said waivers being as follows:

1. The setback requirements along Harper Avenue are waived from 20 feet for the first and second stories to 10 feet, and from 25 feet for the third and fourth stories to 20 feet.

Section 4. Notices. Any notice delivered with respect to this Order shall be in writing and be deemed to be delivered (whether or not actually received) (i) when hand delivered to the other party, (ii) upon receipt when deposited in the United States Mail, postage prepaid, certified mail, return receipt requested, or (iii) the next business day after being sent by nationally recognized overnight delivery service for the next business day delivery, all addressed to the party at the address appearing on the first page of this Order, or such other person or address as the party shall have specified by written notice to the other party delivered in accordance with this Section.

Section 5. Covenant Running with the Land. This Order shall run with the Property and inure to and be for the benefit of the parties hereto and their respective successors and assigns and any person, firm, corporation, or entity who may become the successor in interest to the Property or any portion thereof.

Section 6. Time of the Essence. Time is hereby declared of the essence as to the lawful performance of all duties and obligations set forth in this Order.

Section 7. Recording. An original of this Order shall be recorded in the Public Records of Orange County, Florida, no later than fourteen (14) days after the full execution hereof. Said recording shall serve as notice that the benefits, responsibilities and obligations of the parties arising from the terms and conditions of this Order shall inure to, and be binding upon, all successors in interest of the parties, of whatever nature. All recording costs relating to this Order shall be paid by the Developer and Owner.

Section 8. Applicable Laws and Venue. This Order shall be governed by and interpreted according to the laws of the State of Florida. The venue for any action brought in state courts shall be Orange County, Florida. The venue for any action brought in Federal Court shall be in the Middle District of Florida, Orlando Division.

Section 9. Order; Amendment. This Order constitutes the entire agreement between the parties, and supersedes all previous discussions, understandings and agreements, with respect to the subject matter hereof. Amendments to and waivers of the provisions of this Order shall be made by the parties only in writing by formal amendment which shall be recorded in the Public Records of Orange County, Florida.

Section 10. Further Documentation. The parties agree that at any time following a request therefor by the other party, each shall execute and deliver to the other party such further documents and instruments, in form and substance reasonably necessary to confirm and/or effectuate the obligations of either party hereunder.

Section 11. Specific Performance. The Owner and Developer acknowledge and agree that the City shall have the right to seek specific performance of the Developer's obligations. The parties agree that the obligations contained in Section 2 shall be binding and enforceable regardless of the completion or default of the other covenants or provisions in this Order and that the provisions of Section 2 shall be enforceable by specific performance together with and in addition to any other remedy provided by law. The Owner and Developer acknowledge receipt of separate and attendant consideration including but not limited to the receipt of ten (\$10.00), which Owner and Developer agree is adequate and sufficient.

Section 12. Subordination/Joinder. Unless otherwise agreed to by the City, all liens, mortgages and other encumbrances not satisfied or released of record, must be subordinated to the terms of this Order or the lienholder join in this Order. It shall be the responsibility of the Owner/Developer to promptly obtain the said subordination or joinder, in form and substance acceptable to the City Attorney, prior to the City's execution of the Order.

Section 13. Attorney's Fees. In the event that either party finds it necessary to commence an action against the other party to enforce any provision of this Order or because of a breach of the other party of any terms hereof, the prevailing party shall be entitled to recover from the other party its reasonable attorneys' fees, legal assistants' fees, and costs incurred in connection therewith, at both trial and appellate levels, including bankruptcy proceedings, without regard to whether any legal proceedings are commenced or whether or not such action is prosecuted to judgment.

Section 14. Captions. Captions and subsections of this Order are for convenience and reference only, and the words contained therein shall in no way be held to explain, modify, amplify or aid in the interpretation, construction, or meaning of the provisions of this Order.

Section 15. Severability. If any provision of this Order, or its application to any person, entity or circumstances is specifically held to be invalid or unenforceable by a Court of competent jurisdiction, the remainder of this Order and the application of the provisions hereof to other persons, entities or circumstances shall not be affected thereby and, to that end, this Order shall continue to be enforced to the greatest extent possible consistent with law and the public interest.

Section 16. Counterparts. This Order may be executed in any number of counterparts, each of which shall be deemed an original, but all of which, taken together, shall constitute one and the same document.

Section 17. Effective Date. The effective date of this Order shall be as of the date the last party signs this Order.

IN WITNESS WHEREOF, the parties hereto have caused these presents to be executed as of the day and year first above written.

Signed, sealed and delivered in the presence of:

CITY OF WINTER PARK, FLORIDA, a political subdivision of the State of Florida

Name: _____

By: _____
Kenneth W. Bradley, Mayor

Name: _____

ATTEST:
By: _____
Cynthia S. Bonham, City Clerk

Date: _____

STATE OF FLORIDA
COUNTY OF ORANGE

The foregoing instrument was acknowledged before me this ____ day of _____, 2014, by Kenneth W. Bradley, Mayor of THE CITY OF WINTER PARK, FLORIDA, a municipal corporation, on behalf of the corporation. He (She) is personally known to me or has produced _____ as identification.

(NOTARY SEAL)

Notary Public Signature

(Name typed, printed or stamped)

By: _____
Charles Whittall

Date: _____

Name: _____

Name: _____

STATE OF FLORIDA
COUNTY OF ORANGE

The foregoing instrument was acknowledged before me this _____ day of _____, 2014, by Charles Whittall, who is personally known to me or has produced _____ as identification.

(NOTARY SEAL)

Notary Public Signature

(Name typed, printed or stamped)

EXHIBIT "A"
[OWNER'S PROPERTY]

Property located in the County of Orange, State of Florida, described as:

Lots 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27 and 28,
LESS AND EXCEPT road right of way, Block 10, Lake Island Estates, according to the map or
plat thereof, as recorded in Plat Book M, Page 95, Public Records of Orange County, Florida.

EXHIBIT "B"
[3 ACRES – THE PROPERTY]

DESCRIPTION:

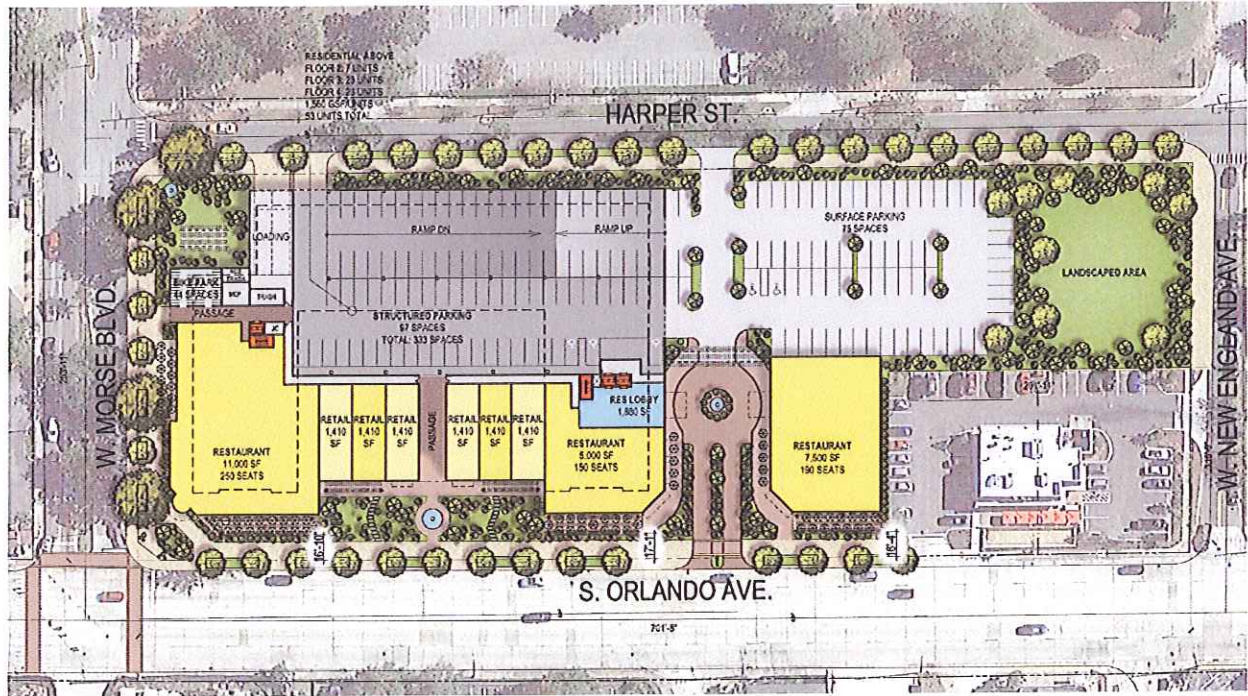
THAT PART OF LOTS 5 THROUGH 14, ALL OF LOTS 15 THROUGH 24, AND PART OF LOT 25, ALL IN BLOCK 10, OF THE PLAT OF "LAKE ISLAND ESTATES", SAID PLAT AS RECORDED IN PLAT BOOK M, PAGE 95 OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, AND BEING FURTHER DESCRIBED AS FOLLOWS:

COMMENCE AT THE SOUTHEAST CORNER OF LOT 28, OF SAID BLOCK 10 OF SAID PLAT OF LAKE ISLAND ESTATES, BEING AT THE INTERSECTION OF THE NORTHERLY RIGHT-OF-WAY LINE OF NEW ENGLAND AVENUE (50' WIDE RIGHT-OF-WAY), ALSO KNOWN AS FAIRVIEW AVENUE, AND THE WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET (50' WIDE RIGHT-OF-WAY); THENCE RUN $N00^{\circ}44'20''W$ ALONG SAID WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET FOR A DISTANCE OF 183.50 FEET TO THE POINT OF BEGINNING; THENCE, DEPARTING SAID WESTERLY RIGHT-OF-WAY LINE, RUN $S89^{\circ}39'14''W$ FOR A DISTANCE OF 135.00 FEET; THENCE RUN $N00^{\circ}44'20''W$ FOR A DISTANCE OF 20.42 FEET; THENCE RUN $S89^{\circ}49'40''W$ FOR A DISTANCE OF 122.98 FEET TO THE CURRENT EASTERLY RIGHT-OF-WAY LINE OF STATE ROAD NO. 15 & 600, U.S. HIGHWAY 17-92, ALSO KNOWN AS ORLANDO AVENUE, SAID EASTERLY RIGHT-OF-WAY LINE BEING DISTANT 40.00 FEET EASTERLY FROM THE CENTERLINE OF SURVEY OF FDOT RIGHT-OF-WAY MAP, SECTION 75030, STATE ROAD NO. 15/600, ORANGE COUNTY, SHEET 8 OF 20; THENCE RUN ALONG SAID EASTERLY RIGHT-OF-WAY LINE AND A NON-TANGENT CURVE CONCAVE EASTERLY HAVING A RADIUS OF 34,186.95 FEET, A CHORD BEARING OF $N00^{\circ}15'08''W$ AND A CHORD DISTANCE OF 485.19 FEET; THENCE RUN NORTHERLY, ALONG SAID EASTERLY RIGHT-OF-WAY LINE AND ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF $00^{\circ}48'47''$ FOR A DISTANCE OF 485.20 FEET TO A NON-TANGENT LINE; THENCE, DEPARTING SAID EASTERLY RIGHT-OF-WAY LINE, RUN $N38^{\circ}43'39''E$ ACROSS A CORNER CLIP FOR A DISTANCE OF 19.25 FEET TO THE SOUTHERLY RIGHT-OF-WAY LINE OF MORSE BOULEVARD (70' WIDE RIGHT-OF-WAY); THENCE RUN $N89^{\circ}54'00''E$ ALONG SAID SOUTHERLY RIGHT-OF-WAY LINE FOR A DISTANCE OF 236.64 FEET; THENCE, DEPARTING SAID SOUTHERLY RIGHT-OF-WAY LINE, RUN $S45^{\circ}59'02''E$ ACROSS A CORNER CLIP FOR A DISTANCE OF 7.12 FEET TO THE AFORESAID WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET; THENCE RUN $S00^{\circ}44'20''E$ ALONG SAID WESTERLY RIGHT-OF-WAY LINE FOR A DISTANCE OF 514.91 FEET TO THE POINT OF BEGINNING.

CONTAINING 130,844 SQUARE FEET (3.003 ACRES MORE OR LESS) AND BEING SUBJECT TO ANY RIGHTS-OF-WAY, RESTRICTIONS AND EASEMENTS OF RECORD.

EXHIBIT "C"

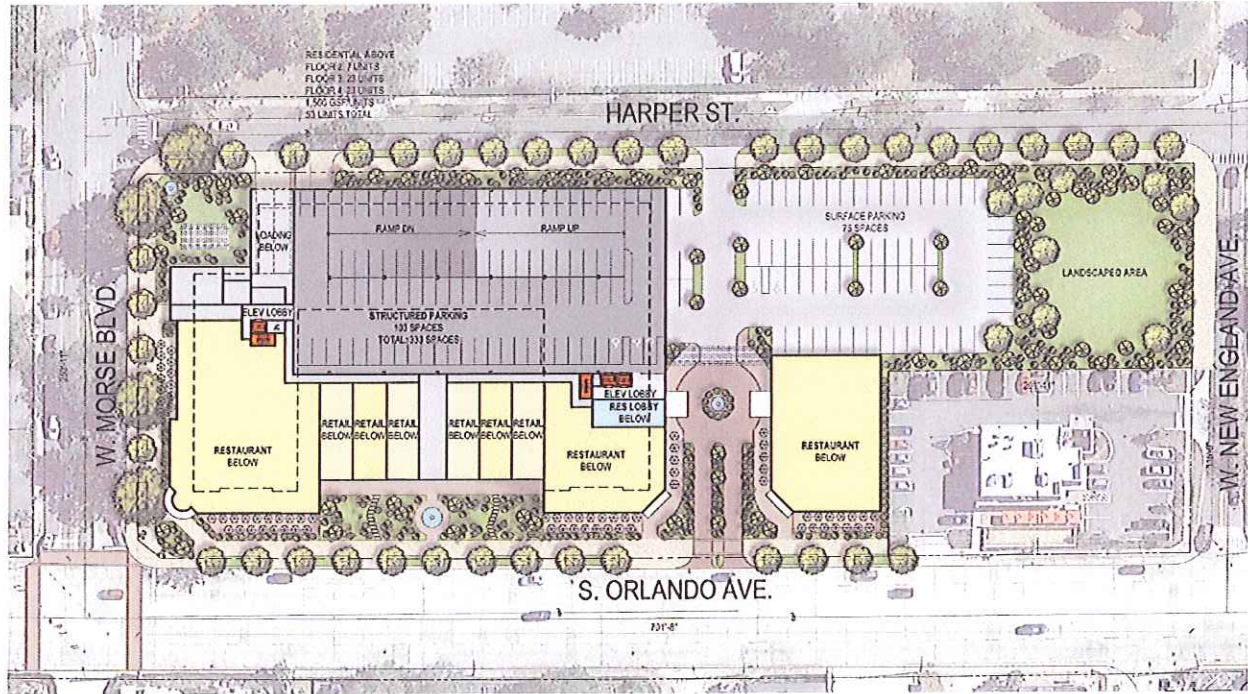
[DEVELOPMENT PLAN]



First Floor Plan

EXHIBIT "C"

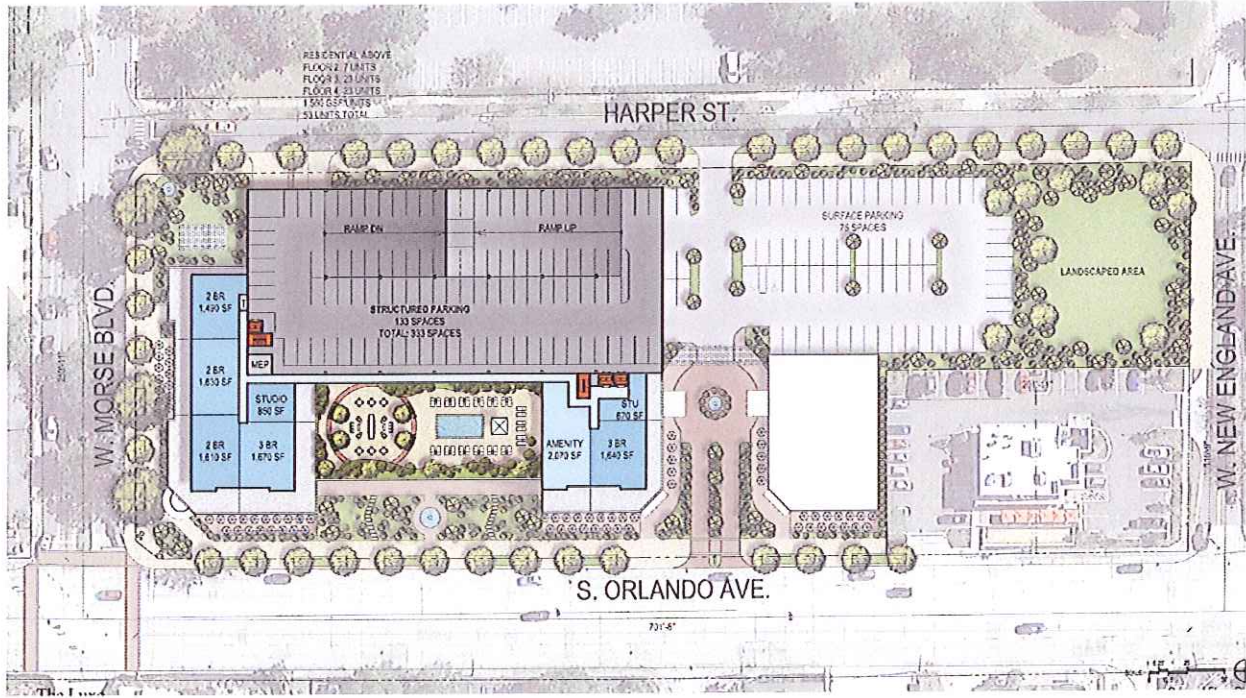
[DEVELOPMENT PLAN]



Mezzanine Floor Plan

EXHIBIT "C"

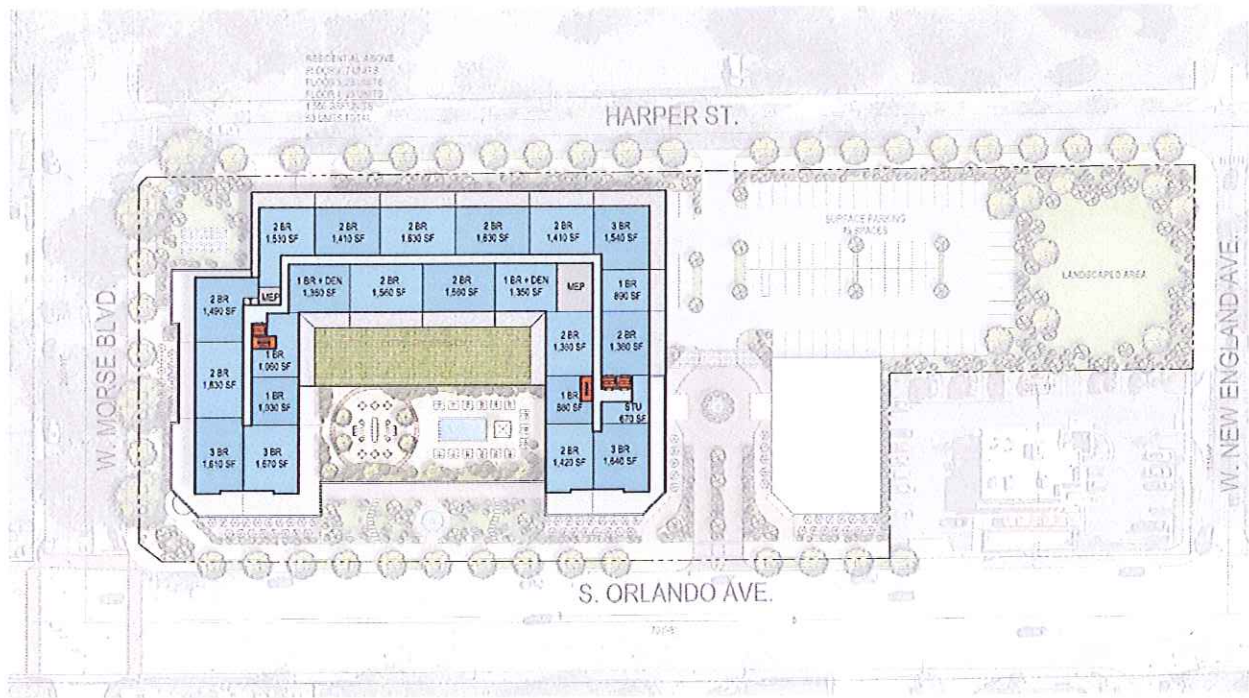
[DEVELOPMENT PLAN]



Second Floor Plan

EXHIBIT "C"

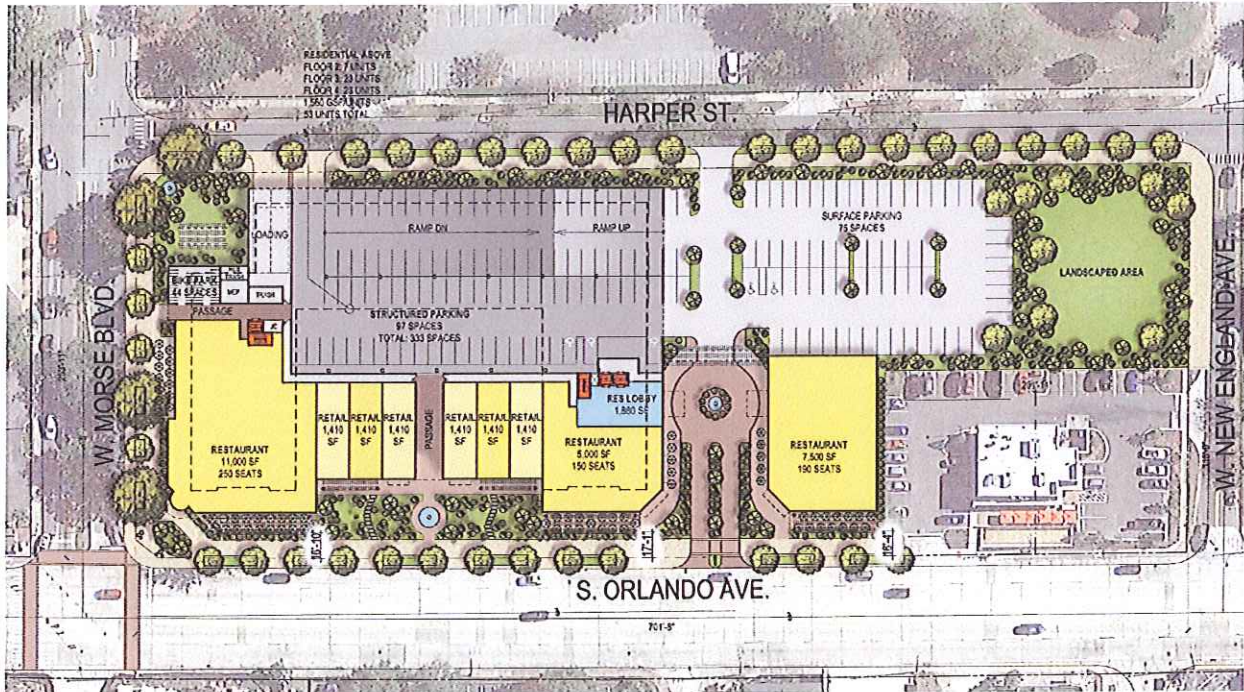
[DEVELOPMENT PLAN]



Typical Floor Plan

EXHIBIT "C"

[DEVELOPMENT PLAN]



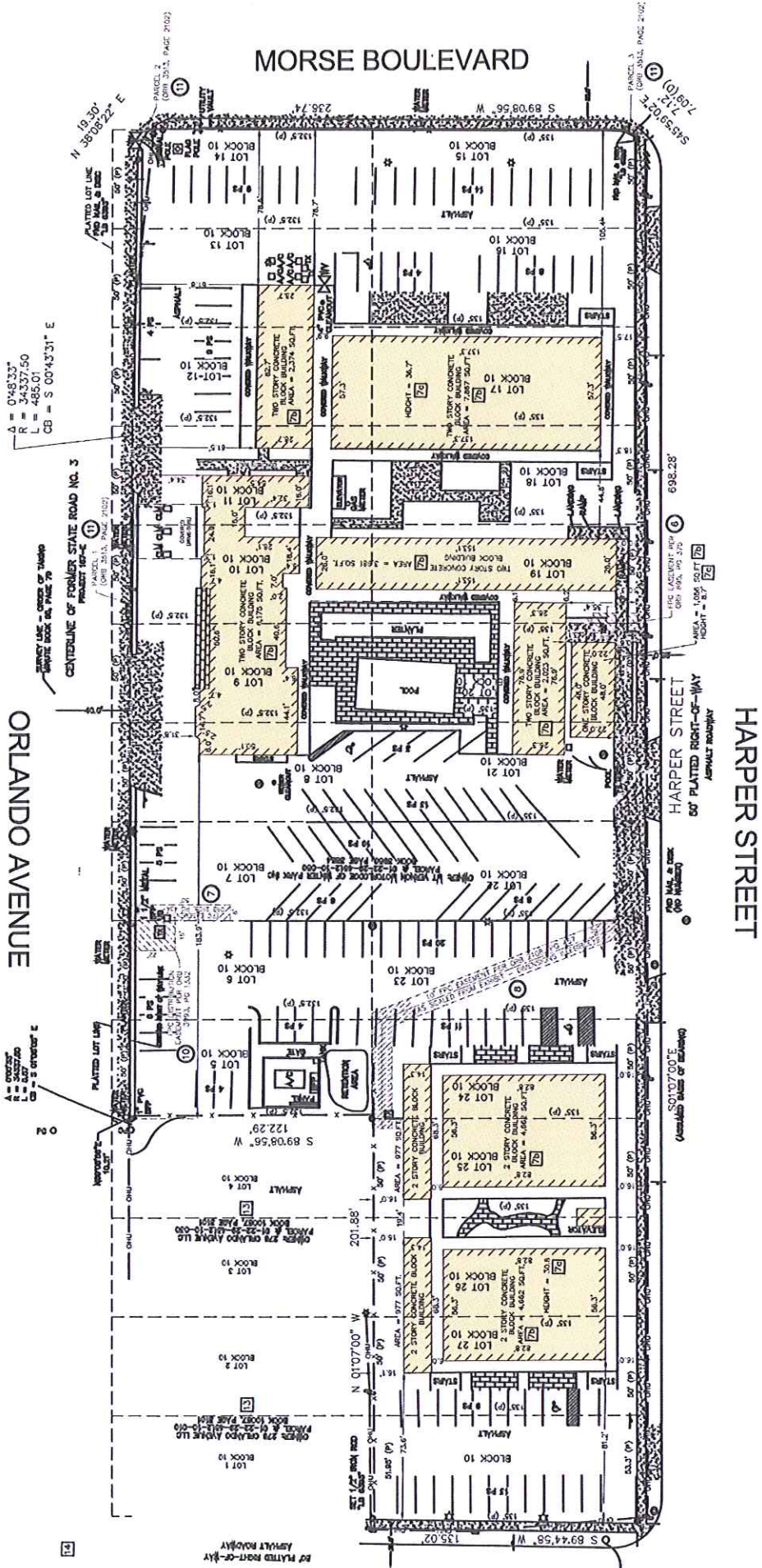
Setback Diagram



The Luxe Writer Park

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NEW ENGLAND AVENUE



HARPER STREET

MORSE BOULEVARD

ORLANDO AVENUE

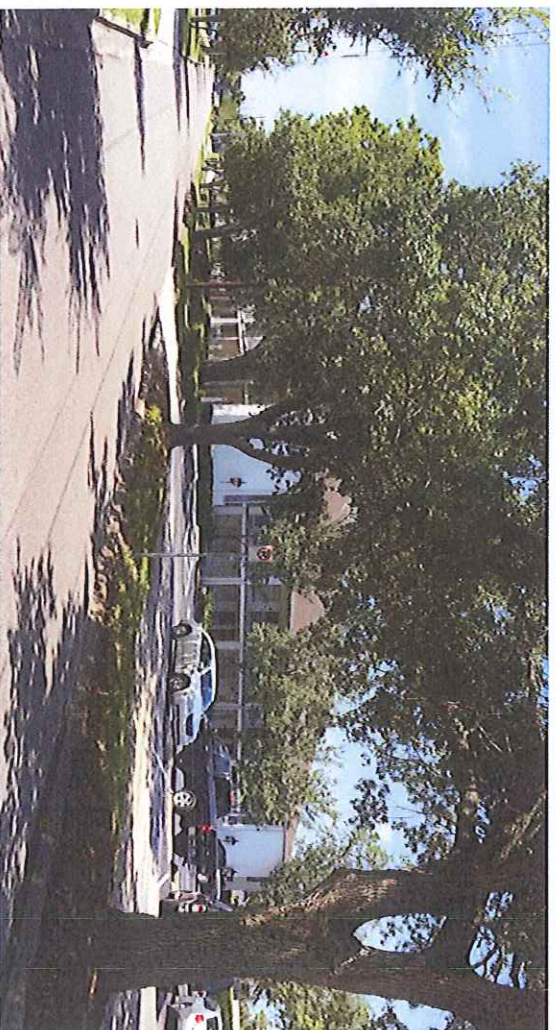
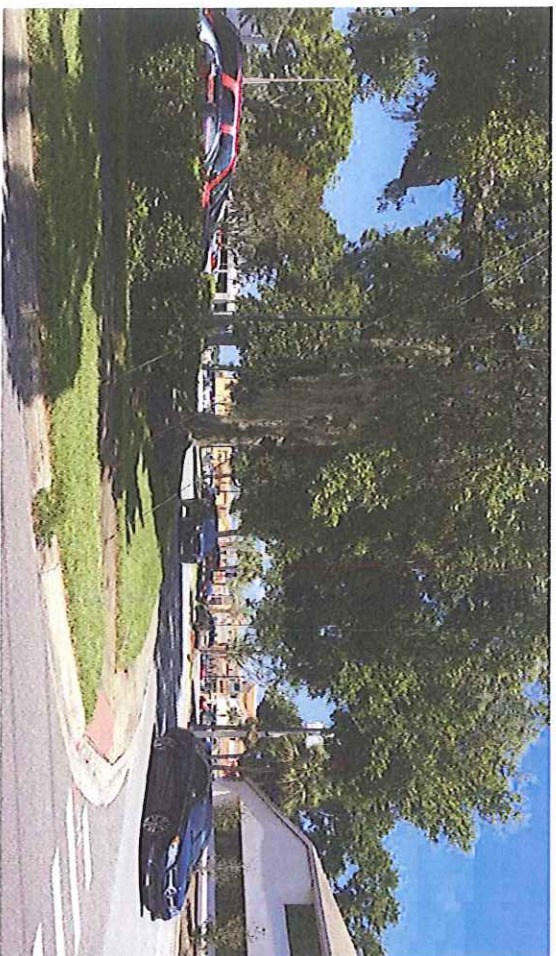
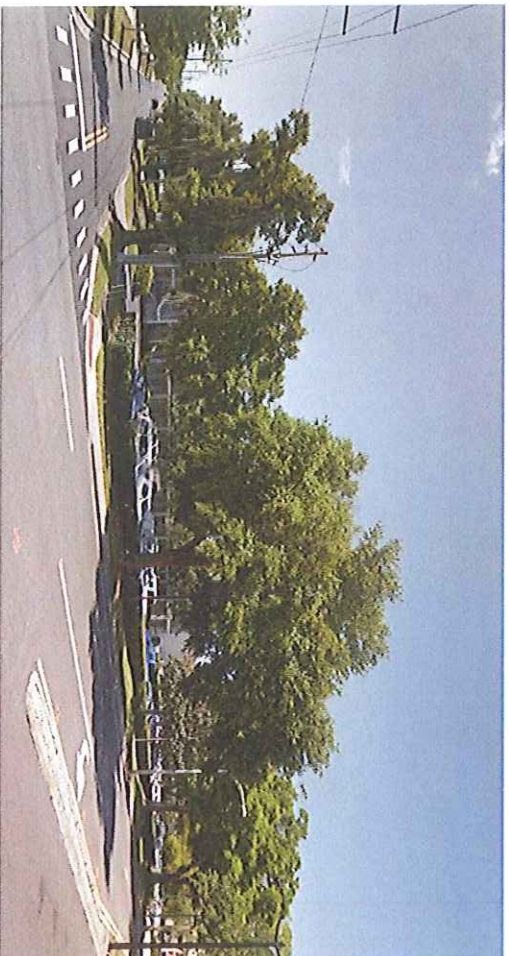
The Luxe
Writer Park

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Site Survey - Mt. Vernon Inn | 2

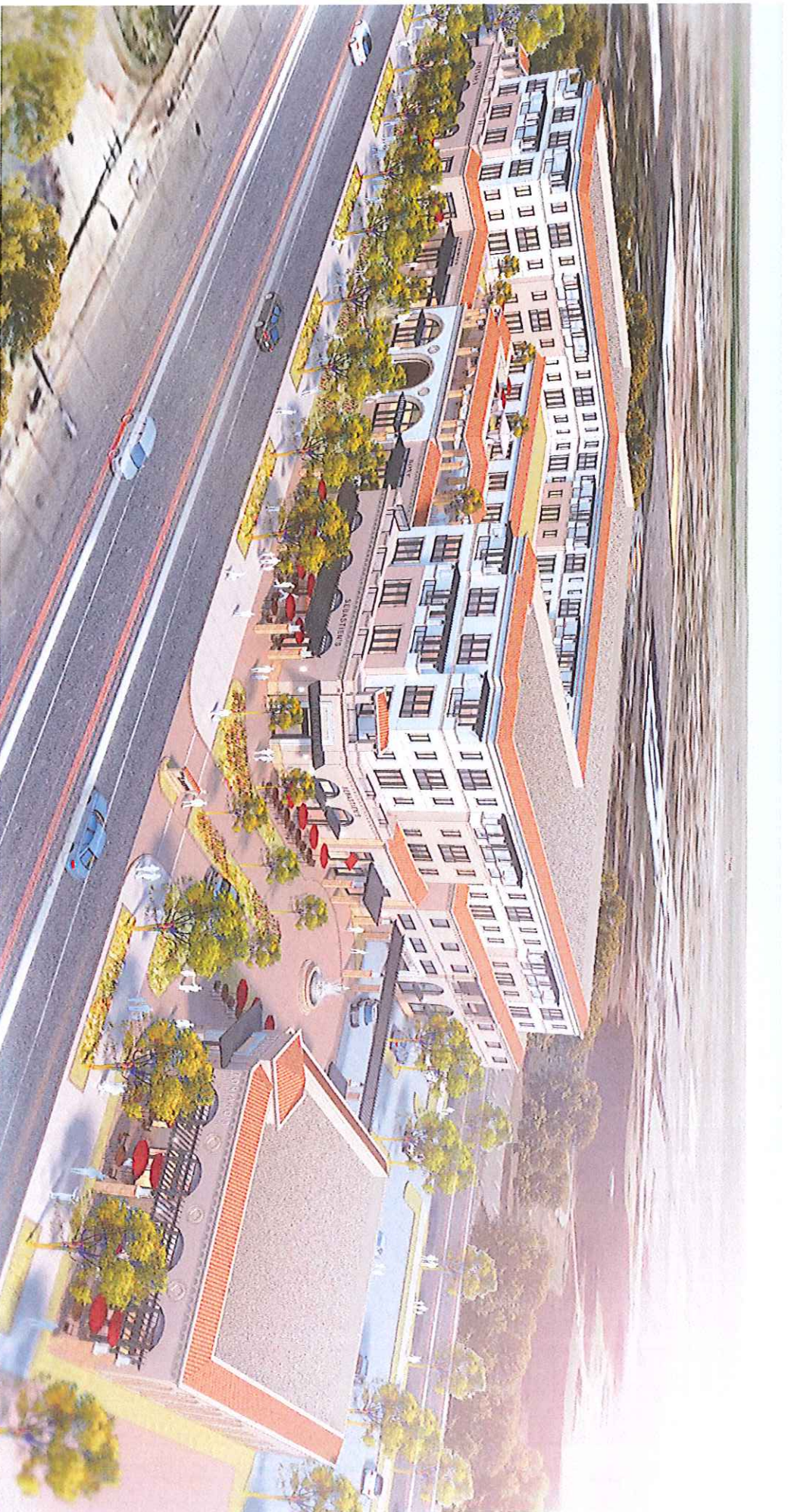
October 24, 2014





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Street View: S. Orlando Ave. & W. New England Ave. | 5

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Street View: S. Orlando Ave. & W. Morse Blvd. | 6

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Street View: Harper St. & W. Morse Blvd. | 7

October 24, 2014

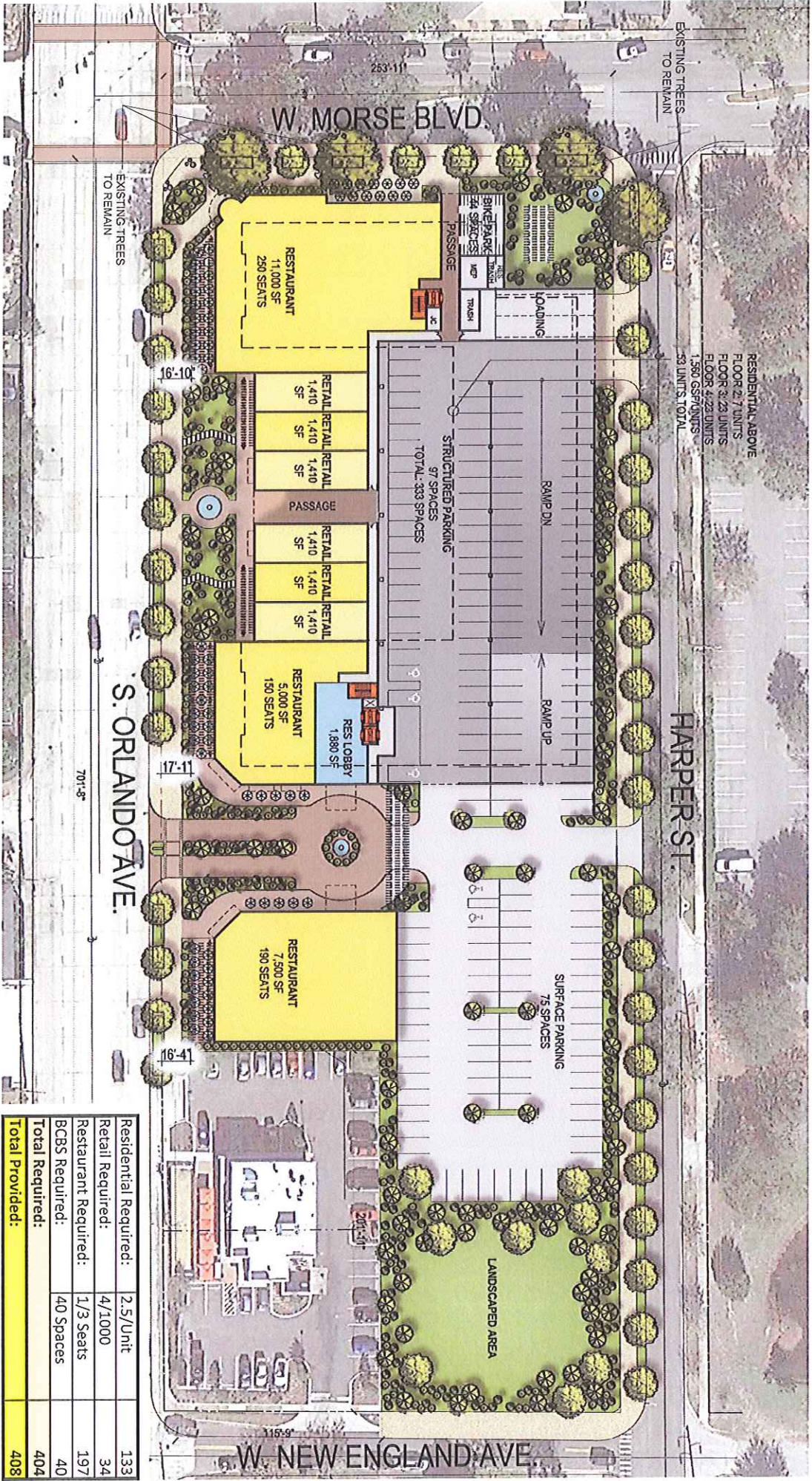


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Street View: Harper St. & W. New England Ave. | 8

October 24, 2014



RESIDENTIAL ABOVE
 FLOOR 2: 7 UNITS
 FLOOR 3: 23 UNITS
 FLOOR 4: 23 UNITS
 1,580 GSF/UNITS
 53 UNITS TOTAL

EXISTING TREES TO REMAIN

S. ORLANDO AVE.

HARPER ST.

W. NEW ENGLAND AVE.

Residential Required:	2.5/Unit	133
Retail Required:	4/1000	34
Restaurant Required:	1/3 Seats	197
BCBS Required:	40 Spaces	40
Total Required:		404
Total Provided:		408

The Luxe
 Winter Park

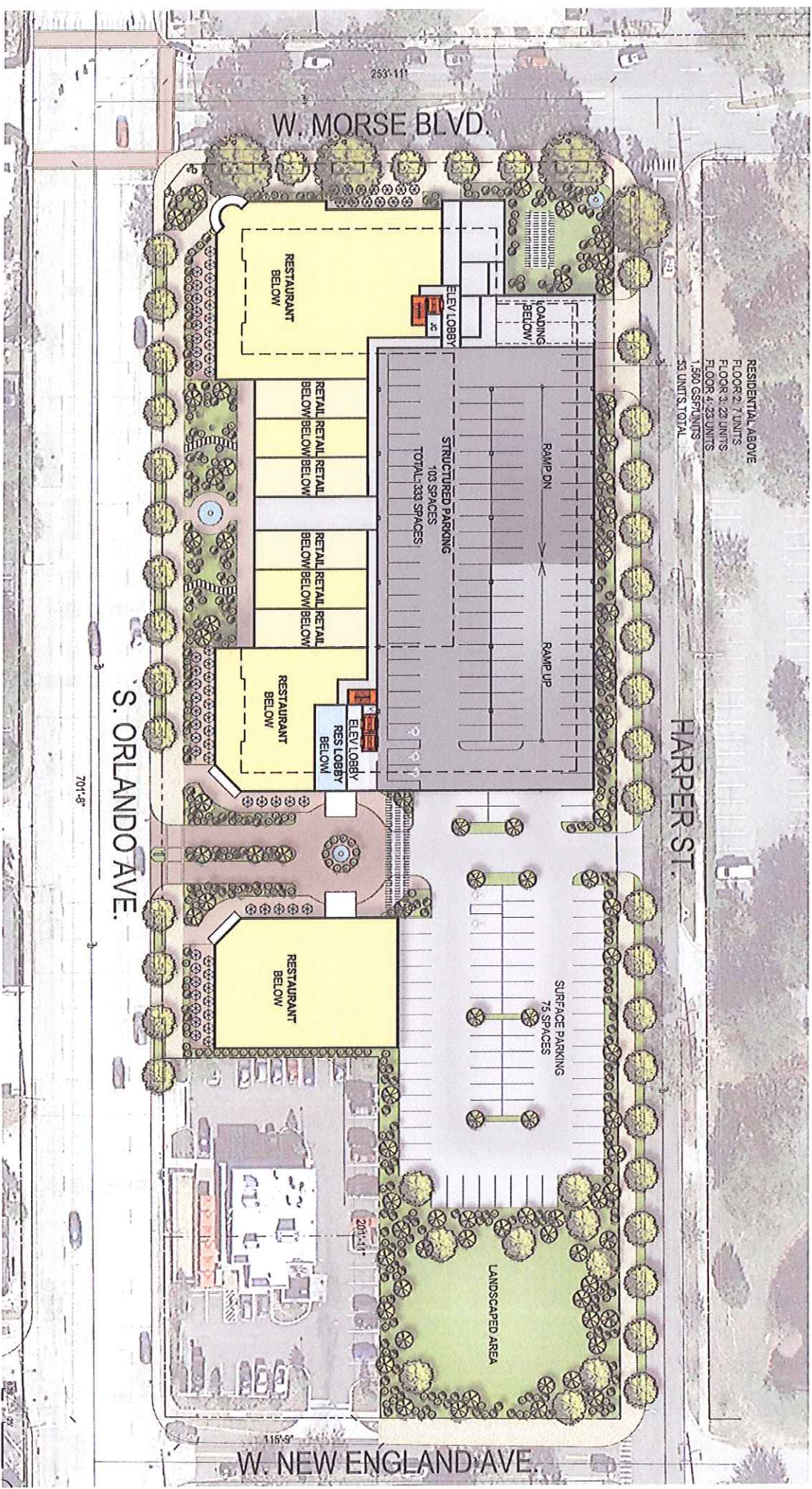
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First Floor Plan | 9

October 24, 2014



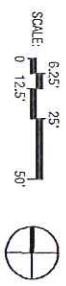
RESIDENTIAL ABOVE
 FLOOR 2: 7 UNITS
 FLOOR 3: 23 UNITS
 FLOOR 4: 23 UNITS
 1,580 GSF UNITS
 53 UNITS TOTAL



The **Luxe**
 Winter Park

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Mezzanine Floor Plan | 10



October 24, 2014

RESIDENTIAL ABOVE
 FLOOR 2: 7 UNITS
 FLOOR 3: 22 UNITS
 FLOOR 4: 23 UNITS
 1,960 GSF/UNITS
 53 UNITS TOTAL



The Luxe
 Writer Park

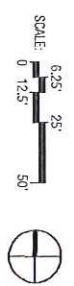
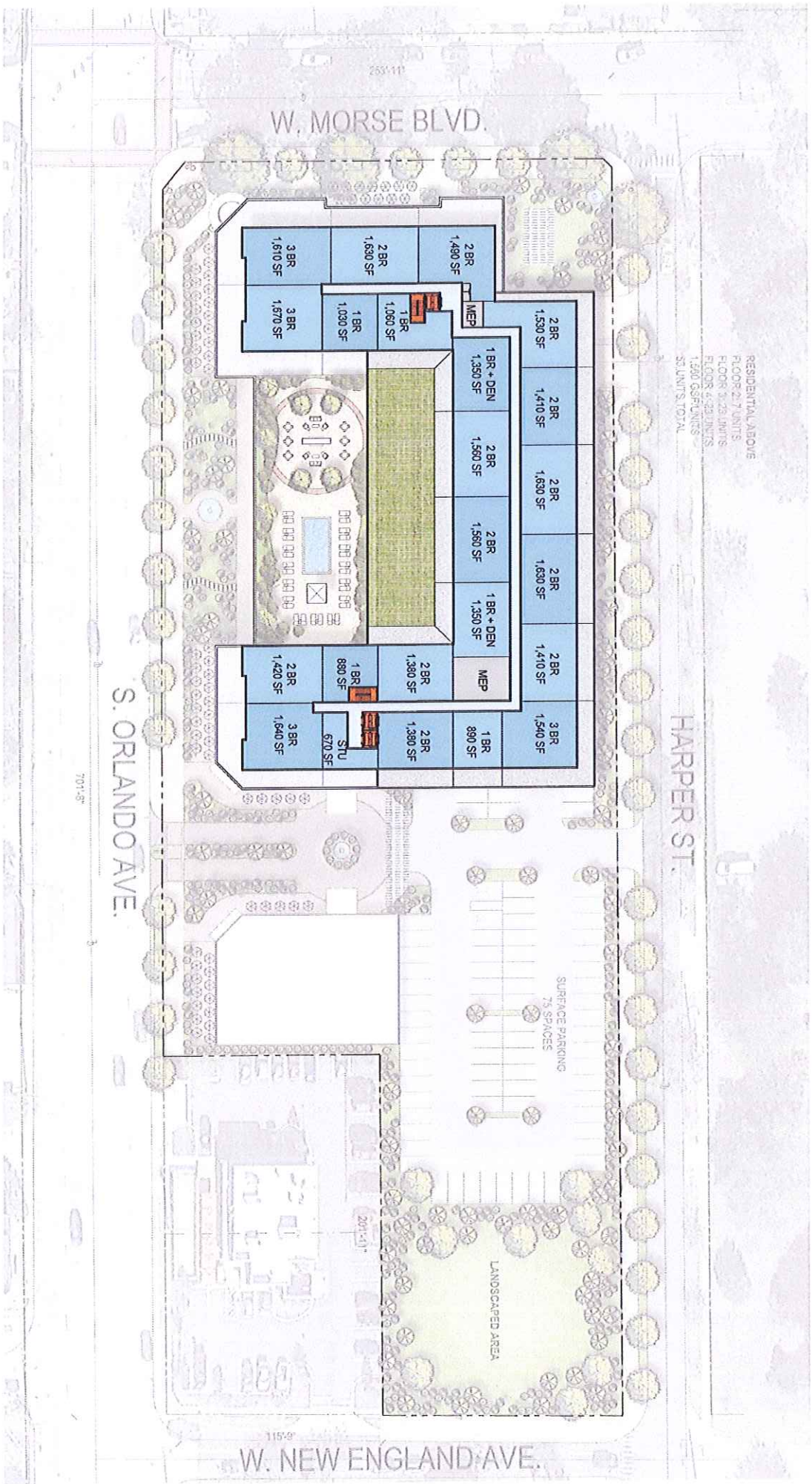
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Second Floor Plan | 11

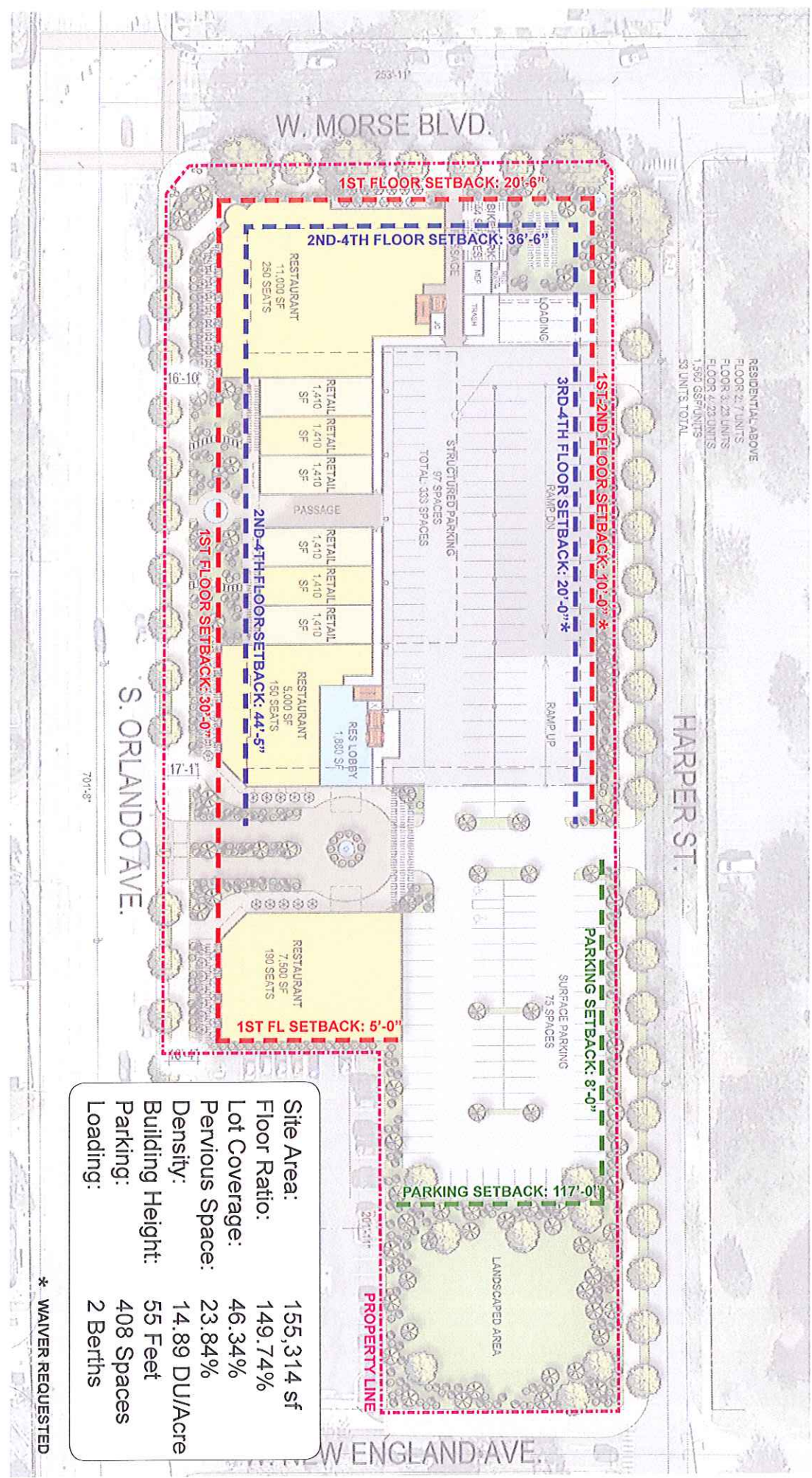


October 24, 2014

RESIDENTIAL ABOVE
 FLOOR #17 UNITS
 FLOOR #23 UNITS
 1,660 GSF/UNITS
 33 UNITS TOTAL

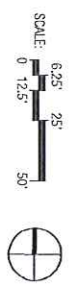


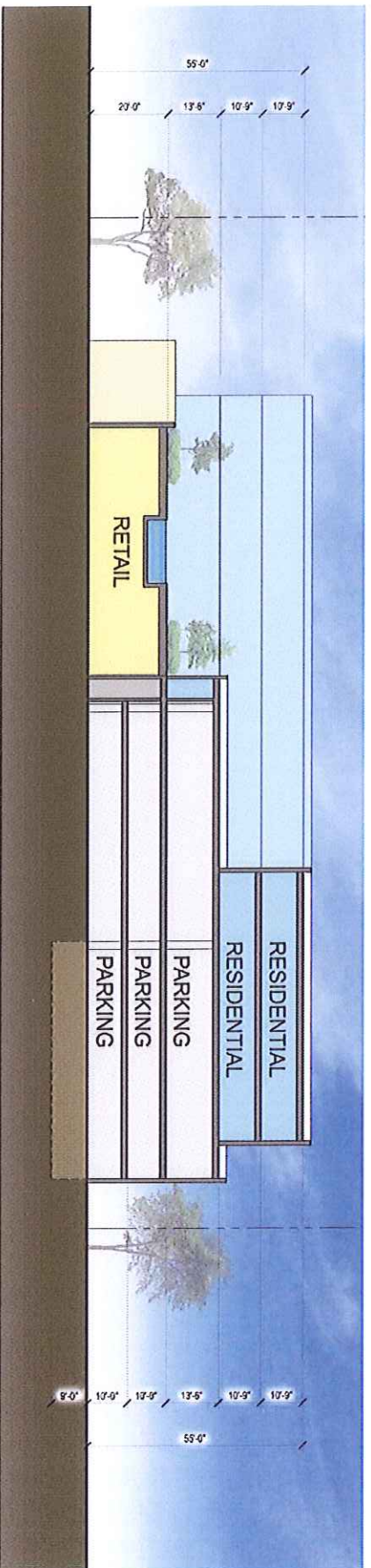
RESIDENTIAL ABOVE
 FLOOR 2, 7 UNITS
 FLOOR 3, 23 UNITS
 FLOOR 4, 23 UNITS
 1,592 GSF/UNITS
 53 UNITS TOTAL



Site Area:	155,314 sf
Floor Ratio:	149.74%
Lot Coverage:	46.34%
Pervious Space:	23.84%
Density:	14.89 DU/Acre
Building Height:	55 Feet
Parking:	408 Spaces
Loading:	2 Berths

* WAIVER REQUESTED





Cross Section

THE LUXE - AREA ANALYSIS: PD-1												
LEVEL	BUILDING HEIGHT	PARKING			RETAIL			RESIDENTIAL			GROSS BUILT AREA	
		GARAGE	SURFACE	GSF	COMMON	RSF	GSF	UNITS	COMMON	RSF		GSF
4	55'-0"							23	4,300	31,640	35,940	35,940
3	43'-0"							23	4,300	31,640	35,940	35,940
2	31'-0"	133		42,530				7	4,580	9,450	14,030	56,560
M	20'-0"	103		32,160								32,160
1	10'-0"	97	75	31,608	6,502	31,980	38,482		1,880		1,880	71,970
TOTAL	55'-0"	333	75	106,298	6,502	31,980	38,482	53	15,060	72,730	87,790	232,570

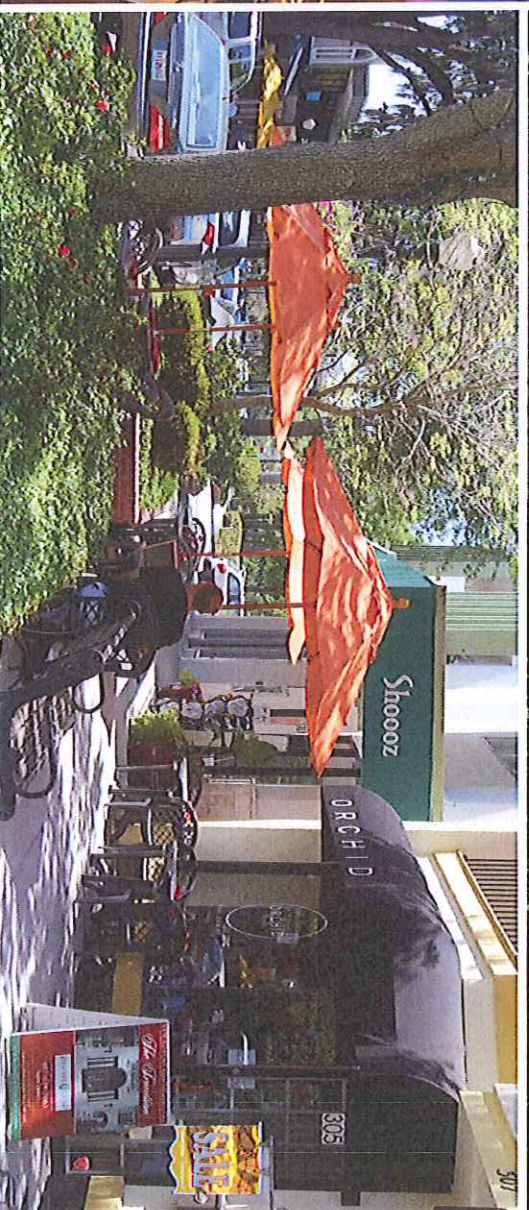
SITE AREA	155,314
LOT COVERAGE	46.34%
FAR	149.74%
PARKING TOTAL	408

PD-1/C-3 DISTRICT DEVELOPMENT STANDARDS MATRIX				ANTUNOVICH ASSOCIATES / UNICORP NATIONAL DEVELOPMENTS INC.			
DISTRICT	SECTION	REQUIREMENT	QUALIFIER	REQUIRED	REQUIRED - BLENDED	PROVIDED	
PD-1	E (1)	Min/Max Property Size	Properties between 3 and 6 acres	50% used for PD	-	3.00 acres for PD-1 (84.3%), 0.56 acres for C-3 (15.7%)	
PD-1	E (2)	FAR	4-story properties	Maximum 175%	-	149.74% (232,570 SF)	
C-3	E (3)			Maximum 45%	155%		
PD-1	E (3)	Lot Coverage		Maximum 47%	-	46.34% (71,970 SF)	
C-3	E (3)			Maximum 45%	46.69%		
PD-1	E (4)	Minimum Open Space	Natural pervious green space	Minimum 25%	-	23.84% (37,030 SF)	
C-3	E (3)			Minimum 15%	23.43%		
PD-1	E (5)	Street front setbacks	Primary 4 lane road / 1 story	20 feet	-	30'-0"	
			Primary 4 lane road / 3+ story	30 feet	-	44'-5"	
			Secondary 2 lane road / 1 story	20 feet	-	10'-0"	
			Secondary 2 lane road / 3+ story	25 feet	-	20'-0"	
PD-1	E (6)	Landscaping Protection Zones	Along primary four lane roads	7 feet wide	-	8 feet wide	
PD-1	E (7)	Side setbacks	one or two story building components	5 feet	-	5 feet	
PD-1	E (8)	Rear setbacks	one or two story building components	10 feet	-	N/A	
PD-1	E (9)	Building Length	In excess of 120 feet	articulation to create relief	-	provided	
PD-1	E (10)	Terracing	For any building over 2 stories	minimum 5' step back	-	5' - 15' step back	
PD-1	E (12)	Density	Residential Units	17 Units Per Acre	53 Units Total	14.89 Units per Acre (55 units)	
C-3							
PD-1	E (13)	Building Height	4-story maximum height	55 feet	-	55 feet	
ALL	-	Parking	Residential	2 spaces per Unit (106 spaces)	-		
			Retail	4 spaces per 1,000 SF (34 spaces)	-		
			Restaurant	1 space per 3 seats (197 spaces)	-	408	
			BCBS	40 spaces	-		
			TOTAL	404 spaces	-		
ALL	-	Loading	Retail	(1) 55' berth per 20,000 SF	-	(2) 55' berths	

THE LUXE - COMPARISON CHART

	EXISTING		PROPOSED		PD-1
	MT. VERNON INN	C-3	THE LUXE	PD-1/C-3 BLENDED	
SITE AREA	155,314 SF				
FAR	59.48 % (92,380 SF)	Maximum 45%	149.74% (232,570 SF)	Maximum 154.59%	Maximum 175%
LOT COVERAGE	30.10% (46,720 SF)	Maximum 45%	46.34% (71,970 SF)	Maximum 46.69%	Maximum 47%
PERVIOUS SPACE	17.15% (26,640 SF)	Minimum 15%	23.84% (37,030 SF)	Minimum 23.43%	Minimum 25%
HEIGHT	36'	55'	55'	55'	55'
PARKING	160 Spaces	165 Spaces Required + Restaurant Requirement*	408 Spaces	404 Spaces Required (40 Spaces for BCBS)	-

* On-site restaurant capacity unknown and required spaces for this use are not included in the number. Existing inn contains 147 guest rooms and 5 conference rooms. The required parking number is based on these two uses only. Conference rooms are estimated to be approximately 4,000 sf based on capacity of 150 persons.

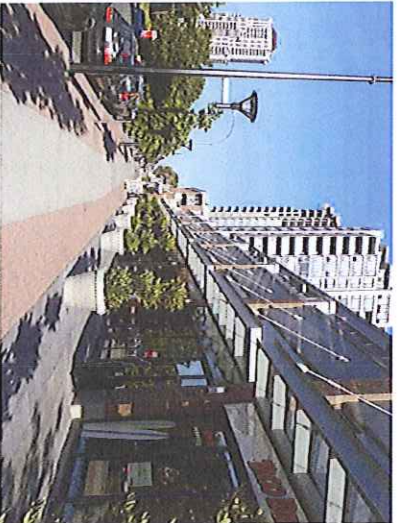


The Luxe Winter Park

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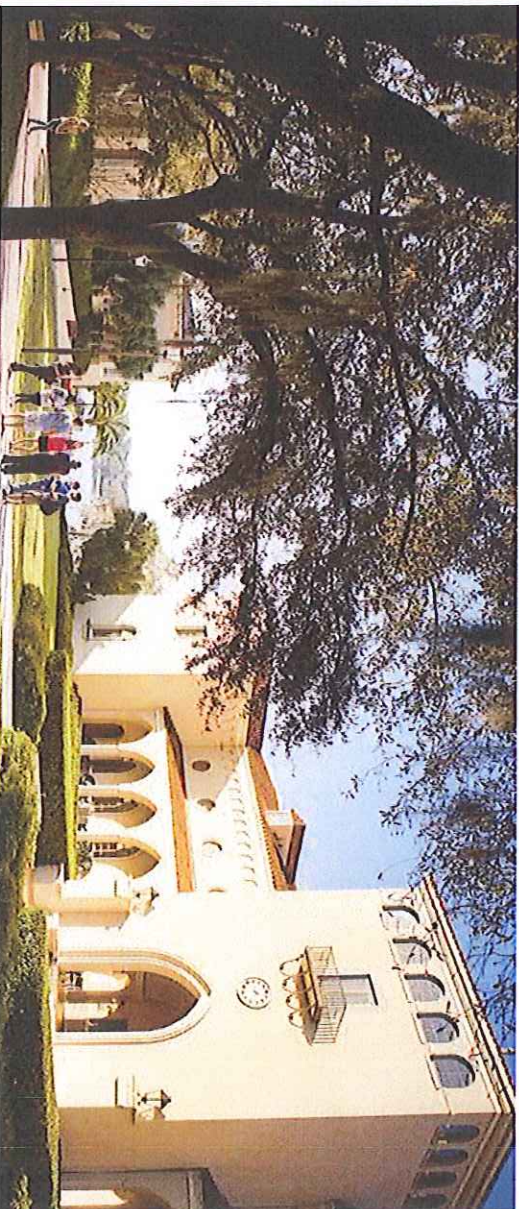
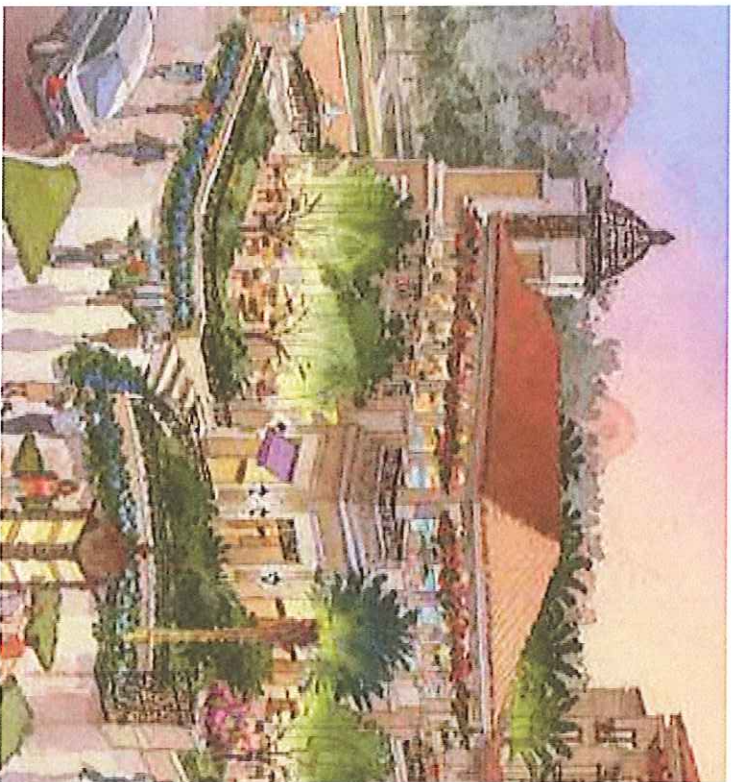
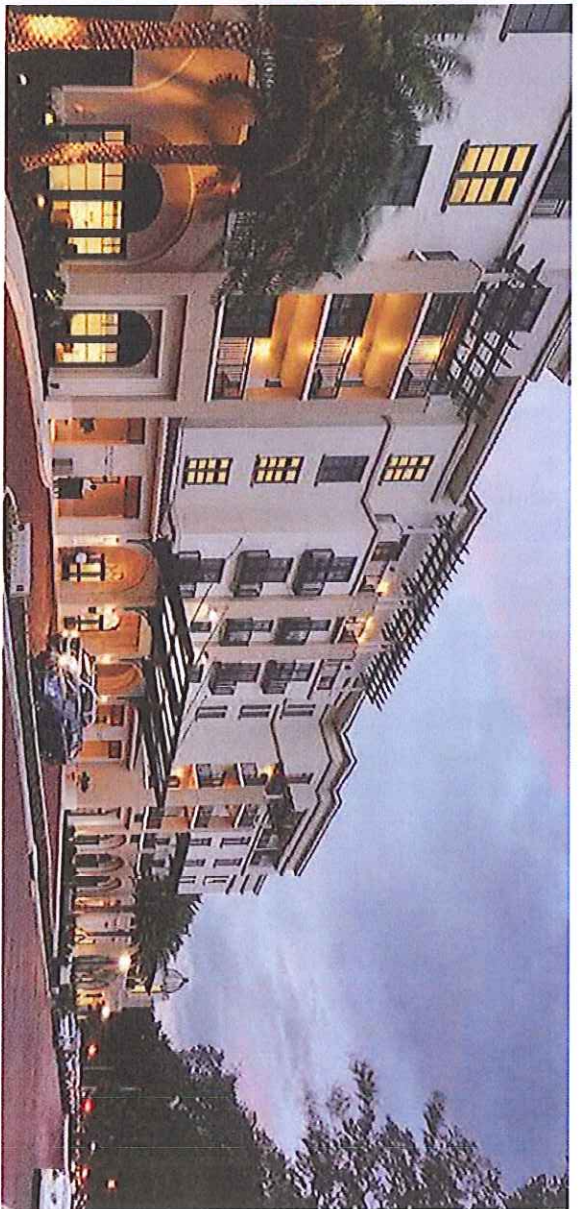
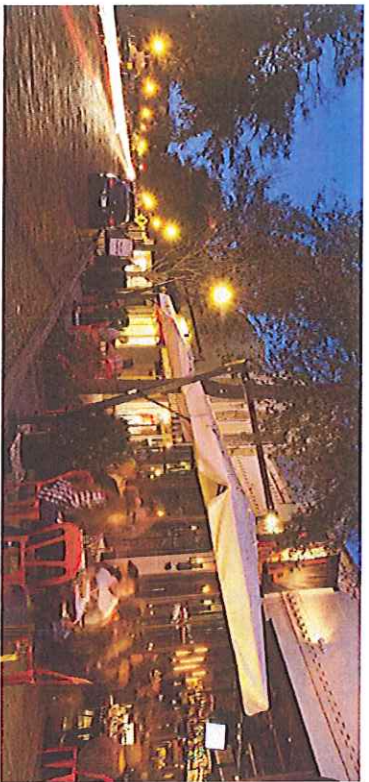
Winter Park | 17

October 24, 2014



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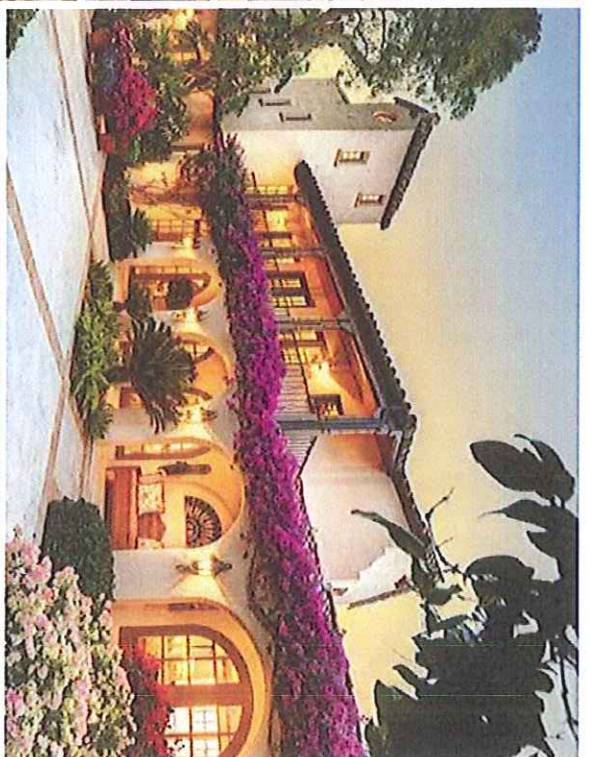
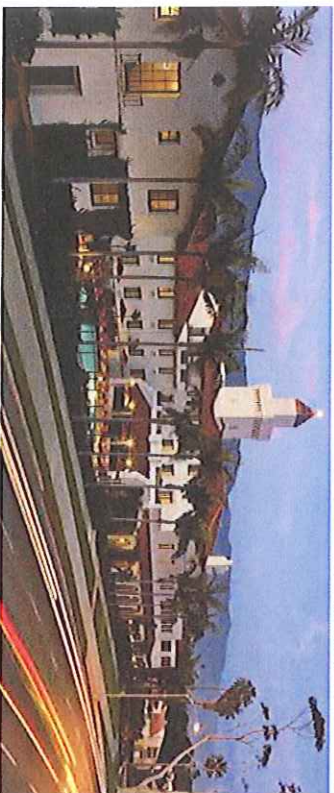
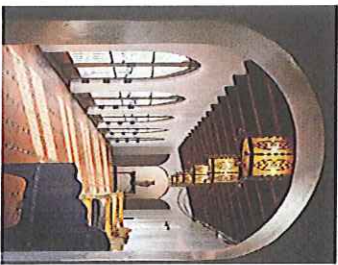
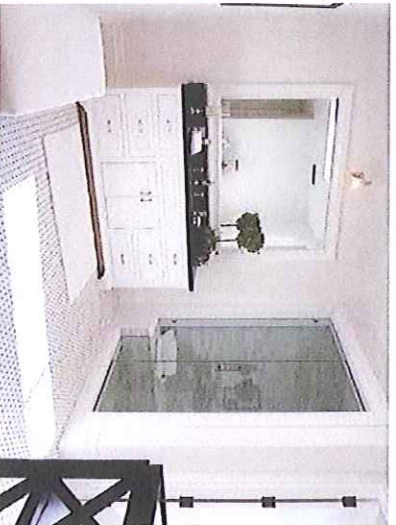


The Luxe Writer Park

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The Alford Inn | 19

October 24, 2014



The Luxe Winter Park

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Residential Reference Images | 20

October 24, 2014

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF WINTER PARK, FLORIDA AMENDING CHAPTER 58, "LAND DEVELOPMENT CODE", ARTICLE I "COMPREHENSIVE PLAN" IN THE FUTURE LAND USE ELEMENT SO AS TO CHANGE THE COMPREHENSIVE PLAN FUTURE LAND USE MAP DESIGNATION FOR THREE ACRES OF THE PROPERTY LOCATED AT 110 S. ORLANDO AVENUE FROM COMMERCIAL TO PLANNED DEVELOPMENT ONE AND TO AMEND THE FUTURE LAND USE SERIES MAP 1A TO REFLECT THIS PROPERTY AS A CANDIDATE AREA FOR PLANNED DEVELOPMENT FUTURE LAND USE; PROVIDING FOR CONFLICTS, SEVERABILITY AND EFFECTIVE DATE.

WHEREAS, the Winter Park City Commission adopted its Comprehensive Plan on February 23, 2009 via Ordinance 2762-09, and

WHEREAS, Section 163.3184, Florida Statutes, establishes a process for adoption of comprehensive plans or plan amendments amending the future land use designation of property; and

WHEREAS, the owners of the property at 110 S. Orlando Avenue, as petitioner for a future land use amendment, is desirous of amending the future land use designation from Commercial to Planned Development One; and

WHEREAS, Section 163.3187(1)(c) allows the City to revise the maps of the City's Comprehensive Plan utilizing the small scale adoption process when such map revisions relate to and are adopted simultaneously with small scale future land use map amendments; and

WHEREAS, the Comprehensive Plan amendment meets the criteria established by Chapter 163 and 166, Florida Statutes; and pursuant to and in compliance with law, notice has been given to Orange County and to the public by publication in a newspaper of general circulation to notify the public of this proposed Ordinance and of public hearings to be held; and

WHEREAS, the Winter Park Planning and Zoning Commission, acting as the designated Local Planning Agency, has reviewed and recommended adoption of the proposed Comprehensive Plan amendment, having held an advertised public hearing on November 4, 2014, provided for participation by the public in the process, and rendered its recommendations to the City Commission; and

WHEREAS, the Winter Park City Commission has reviewed the proposed Comprehensive Plan amendment and held advertised public hearings on December 8, 2014 and January 12, 2015 and provided for public participation in the process in accordance with the requirements of state law and the procedures adopted for public participation in the planning process.

NOW THEREFORE BE IT ENACTED BY THE CITY COMMISSION OF THE CITY OF WINTER PARK, FLORIDA, AS FOLLOWS:

SECTION 1. Future Land Use Map Amendment. That Chapter 58 "Land Development Code", Article I, "Comprehensive Plan" future land use plan map is hereby amended so as to change the future land use map designation from Commercial to Planned Development One on three acres of the property at 110 S. Orlando Avenue, Property Tax ID # 01-22-29-4512-10-050, more particularly described in Attachment "A".

SECTION 2. Planned Development Candidate Area Map Amendment. That Chapter 58 "Land Development Code", Article I, "Comprehensive Plan", is hereby amended to revise within the Future Land Use map series, Map 1-6a in order to indicate the property at 110 S. Orlando as a candidate for Planned Development One future land use, as shown in Attachment "B".

SECTION 3. Severability. If any Section or portion of a Section of this Ordinance proves to be invalid, unlawful, or unconstitutional, it shall not be held to invalidate or impair the validity, force, or effect of any other Section or part of this Ordinance.

SECTION 4. Conflicts. All Ordinances or parts of Ordinances in conflict with any of the provisions of this Ordinance are hereby repealed.

SECTION 5. Effective Date. An amendment adopted under this paragraph does not become effective until 31 days after adoption. If timely challenged, an amendment may not become effective until the state land planning agency or the Administration Commission enters a final order determining that the adopted small scale development amendment is in compliance.

ADOPTED at a regular meeting of the City Commission of the City of Winter Park, Florida, held in City Hall, Winter Park, on this _____ day of _____, 2015.

Mayor

Attest:

City Clerk

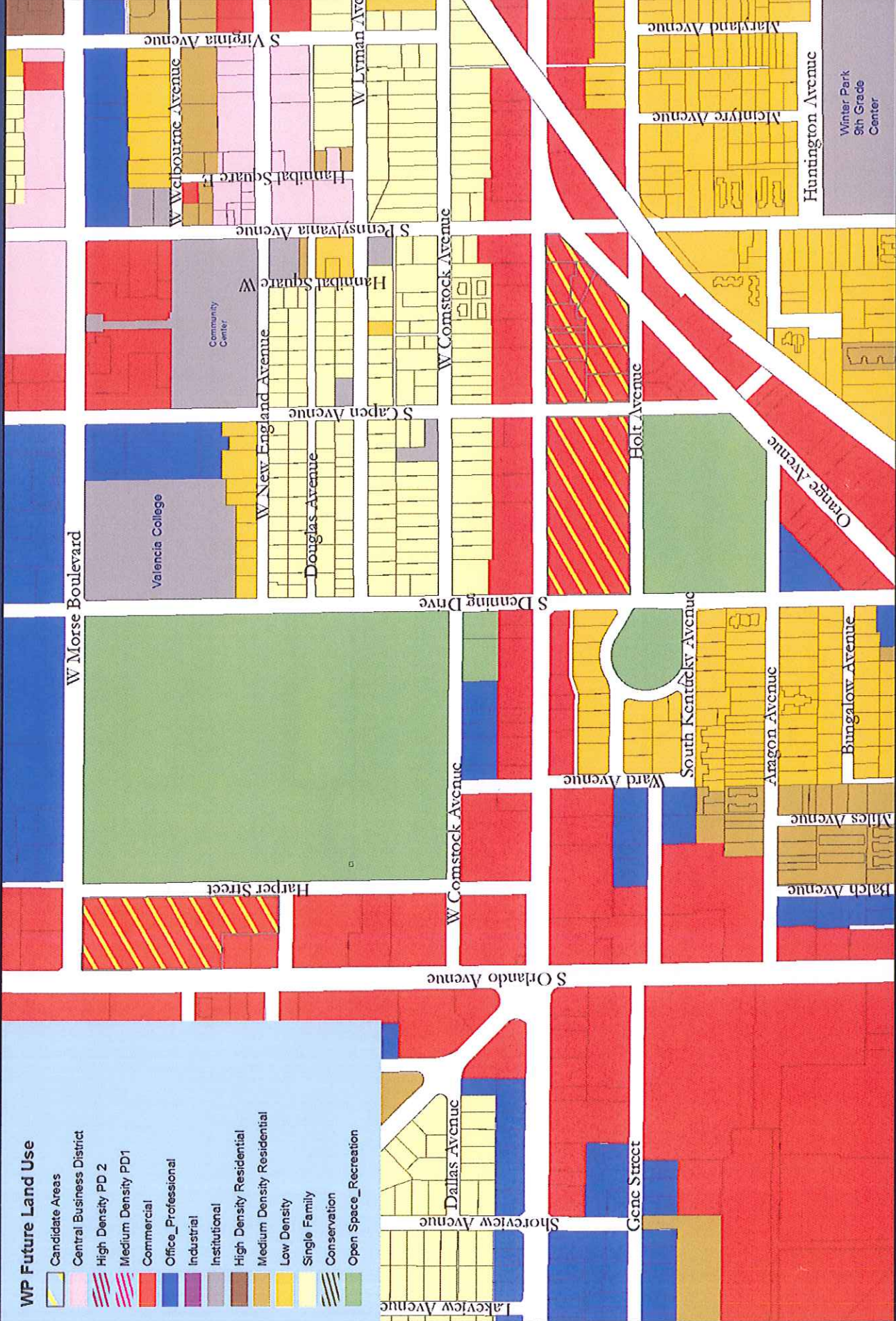
ATTACHMENT A
LEGAL DESCRIPTION

DESCRIPTION:

THAT PART OF LOTS 5 THROUGH 14, ALL OF LOTS 15 THROUGH 24, AND PART OF LOT 25, ALL IN BLOCK 10, OF THE PLAT OF "LAKE ISLAND ESTATES", SAID PLAT AS RECORDED IN PLAT BOOK M, PAGE 95 OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, AND BEING FURTHER DESCRIBED AS FOLLOWS:

COMMENCE AT THE SOUTHEAST CORNER OF LOT 28, OF SAID BLOCK 10 OF SAID PLAT OF LAKE ISLAND ESTATES, BEING AT THE INTERSECTION OF THE NORTHERLY RIGHT-OF-WAY LINE OF NEW ENGLAND AVENUE (50' WIDE RIGHT-OF-WAY), ALSO KNOWN AS FAIRVIEW AVENUE, AND THE WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET (50' WIDE RIGHT-OF-WAY); THENCE RUN N00°44'20"W ALONG SAID WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET FOR A DISTANCE OF 183.50 FEET TO THE POINT OF BEGINNING; THENCE, DEPARTING SAID WESTERLY RIGHT-OF-WAY LINE, RUN S89°39'14"W FOR A DISTANCE OF 135.00 FEET; THENCE RUN N00°44'20"W FOR A DISTANCE OF 20.42 FEET; THENCE RUN S89°49'40"W FOR A DISTANCE OF 122.98 FEET TO THE CURRENT EASTERLY RIGHT-OF-WAY LINE OF STATE ROAD NO. 15 & 600, U.S. HIGHWAY 17-92, ALSO KNOWN AS ORLANDO AVENUE, SAID EASTERLY RIGHT-OF-WAY LINE BEING DISTANT 40.00 FEET EASTERLY FROM THE CENTERLINE OF SURVEY OF FDOT RIGHT-OF-WAY MAP, SECTION 75030, STATE ROAD NO. 15/600, ORANGE COUNTY, SHEET 8 OF 20; THENCE RUN ALONG SAID EASTERLY RIGHT-OF-WAY LINE AND A NON-TANGENT CURVE CONCAVE EASTERLY HAVING A RADIUS OF 34,186.95 FEET, A CHORD BEARING OF N00°15'08"W AND A CHORD DISTANCE OF 485.19 FEET; THENCE RUN NORTHERLY, ALONG SAID EASTERLY RIGHT-OF-WAY LINE AND ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF 00°48'47" FOR A DISTANCE OF 485.20 FEET TO A NON-TANGENT LINE; THENCE, DEPARTING SAID EASTERLY RIGHT-OF-WAY LINE, RUN N38°43'39"E ACROSS A CORNER CLIP FOR A DISTANCE OF 19.25 FEET TO THE SOUTHERLY RIGHT-OF-WAY LINE OF MORSE BOULEVARD (70' WIDE RIGHT-OF-WAY); THENCE RUN N89°54'00"E ALONG SAID SOUTHERLY RIGHT-OF-WAY LINE FOR A DISTANCE OF 236.64 FEET; THENCE, DEPARTING SAID SOUTHERLY RIGHT-OF-WAY LINE, RUN S45°59'02"E ACROSS A CORNER CLIP FOR A DISTANCE OF 7.12 FEET TO THE AFORESAID WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET; THENCE RUN S00°44'20"E ALONG SAID WESTERLY RIGHT-OF-WAY LINE FOR A DISTANCE OF 514.91 FEET TO THE POINT OF BEGINNING.

CONTAINING 130,844 SQUARE FEET (3.003 ACRES MORE OR LESS) AND BEING SUBJECT TO ANY RIGHTS-OF-WAY, RESTRICTIONS AND EASEMENTS OF RECORD.



WP Future Land Use

- Candidate Areas
- Central Business District
- High Density PD 2
- Medium Density PD1
- Commercial
- Office_Professional
- Industrial
- Institutional
- High Density Residential
- Medium Density Residential
- Low Density
- Single Family
- Conservation
- Open Space_Recreation

City of Winter Park Planning & Community
Development Department
October, 2014
Source: Orange County Property Appraiser
Created in ArcGIS 10.2 using ArcMap

**East Fairbanks/Orlando Avenue
Planned Development
Candidate Area PD 1**

Coordinate System
NAD 1983 State Plane FL
East FIPS 9901 Feet

After recording return to:
City Clerk
City of Winter Park
401 Park Avenue South
Winter Park, FL 32789

ORDINANCE NO.: _____
(LUXE PD)

AN ORDINANCE OF THE CITY OF WINTER PARK, FLORIDA, AMENDING THE CITY OF WINTER PARK OFFICIAL ZONING DISTRICT MAP AS ESTABLISHED IN THE LAND DEVELOPMENT CODE OF THE CITY OF WINTER PARK, FLORIDA, AS AMENDED AND SUPPLEMENTED; CHANGING THE ZONING OF PROPERTY LOCATED AT 110 SOUTH ORLANDO AVENUE, AS DESCRIBED IN SECTION 1 OF THIS ORDINANCE FROM THE COMMERCIAL C-3 DISTRICT TO THE PLANNED DEVELOPMENT (PD-1) DISTRICT IN ORDER TO CREATE THE LUXE PD AS REFERENCED IN SECTION 2 OF THIS ORDINANCE; PROVIDING FOR CONFLICTS, SEVERABILITY AND EFFECTIVE DATE.

WHEREAS, two public hearings on the question of establishing a zoning district of the property hereinafter described have been held in the City of Winter Park, Florida, and at such hearings, interested parties and citizens for and/or against the proposed establishment of this zoning district were heard; and

WHEREAS, the property hereinafter described has a designation on the City of Winter Park Comprehensive Plan's Future Land Use Map as PD-1; and

WHEREAS, the proposed rezoning to PD-1 is consistent with the future land use designation; and

WHEREAS, the Luxe PD Order (Attachment B) proposes 31,960 square feet of commercial development, including restaurants, retail space and 53 multi-family units; and

WHEREAS, after said public hearings, the City Commission of the City of Winter Park, Florida determines that the zoning district of said property shall be established as PD-1 as defined in the Zoning Code, Code of Ordinances of the City of Winter Park, Florida.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF WINTER PARK, FLORIDA, AS FOLLOWS:

SECTION 1. OFFICIAL ZONING MAP AMENDMENT. That Chapter 58 "Land Development Code", Article III, "Zoning" and the Official Zoning Map is hereby amended so as to change the zoning designation from Commercial (C-3) District to Planned Development One (PD-1) District on three acres of the property at 110 S. Orlando Avenue, subject to the terms of a development order agreement, said property more particularly described in Attachment "A".

SECTION 2. LUXE DEVELOPMENT ORDER. This Ordinance shall be governed by the terms and conditions of the Luxe PD Order attached (Attachment B) and incorporated herein by reference.

SECTION 3. CONFLICTS. All ordinance or parts of ordinances in conflict with any of the provisions of this Ordinance are hereby repealed to the extent of such conflict.

SECTION 4. SEVERABILITY. If any section, subsection, sentence, clause, phrase, word or provision of this Ordinance is for any reason held invalid or unconstitutional by any court of competent jurisdiction, whether for substantive, procedural, or any other reason, such portion shall be deemed a separate, distinct and independent provision, and such holding shall not affect the validity of the remaining portions of this Ordinance.

SECTION 5. EFFECTIVE DATE. This Ordinance shall become effective upon the effective date of Ordinance No. _____. If Ordinance No. _____ does not take effect, this Ordinance will not become effective.

ADOPTED by the City Commission of the City of Winter Park, Florida, in a regular meeting held on the _____ day of _____, 2014.

Kenneth W. Bradley, Mayor

ATTEST:

Cynthia S. Bonham, City Clerk

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ATTACHMENT A
LEGAL DESCRIPTION

DESCRIPTION:

THAT PART OF LOTS 5 THROUGH 14, ALL OF LOTS 15 THROUGH 24, AND PART OF LOT 25, ALL IN BLOCK 10, OF THE PLAT OF "LAKE ISLAND ESTATES", SAID PLAT AS RECORDED IN PLAT BOOK M, PAGE 95 OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, AND BEING FURTHER DESCRIBED AS FOLLOWS:

COMMENCE AT THE SOUTHEAST CORNER OF LOT 28, OF SAID BLOCK 10 OF SAID PLAT OF LAKE ISLAND ESTATES, BEING AT THE INTERSECTION OF THE NORTHERLY RIGHT-OF-WAY LINE OF NEW ENGLAND AVENUE (50' WIDE RIGHT-OF-WAY), ALSO KNOWN AS FAIRVIEW AVENUE, AND THE WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET (50' WIDE RIGHT-OF-WAY); THENCE RUN N00°44'20"W ALONG SAID WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET FOR A DISTANCE OF 183.50 FEET TO THE POINT OF BEGINNING; THENCE, DEPARTING SAID WESTERLY RIGHT-OF-WAY LINE, RUN S89°39'14"W FOR A DISTANCE OF 135.00 FEET; THENCE RUN N00°44'20"W FOR A DISTANCE OF 20.42 FEET; THENCE RUN S89°49'40"W FOR A DISTANCE OF 122.98 FEET TO THE CURRENT EASTERLY RIGHT-OF-WAY LINE OF STATE ROAD NO. 15 & 600, U.S. HIGHWAY 17-92, ALSO KNOWN AS ORLANDO AVENUE, SAID EASTERLY RIGHT-OF-WAY LINE BEING DISTANT 40.00 FEET EASTERLY FROM THE CENTERLINE OF SURVEY OF FDOT RIGHT-OF-WAY MAP, SECTION 75030, STATE ROAD NO. 15/600, ORANGE COUNTY, SHEET 8 OF 20; THENCE RUN ALONG SAID EASTERLY RIGHT-OF-WAY LINE AND A NON-TANGENT CURVE CONCAVE EASTERLY HAVING A RADIUS OF 34,186.95 FEET, A CHORD BEARING OF N00°15'08"W AND A CHORD DISTANCE OF 485.19 FEET; THENCE RUN NORTHERLY, ALONG SAID EASTERLY RIGHT-OF-WAY LINE AND ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF 00°48'47" FOR A DISTANCE OF 485.20 FEET TO A NON-TANGENT LINE; THENCE, DEPARTING SAID EASTERLY RIGHT-OF-WAY LINE, RUN N38°43'39"E ACROSS A CORNER CLIP FOR A DISTANCE OF 19.25 FEET TO THE SOUTHERLY RIGHT-OF-WAY LINE OF MORSE BOULEVARD (70' WIDE RIGHT-OF-WAY); THENCE RUN N89°54'00"E ALONG SAID SOUTHERLY RIGHT-OF-WAY LINE FOR A DISTANCE OF 236.64 FEET; THENCE, DEPARTING SAID SOUTHERLY RIGHT-OF-WAY LINE, RUN S45°59'02"E ACROSS A CORNER CLIP FOR A DISTANCE OF 7.12 FEET TO THE AFORESAID WESTERLY RIGHT-OF-WAY LINE OF HARPER STREET; THENCE RUN S00°44'20"E ALONG SAID WESTERLY RIGHT-OF-WAY LINE FOR A DISTANCE OF 514.91 FEET TO THE POINT OF BEGINNING.

CONTAINING 130,844 SQUARE FEET (3.003 ACRES MORE OR LESS) AND BEING SUBJECT TO ANY RIGHTS-OF-WAY, RESTRICTIONS AND EASEMENTS OF RECORD.

ATTACHMENT B
[LUXE PD ORDER]

MEMORANDUM

DATE: October 28, 2014

TO: Mr. Jeffrey Briggs
Manager, Planning & Community Development
City of Winter Park, Florida

FROM: Joseph Antunovich
Antunovich Associates

SUBJECT: The Luxe Winter Park
Southeast Corner of S. Orlando Avenue & W. Morse Avenue, Winter Park
Planned Development Application for Planned Development One (PD-1) Zoning

ATTACHMENT: The Luxe Winter Park Project Design Booklet, dated October 24, 2014
Traffic Impact Analysis, dated August 15, 2014

Mr. Briggs,

On behalf of our Client, Unicorp National Developments, Inc, thank you for the opportunity to submit these additional materials to support our application for The Luxe Planned Development project in Winter Park, Florida. We have included a series of project-specific responses to each subsection (a-r) of Paragraph 3 of Sec. 58-82, "Planned Development One (PD-1) District," of the Winter Park Zoning Code. These responses are found below. We have also included herewith an updated design booklet for The Luxe Winter Park, dated October 24, 2014, and the previously submitted Traffic Impact Analysis, dated August 15, 2014. Copies of these documents are being delivered via overnight mail.

(3) In addition to meeting the requirements of the section, each application for Planned Development zoning shall provide the same application submittals required for conditional uses, and shall also address at a minimum:

a. How the project's level of density/intensity, based on proximity to the historic core of Winter Park, is compatible with the surrounding area.

- *The project is located on a 155,314 SF site (3.57 acres) along a major 4-lane arterial State highway, S. Orlando Avenue, where 29,500 cars drive by per day. The height, massing and scale of The Luxe Winter Park are comparable to other existing structures in the area. The design of the building will be executed in a contextual, sensitive manner, with scale and architecture that is respectful to the historic character of Downtown Winter Park, located just blocks from this site.*

b. The project's retail, office, and residential use standards are to be

- *The project consists of 8,480 RSF of general retail area, and 23,500 RSF of restaurant space, to create a total of 39,580 GSF of retail area. The restaurant space will contain a total maximum of 590 seats. Three residential floors will be constructed above the retail comprised of 87,790 GSF and containing a total of 53 units. The residential units will have an average unit size of 1,656 GSF per unit, inclusive of common area. 408 parking spaces will be provided on site for the development.*

c. The amount of open space and permeable space provided in excess of development anticipated by the existing zoning

- *The project proposes a blended PD-1/C-3 Building Lot Coverage of 71,970 SF (46.34% of the site area), thereby providing 53.66% of total open space. The pervious area provided is a blended PD-1/C-3 37,030 SF (23.84% of the site area). The proposed PD-1 zoning is limited to 3 acres of the site, and requires a 47% maximum lot coverage and 25% pervious area. The existing C-3 zone, the remainder of the site, allows 45% maximum lot coverage, and 15% pervious space.*

d. A justification as to the appropriateness of the proposed height, scale, mass, setbacks, lot coverage and density restrictions

- *The project is proposed within a blended PD-1/C-3 zoning scenario. The "required" zoning figures noted below reflect this proposed zoning blend.*

- *Maximum Allowable height: 55'*

- *Proposed height: 55'*

- *Required Setbacks:*

- 1. Primary 4 lane road / 1 story: 20 feet*
- 2. Primary 4 lane road / 3+ stories: 30 feet*
- 3. Secondary 2 lane road / 1 story: 20 feet*
- 4. Secondary 2 lane road / 3+ stories: 25 feet*

- *Proposed Setbacks:*

- 1. Primary 4 lane road / 1 story: 20 feet*
- 2. Primary 4 lane road / 3+ stories: 30 feet*
- 3. Secondary 2 lane road / 1 story: 10 feet*
- 4. Secondary 2 lane road / 3+ stories: 20 feet*

- *Maximum Allowable Lot Coverage: 46.69%*

- *Proposed Lot Coverage: 46.34%*

- *Maximum Allowable Density: 154.59% Max*

- *Proposed Density: 149.74%*

- *The proposed Luxe Winter Park is located along a major 4-lane arterial street, which contains a dense mix of uses including retail, restaurant, office, and residential use. The proposed height, lot coverage and density are within the allowable limits of the blended PD-1/C-3 zoning. We are requesting a waiver on the proposed setback dimensions on the secondary 2-lane road façade at Harper Street. The building is tiered with setbacks thereby reducing the mass of the structure, as the building ascends vertically. This design creates a more pedestrian-friendly experience, by increasing the amount of natural light penetrating onto the surrounding streetscape.*

e. Provisions to guarantee the long term maintenance of the buildings and amenities

- *Unicorp National Developments, Inc. have established a unique reputation in their numerous developments throughout Orlando that they are dedicated to maintaining luxurious grounds with impeccable maintenance to the architecture and surrounding landscape and amenities. The buildings will be built with durable natural materials, with the local climate and temperatures in mind, and all landscaping elements will consist of native species to ensure their prolonged lifespan in this particular site. Further, Unicorp National Developments, Inc. will be charging a common area maintenance fee to its tenants, which will strictly be used for the comprehensive maintenance of the facilities.*

f. A parking management plan describing the operation of the parking lots and parking garages including whether any fees for parking will be charged, methods of reserving spaces and other anticipated operational functions

- *There will be no charges or control mechanism for retail parking spaces. The residential parking may be separated from retail parking if such separation is needed, allowing a more secure residential parking environment, to be determined at a later date. All required handicap spaces will be included on each level of the parking structure, and clearances will be provided for handicap vans and emergency vehicles on the ground level.*

g. Methods of protection and buffering of residential units within the project, or in the vicinity outside the project, from noise, exhaust and lighting

- *The Luxe Winter Park has a substantial setback along South Orlando Avenue, which includes landscape buffers separating the street from the sidewalk, and the sidewalk from the public seating along the restaurant spaces. The building is further recessed in the middle of the structure along South Orlando Avenue to allow for a lush green area between the sidewalk and the building face. The project also has a substantial setback to the upper residential floors at the West side of the property, along South Orlando Avenue. This setback creates a 3-sided "courtyard" which looks out over South Orlando Avenue. The courtyard will include lush natural plantings, walkable green surfaces, and programmed landscape elements. This assemblage of open space and natural environment will provide ample buffering from noise, exhaust and lighting. In addition to this courtyard, each streetscape will be replete with pedestrian furniture, planting beds, and mature deciduous trees, to provide additional screening from harsh pollutant elements.*

h. How landscaping will be provided in excess of the minimum requirements of the existing zoning.

- *The project proposes 23.84% pervious space, which is far greater than the 15% required within the C-3 zoning classification. As Unicorp National Developments, Inc. has shown in recent projects, these pervious areas will contain large, lush, well-maintained, native landscape species. Street trees will line South Orlando Ave, West Morse Boulevard and Harper Street. The building setbacks along South Orlando Avenue will allow for the opportunity for a large open space that will consist of a combination of landscape, some hardscape, and water features. The Northeast corner of the project will highlight another open green space, and contain a covered trellis, water feature, and green wall. The South end of the development, along West New England Avenue, will contain a large open green space will be included, to create a connectivity with the beautiful park immediately to the East of Harper Street. The project proposes to retain four large, existing live oak trees on the Northern portion of the site.*

i. How the provision of public spaces (courtyards/gathering places) is superior to that required by the existing zoning

- *The Luxe Winter Park will include numerous undulations in the building envelope to create gathering spaces and courtyards, on different levels. At the ground level, a large portion of the retail level steps back to create a large green area with ample space for gathering and congregating. Exterior restaurant seating is provided throughout the project as well. On the first residential level, a very large residential terrace will be provided including green space, lush landscaping, and exterior amenities such as an exterior fireplace, swimming pool, cabanas and public seating areas.*

j. How transportation connections to bus transit and bike trails will occur

- *Bus Route 14 stops at the corner of Morse Blvd. and S. Orlando Ave. 0.3 miles north of our site is a large public transportation hub, at "Winter Park Village." Two existing bike trails occur at or near the project site, neither of which will be disrupted by the proposed project. One existing trail travels North/South on S. Orlando Ave, and another exists within the park to the East, circling Lake Mendsen.*

k. The adequacy of egress/ingress requirements including on-site stacking

- *A new, unobstructed connection between S. Orlando Ave. and Harper St. will be created at mid-block in the proposed development to create an accessible, easy ingress and egress around the proposed site. This new driveway will act as a centrally organizing interior street, with which retail patrons, residents, emergency vehicles, county vehicles, or any other user to the site can use to gain access to parking or queuing. A 75-car surface parking lot is located south of this access road. A 333-car structured parking facility will be located north of this access road, with additional ingress/egress to Harper Street.*
- *The Traffic Report, dated August 15, 2014, estimates the development "will generate 1,024 new net daily trips and 91 new net P.M. peak hour trips" and concludes that "the impacted roadways and intersections within the project's one-half mile impact area are currently operating at satisfactory Levels of Service, and will continue to do so at the project buildout in 2015." In addition, the single driveway on US 17-92 and two driveways on Harper Street "will provide adequate access to the proposed uses."*

l. To what degree affordable and workforce housing has been included within the project, and if not, methods of providing that public need off-site.

- *The project is a very low-density project, not requesting any bonuses for additional residential units per acre. Thus, the project will not provide affordable or workforce housing.*

m. The method of sidewalk and streetscape enhancement that will be undertaken that is superior to that required by the existing zoning

- *The project is adhering to the PD-1 additional landscape requirements, which is more stringent than the C-3 landscape standards. All sidewalks will be upgraded with new paving, as well as street trees planted within landscape buffers, which still allow visual access between them. Three new brick-paved cross walks will be installed at the corner of South Orlando Avenue and West Morse Boulevard, providing connectivity to the recently completed Lakeside development to the west of this project and helping create a new urban gateway along West Morse Boulevard to Downtown Winter Park.*

n. Restrictions proposed to ensure compliance with code limitations on single retail tenant use, as provided in subsection C.

- *The project complies with the prohibition of single-use retail stores in excess of 65,000 square feet.*

o. Compliance with the minimum distance requirements between PD buildings and other projects

- *The project complies with the minimum 75-foot separation between any two principal PD project buildings.*

p. Mitigation of any other transportation issues

- *The Traffic Report, dated August 15, 2014, estimates the development "will generate 1,024 new net daily trips and 91 new net P.M. peak hour trips" and concludes that "the impacted roadways and intersections within the project's one-half mile impact area are currently operating at satisfactory Levels of Service, and will continue to do so at the project buildout in 2015." In addition, the single driveway on US 17-92 and two driveways on Harper Street "will provide adequate access to the proposed uses."*

q. The methods of waste management collection and emergency vehicle access.

- *The Northeast corner of the site will be dedicated to serve all loading, waste, and trash collection for the project. Two full loading berths are provided with direct access to individual, temperature-controlled rooms for retail and residential trash.*
- *Emergency vehicles have direct access through the site from the new curb cut located mid-block, connecting S. Orlando Ave. to Harper Street.*

r. The tools for limiting mass of individual buildings to ensure compatibility with the historic village character of Winter Park such as: separation of large buildings into separate buildings; restricting wall heights at side yard setbacks; reducing heights along sensitive edges; establishing maximum wall plane lengths that reflect the traditional width of buildings along the street; and alignment of front setbacks.

- *The design of The Luxe Winter Park has incorporated into the architecture regresses in the building façade, both in plan and elevation, to provide a pedestrian-oriented, human-scaled, contextual project. Terraces have been designed into the project on the first residential level, some private, some public, so that a sense of life and vitality at all levels will be achieved. Many undulations in the proposed building envelope will break down the scale of the ground floor to better reflect the historic village character of Winter Park.*

Storm water retention will be achieved in a fully underground manner for this project. All storm water collected on the site will be diverted to a series of below-grade exfiltration vaults, primarily located below the surface parking lot on the South half of the site.

Unicorp National Developments, Inc. is requesting a waiver of relief for the setback requirement along Harper Street for this project. On 2-lane, secondary streets, a 20-foot setback is required at the grade level, and a 25-foot setback is required at each level above. The project proposes a 10-foot setback at grade level, and a 20-foot setback at each level above. The particular 2-lane, secondary road that this façade is abutting is fronted by a public park on the opposite side of the street. The proposed design adheres to the landscape buffer (8 feet) that is called for in the PD-1 zoning requirement. The proposed 10-foot setback will be adjacent to the façade of the parking structure. This parking structure façade will be designed to the same architectural standards as the primary retail facades on the other surrounding streets.

Please do not hesitate to contact me if you require any additional information.

Respectfully Submitted,

ANTUNOVICH ASSOCIATES, INC.

Joseph M. Antunovich, AIA

Distribution: Dori Stone, City of Winter Park, Florida
Chuck Whittall, Unicorp National Developments, Inc.
Amy Schuemann, Unicorp National Developments, Inc.
Art Wellington, Unicorp National Developments, Inc.
Daina Rodak, Unicorp National Developments, Inc.
Kevin Sperry, Antunovich Associates, Inc.
George Sorich, Antunovich Associates, Inc.



October 23, 2014

Dori Stone
Director of Economic Development/CRA
City of Winter Park
401 S. Park Avenue
Winter Park, Florida 32789

Re: The Luxe, Winter Park
Conceptual Architectural Review

Dear Dori,

Please find attached ACi's conceptual and preliminary Architectural Review for your use.

We thank you for this opportunity to objectively review what could potentially be a great addition to the City of Winter Park.

Please do not hesitate to call us to discuss.

Sincerely,

John A. Cunningham, AIA
Design Partner, Associated Consulting International, Inc. (ACi)



The Luxe, Winter Park Conceptual Review

Overall massing of the buildings makes sense and is well articulated with a strong one story retail base along Morse Boulevard, U.S. 17-92. West New England Avenue and Harper Street sets back responsibly along these streets and above the one story retail base steps back in an articulated and appropriate manner as well.

Of note on the street setbacks, there is a very clear and deliberate creation of well defined usable and defensible space for outdoor dining and retail, much like one would find on any great street with a boulevard around the world. The park spaces at Morse Boulevard/Harper Street and West New England/Harper Street are a welcome surprise.

With regard to the architecture, the overall massing and composition of elements is appropriate and would be a great asset as a gateway to Park Avenue along Morse Boulevard. The three story façade along Harper with the step back of remaining floors will properly define the edge of the Martin Luther King Park and put "eyes on the street". As to the loading dock on Harper Street, the aesthetic of the garage doors should be a minimum of that which they show and should be closed at all times other than deliveries and trash removal.

The architectural style seems to be somewhat unresolved at this point, there is an attempt to contemporize/streamline Mediterranean style architecture. I caution this as it could look to sterile and uninviting along a street (17-92) that is already challenged.

Should this project move forward we recommend that great attention to the scale proportion of detailing of project elements, color, texture and type of materials must be adhered to.

Examples of elements, details, scale, proportion, color, texture and materials to pay closer attention to:

- Articulation of the 1, 2, 3 story base of buildings
- Arched openings
- Cornices
- Entablatures
- Railings
- Columns/Pilasters
- Building lighting
- Trellis's
- Window Trim (headers, sills)
- Location, type of materials, color of signage and way finding signage
- Internal roof drains versus scuppers with leader boxes and downspouts
- Awning design, size color creation

See attached Architectural Patterns for relevant examples.

Additional recommendations:

- Larger Trees along US 17-92 with raised planters to not only further define the outdoor spaces, but to provide defensible space due to fast moving cars.
- Screen Blank north wall of loading area with mature landscape.
- Height of trellis's along US 17-92 seem to be in conflict with the arched window openings (resolve).
- Tile roof element over one story retail corner elements (over Shinsen and Sebastiens) seem to be unresolved, in that the element on the ground floor is curved and the tile roofed element is straight.
- Special attention should be paid to the landscape uplighting.



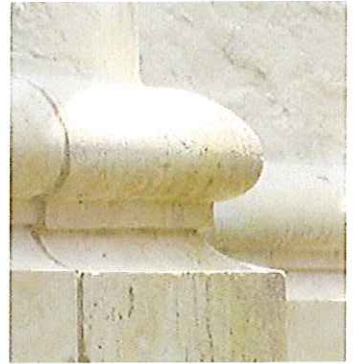
- The two passive parks have the opportunity to become true “places” if they are designed with walkways, benches, lighting, fountains and in the case of the corner of Harper and W. New England, possibly a fun element like an interactive pop jet park for children and therefore a destination.

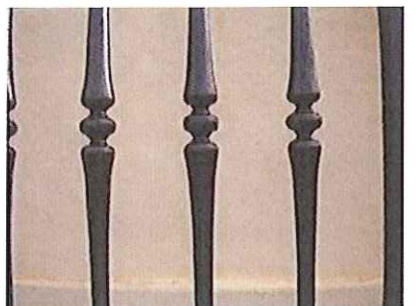
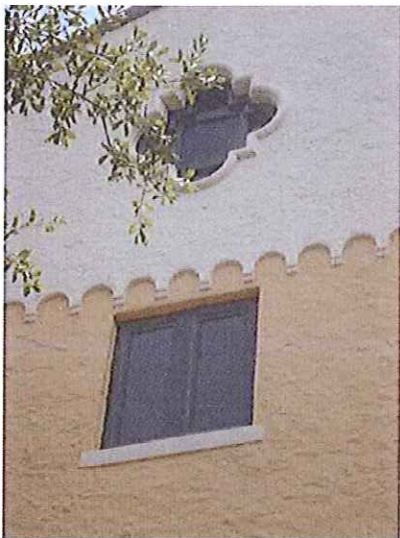
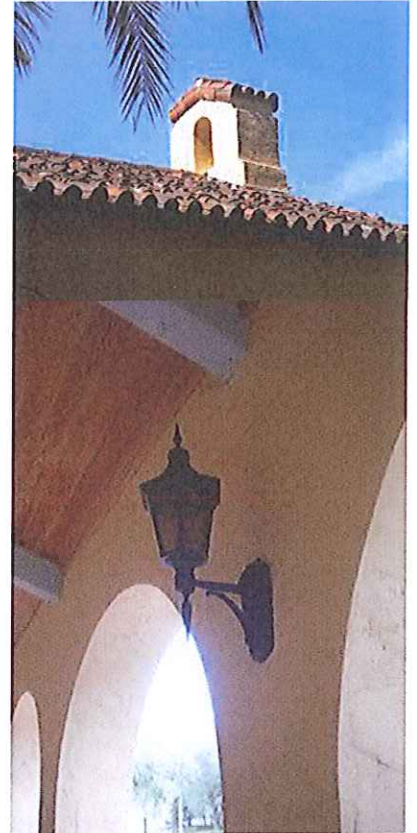
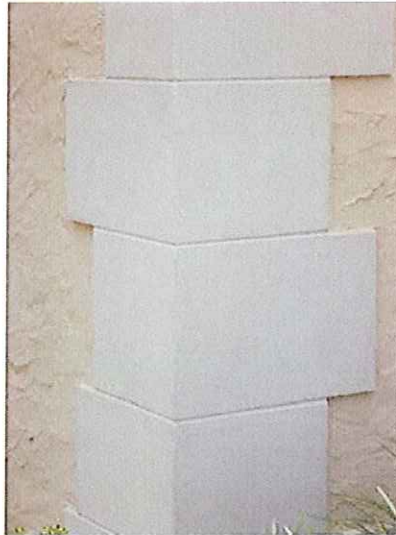
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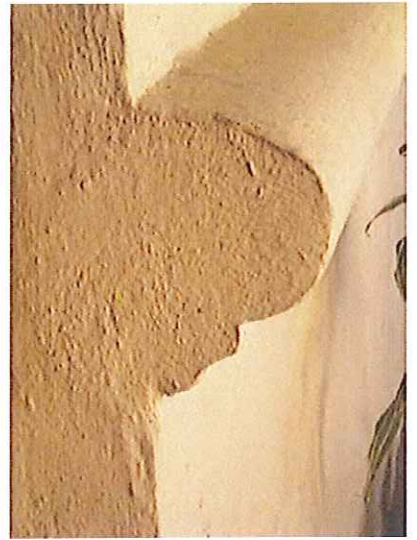
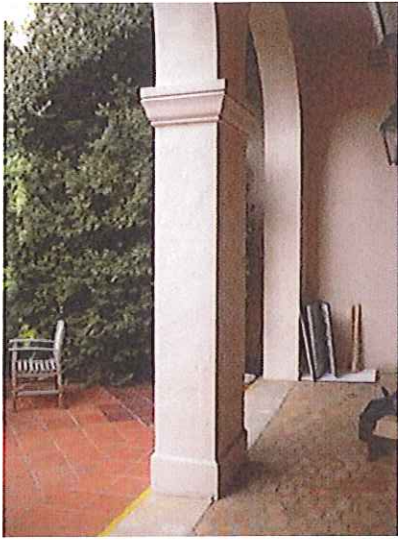
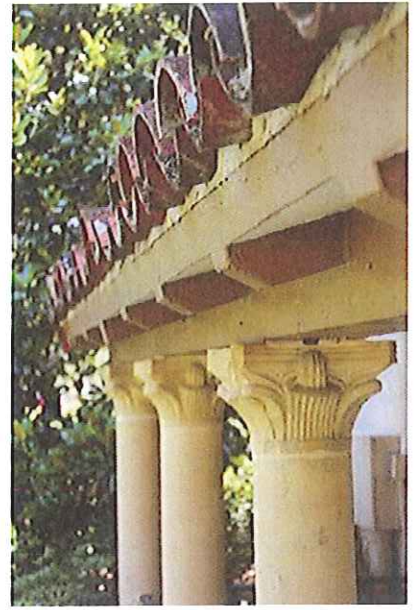
As the gateway to Park Avenue, this development is the “Billboard” of Downtown and must express its architecture in a rich, mindful, purposeful way as Winter Park’s long artistic heritage exemplifies, demands and the community deserves.

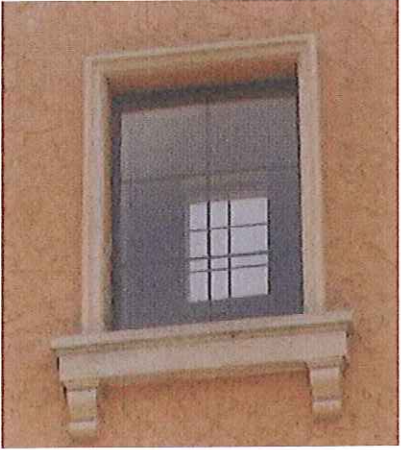
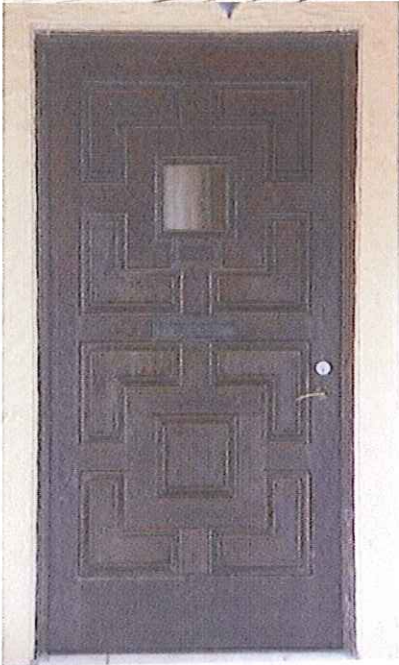
Architectural Patterns











October 28, 2014

Ms. Dori Stone
Director, Planning & Community Development
City of Winter Park
401 Park Avenue South
Winter Park, Florida 32789

Subject: The Luxe Proposed Development

Dear Ms. Stone:

Comprehensive Engineering Services, Inc. (CES) has reviewed the Traffic Impact Analysis and preliminary site plan submitted for the Luxe which is the proposed redevelopment of the Mt. Vernon Inn site. Please note that this review was limited and only looked at potential items the City may want to consider requesting from the developer as part of the final design. The following comments were sent to Traffic Planning and Design (TPD) for the report:


1. The Highway Capacity Analysis (HCS) model results for the intersection of US 17/92 and Fairbanks Ave. appear to indicate that protected plus permissive lefts were allowed on all approaches. As noted in the existing conditions all lefts are protected only. TPD provided a corrected HCS analysis on October 27th, 2014.
2. The HCS results for the Morse Boulevard intersection appear to indicate that the simultaneous movements from the east and west allow protected left turn movements in conflict. TPD provided a corrected HCS analysis on October 27th, 2014.

The intersection of US 17/92 and Fairbanks Avenue is now at level of service (LOS) E in the existing and proposed condition. The impacts of the Luxe development should be minimal. You may see the eastbound left noticeably more over capacity. Consideration should be given to removal of the kick-out of the raised separator in front of the McDonalds. There are some instances in the existing situation that I have observed where the eastbound left queue from Fairbanks extends into the through lane blocking through movements due to the raised separator. This is supported in the HCS analysis which shows this movement to have the highest delay at the intersection in the PM peak and failing at level of service F. This modification would require approval by the FDOT. There may be other reasons for the decision to construct the traffic separator in the present form that we are not aware of.

The signal design at Morse Boulevard should consider future signal head loadings and placements associated with left turn 4-section heads for each approach for such time as the phases are warranted. I am assuming you were also requiring a mast arm signal upgrade for the intersection as well. In addition, the main driveway on US 17/92 should be restricted to right-in/right-out due to the proximity of the driveway to Morse Boulevard and the amount of congestion along the corridor. They will have access to Morse Blvd for the return movements to the south. This possibly would be a requirement by FDOT anyway and FDOT most likely would not object. As we discussed, the City may want to consider requesting that the Luxe developer have a discussion with the property to the south about allowing cross access between the sites.

A revised Traffic Impact Analysis with the corrected HCS analysis should also be required. The analysis should also include changes to the trip distribution if the right-in/right-out configuration is used for the main driveway. Let me know if you have any questions.

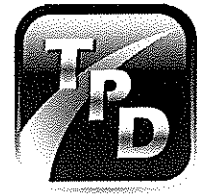
Comprehensive Engineering Services, Inc.



Christopher A. Simoneaux, P.E.

TRAFFIC IMPACT ANALYSIS

THE LUXE
WINTER PARK, FLORIDA



Prepared for:

Unicorp National Developments, Inc.
7940 Via Dellagio Way, Suite 200
Orlando, Florida 32819

Prepared by:

Traffic Planning and Design, Inc.
535 Versailles Drive
Maitland, Florida 32751
407-628-9955

Revised
October 2014

TPD № 4566

PROFESSIONAL ENGINEERING CERTIFICATION

I hereby certify that I am a Professional Engineer properly registered in the State of Florida practicing with Traffic Planning & Design, Inc., a corporation authorized to operate as an engineering business, EB-3702, by the State of Florida Department of Professional Regulation, Board of Professional Engineers, and that I have prepared or approved the evaluations, findings, opinions, conclusions, or technical advice attached hereto for:

PROJECT: The Luxe
LOCATION: Winter Park, Florida
CLIENT: Unicorp National Development, Inc.

I hereby acknowledge that the procedures and references used to develop the results contained in these computations are standard to the professional practice of Transportation Engineering as applied through professional judgment and experience.

NAME: Turgut Dervish

P.E. No.: 20400

DATE: October 27, 2014

SIGNATURE:

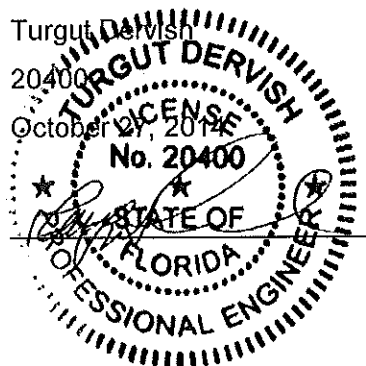


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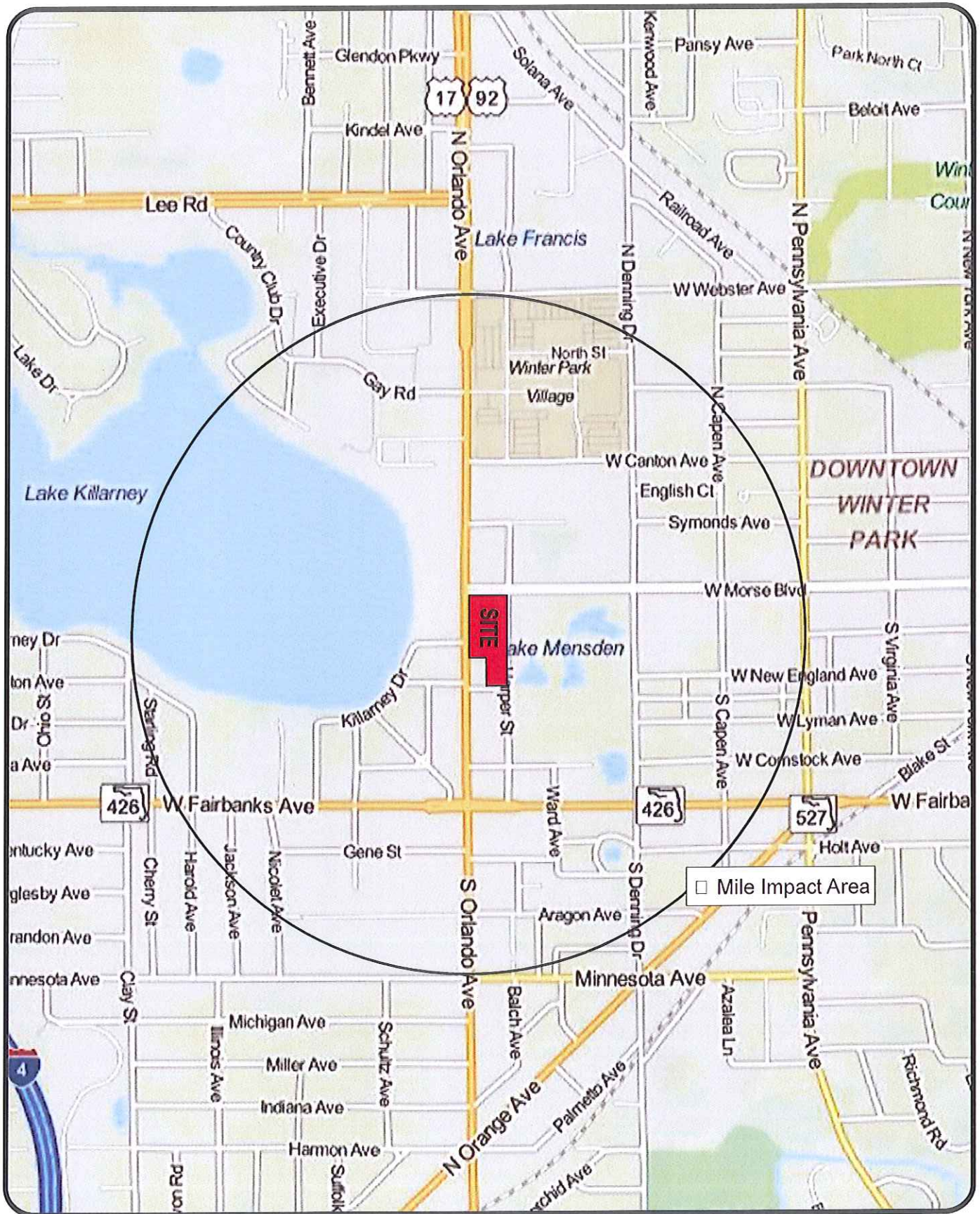
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INTRODUCTION

This analysis was conducted in order to assess the traffic impact of the proposed redevelopment of the existing Mt. Vernon Inn site in Winter Park, Florida. Located in the southeast corner of US 17-92 and Morse Boulevard, the Mt. Vernon Inn is a hotel with 143 rooms. This hotel will be demolished to make room for the proposed redevelopment. Referred to as The Luxe, the new development will consist of 23,500 square feet in three restaurants and 7,740 square feet in retail stores/shops. An alternative plan under consideration will add 53 apartment units to the restaurant and retail store/shops to be constructed on the upper floors. For a conservative assessment of traffic conditions, the apartment units will be included in the traffic analysis. **Figure 1** depicts this site location and its one-half mile impact area.

US 17-92 and Morse will serve to provide external access to the site. US 17-92 is a multi-lane highway with two through lanes in each direction plus a center two-way left turn lane. It carries a daily traffic volume of 29,500 vehicles and has a posted speed limit of 35 mph in this area. Morse Boulevard, which intersects US 17-92 from the east adjacent to the site, is a four-lane divided roadway. It carries a daily traffic volume of approximately 6,000 vehicles and has a speed limit of 30 mph.





The Luxe Winter Park
Project No 4566

Figure 1

Site Location



EXISTING CONDITIONS ANALYSIS

The existing conditions analysis was conducted for roadways and intersections within a one-half mile impact area as required by the City. Capacity analyses were performed for the study roadway segments and intersections for the existing traffic in order to establish their current operating conditions. The roadway segments and intersections were analyzed for P.M. peak hour conditions.

Roadway Segment Analysis

The study roadway segments were analyzed by comparing their existing traffic volumes with their respective capacities at the adopted LOS standard. For US 17-92 and Fairbanks Avenue, the P.M. peak hour traffic volumes were obtained from the 2013 Florida Traffic Information (FTI) DVD and are included in **Appendix A**. For Morse Boulevard, the P.M. peak hour traffic volumes were determined from the intersection counts. The respective capacities of the study roadways were obtained from the *2013 FDOT Quality/Level of Service Handbook*. A summary of the P.M. peak hour roadway capacity analysis is presented in **Table 1**.

Table 1
Existing P.M. Peak Hour Roadway Capacity Analysis

Roadway Segment	No. of Lanes	Adopted		Existing PHPD Volume		Existing LOS
		LOS	Capacity	Direction	Volume	
US 17-92						
Webster Ave to Morse Blvd	4L	E	1,700	NB	1,222	D
Morse Blvd to Fairbanks Ave	4L	E	1,700	NB	1,222	D
Fairbanks Ave to Minnesota Ave	4L	E	1,700	NB	1,192	D
Morse Boulevard						
Us 17-92 TO Denning Dr	4L	D	1,467	EB	252	C
Denning Dr to Pennsylvania Ave	4L	D	1,467	EB	294	C
Fairbanks Avenue						
US 17-92 to Clay Street	4L	E	1,700	EB	1,272	D
US 17-92 to Pennsylvania Ave	4L	E	1,700	EB	1,092	D

The existing conditions analysis reveals that the study segments currently operate at adequate LOS.



Intersection Analysis

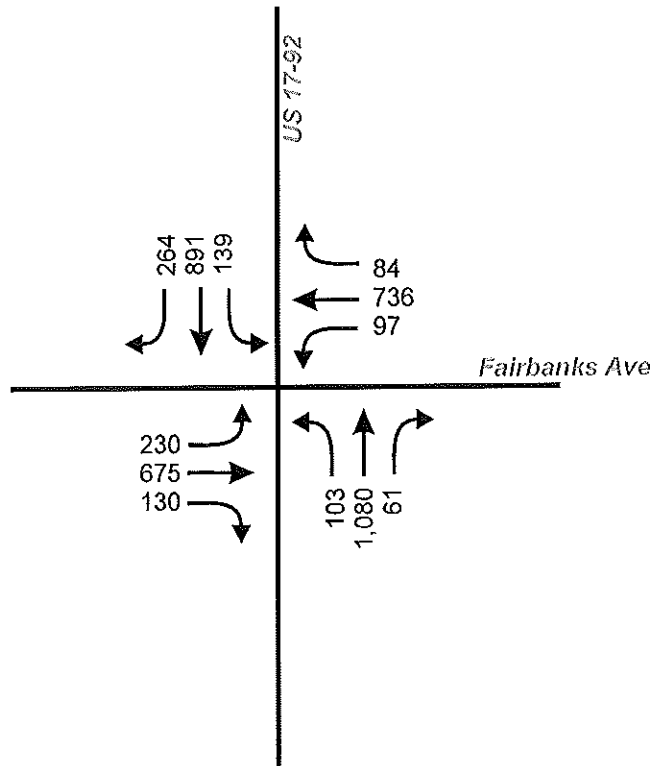
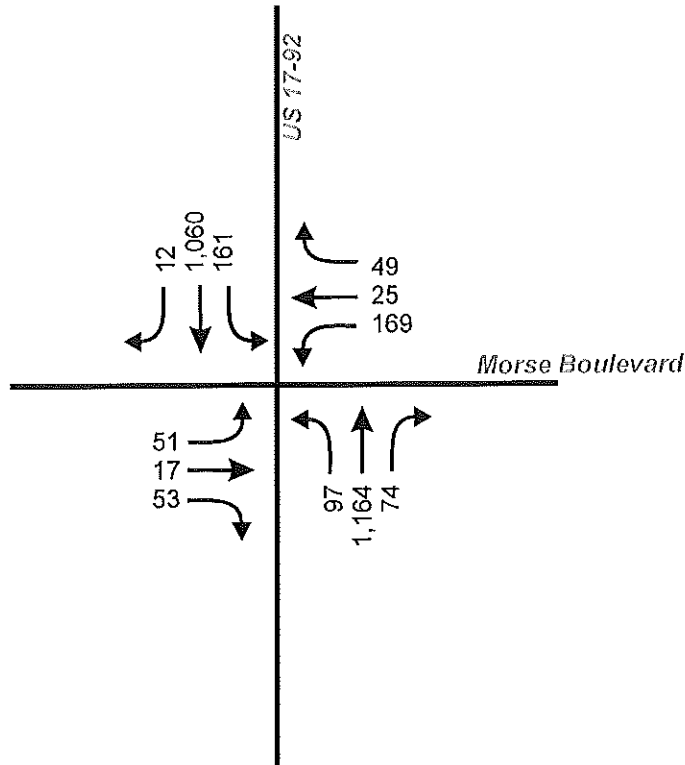
A capacity analysis was conducted for each study intersection using the *Highway Capacity Software (HCS)* in accordance with the procedures of the 2010 *Highway Capacity Manual (HCM)*. The capacity analysis was performed using the existing intersection geometries, traffic volumes during the P.M. peak hour and signal timing/phasing data. Turning movement counts and signal timings were obtained by TPD and are included in **Appendix B**. The existing intersection volumes are displayed in **Figure 2**, and the intersection capacity analysis is summarized in **Table 2**.

Table 2
Existing P.M. Peak Hour Intersection Capacity Analysis

Intersection	Control	EB		WB		NB		SB		Overall	
		Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS
US 17-92 & Morse Boulevard	Signal	22.9	C	25.5	C	16.4	B	7.9	A	13.9	B
US 17-92 & Fairbanks Avenue	Signal	58.0	E	65.7	E	61.0	E	43.9	D	56.4	E

This analysis indicates that the study intersection approaches currently operate at LOS E or better. Detailed HCS worksheets of the existing intersection analysis are included in **Appendix C**.





The Luxe Winter Park
 Project № 4566
 Figure 2

**Existing P.M. Peak Hour
 Turning Movements**



PROPOSED DEVELOPMENT AND TRIP GENERATION

The proposed development is a mixed-use project consisting of restaurant, retail and residential uses. Figure is the first floor plan which depicts the access and parking configuration as well as the proposed restaurant/retail uses. The apartment units will be on second, third and fourth floors. Access to the site will be provided by one driveway on US 17-92 and two driveways on Harper Street which intersects Morse Boulevard adjacent to the site. To determine the impact of the proposed development, an analysis of its trip generation characteristics was made. This included the estimation of the trips to be generated and their distribution/assignment in the area.

Trip Generation

The trip generation of the proposed development was calculated utilizing data from the 9th Edition of the ITE Trip Generation Manual. This calculation is summarized in Table which also includes the trip generation of the existing hotel currently occupying the site. The restaurant and retail uses generate a portion of their trips from the existing traffic shown on the adjacent roadways. The pass-by trip capture rates for these uses as shown in the table were taken from the 2nd Edition of ITE Trip Generation Handbook. The redevelopment of the Mt. Vernon Inn Site as proposed will result in 1,024 new net daily trips and 91 new net P.M. peak hour trips.

**Table
Trip Generation Calculation**

Land Use	ITE Code	Quantity	Daily Trips		P.M. Peak Hour Generation				
			Rate*	Trips	Rate*	Enter	Exit	Total	
Proposed Land Use									
Restaurant	932	23.5 □SF□	127.15/R	2,988	9.85/R	139	93	232	
Retail Commercial	826	7.74 □SF	47.67/E	369	5.17/E	18	22	40	
Apartment	220	53 DU	6.65/R	353	0.62/R	21	12	33	
Total			----	3,710	----	178	127	305	
Restaurant Pass-by Trips (43□)			----	1,285	----	60	40	100	
Retail Pass-by Trips (34□)			----	125	----	6	8	14	
New Net Trips			----	2,300	----	112	79	191	
Existing Land Use									
Hotel	310	143 Rooms	8.92	1,276	0.70	49	51	100	
Trip Increase Due to Redevelopment					1,024		6	2	91

□ R □ Average Rate, E □ Equation
 □ SF □ 1,000 Square Feet



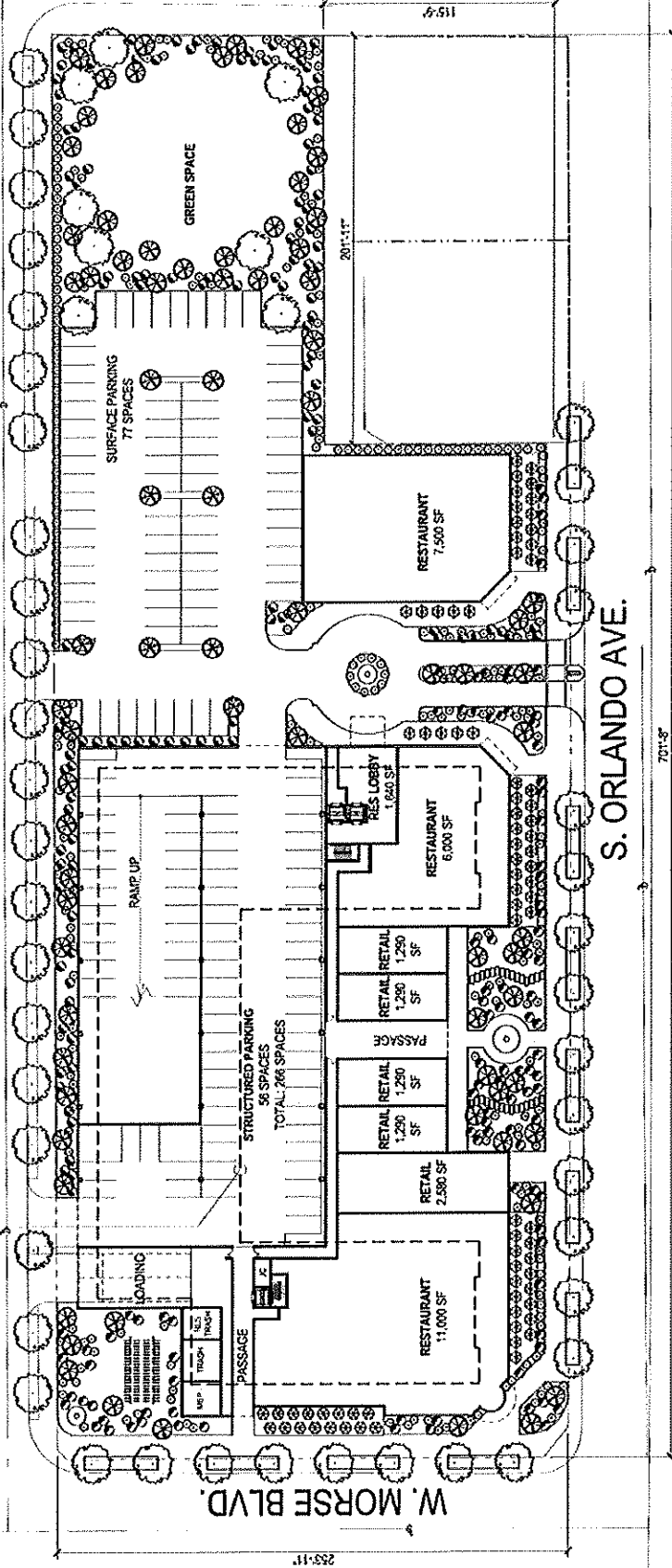
RESIDENTIAL ABOVE
 FLOOR 2: 7 UNITS
 FLOOR 3: 23 UNITS
 FLOOR 4: 23 UNITS
 1,560 GSF UNITS
 53 UNITS TOTAL

HARPER ST.

W. NEW ENGLAND AVE.

W. MORSE BLVD.

S. ORLANDO AVE.



Fisrt Floor Site Plan

The Luxe Winter Park
 Project № 4566
 Figure

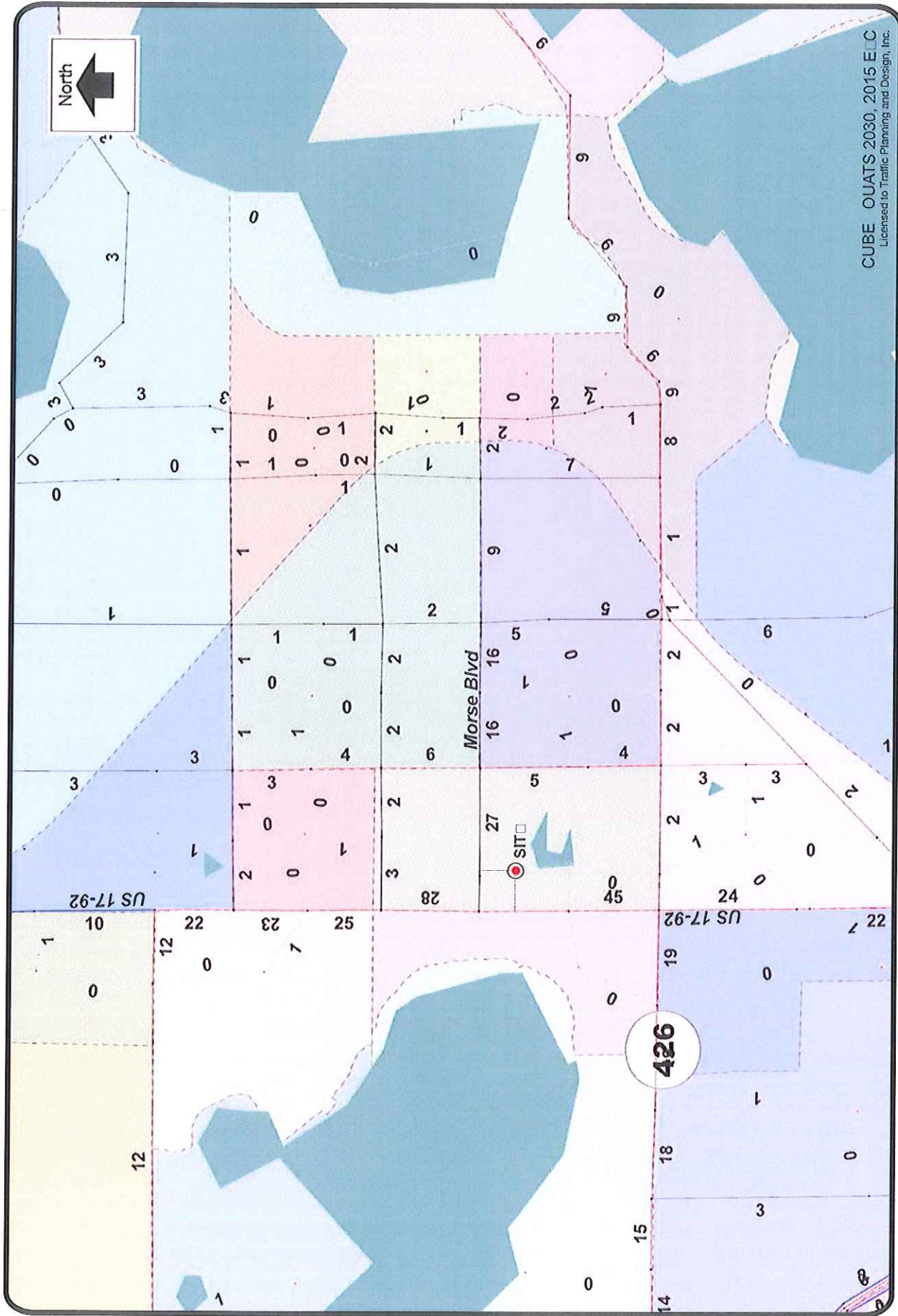


Trip Distribution/Assignment

The distribution of the project's new net trips in the area was determined with the use of the Orlando Urban Area Transportation Study (OUATS) model. Prior to the use of this model, a minor modification was made to add a traffic analysis zone (TAZ) representing the development. Subsequently, the model was run with a select zone analysis to separate the project trips and determine a trip distribution pattern as shown in **Figure 4**. The distribution of the project's pass-by trips will be based upon existing traffic flows on adjacent roadways during the P.M. peak hour.

Utilizing the above trip distribution pattern, the project's new net trips were assigned to area roadways as shown in **Figure 5**. This figure shows the project's daily and P.M. peak hour trips within one-half mile impact area as required by the City.



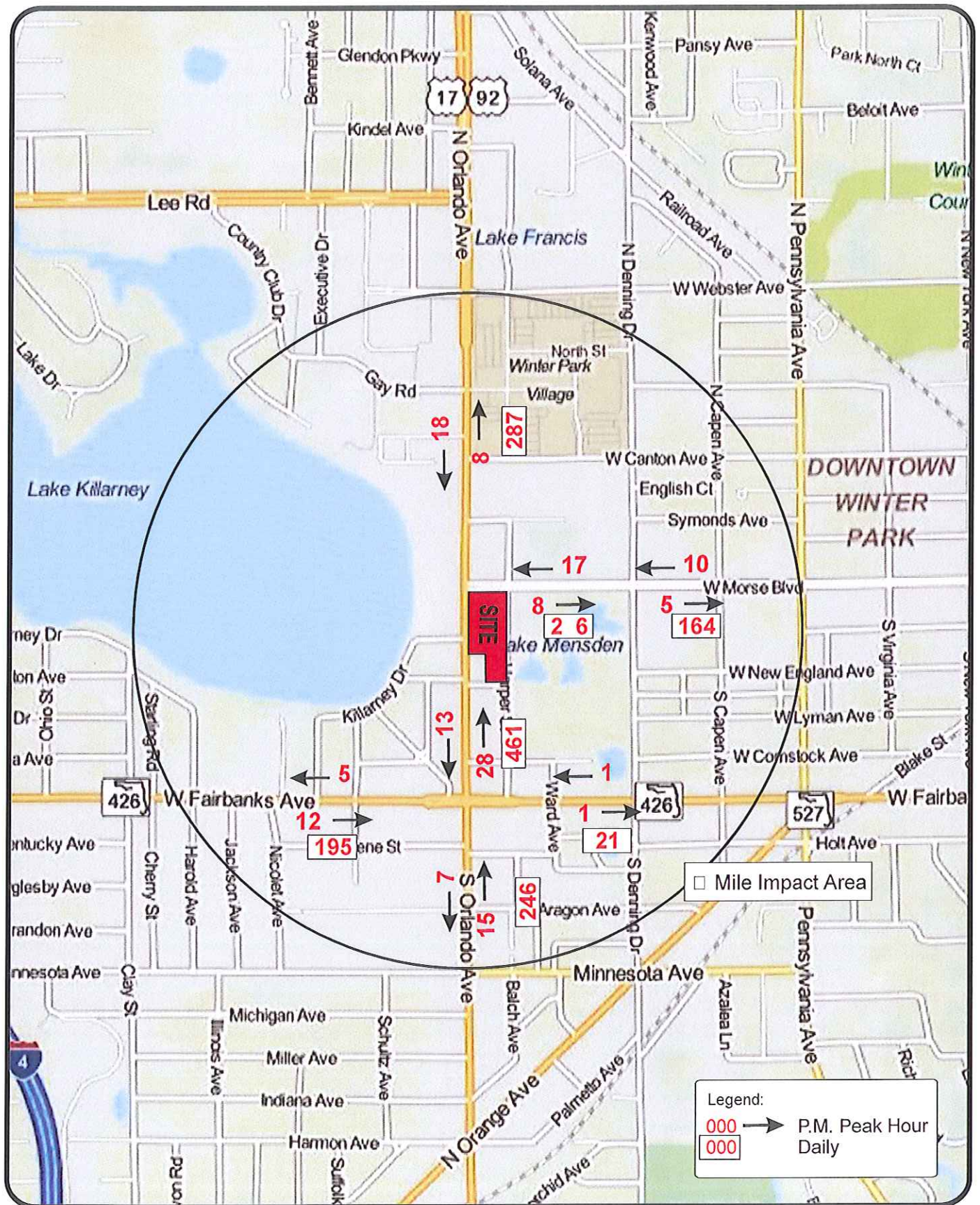


The Luxe Winter Park
 Project No 4566
 Figure 4



Project Trip Distribution

CUBE OUATS 2030, 2015 EOC
 Licensed to Traffic Planning and Design, Inc.



The Luxe Winter Park
Project No 4566

Figure 5

Project Trip Assignment



PROJECTED TRAFFIC CONDITIONS

Projected traffic conditions for the project buildout in 2015 were analyzed using P.M. peak hour traffic volumes for the roadway segments and intersections similar to the existing conditions analysis. The projected traffic volumes used in the analysis consisted of background traffic volumes, the estimated trips of approved but not built Lakeside project and project trips. Background traffic volumes were estimated with the use of an annual growth of 2% or a factor of 1.02 for one-year growth. A review of the historical traffic volumes on US 17-92 and Fairbanks Avenue revealed an annual growth ranging from -1.72% to 0.56% during the last 5-6 years. Therefore, a minimum annual growth of 2% was used in the background traffic estimation. The trends analysis sheets are included in Appendix D.

Roadway Segment Analysis

A roadway segment analysis was performed for the study roadway segments by comparing the projected traffic volumes of the segments with their respective capacities at the adopted LOS standards. The peak hour analysis is summarized in Table 4. The results of the analysis show that the study roadway segments will continue to operate at adequate LOS in the projected conditions.

**Table 4
Projected P.M. Peak Hour Roadway Capacity Analysis**

Roadway Segment	No. of Lns	Adopted		P.M. Peak Hour Peak Background Traffic		Lakeside Trips	Project Trips	Total Volume	Projected LOS
		LOS	Capacity	Direction	Volume				
US 17-92									
Webster Ave to Morse Blvd	4L	E	1,700	NB	1,246	18	8	1,272	D
Morse Blvd to Fairbanks Ave	4L	E	1,700	NB	1,246	28	28	1,302	D
Fairbanks Ave to Minnesota Ave	4L	E	1,700	NB	1,216	15	15	1,246	D
Morse Blvd									
US 17-92 to Denning Dr	4L	D	1,467	EB	257	18	8	283	C
Denning Dr to Pennsylvania Ave	4L	D	1,467	EB	300	10	5	315	C
Fairbanks Ave									
US 17-92 to Clay St	4L	E	1,700	EB	1,297	12	12	1,321	D
US 17-92 to Pennsylvania Ave	4L	E	1,700	EB	1,114	1	1	1,116	D

Existing Volume x 1.02

See Appendix E for Lakeside trip estimation. Lakeside trips assigned to roadways/intersections similar to The Luxe trips.



Intersection Analysis

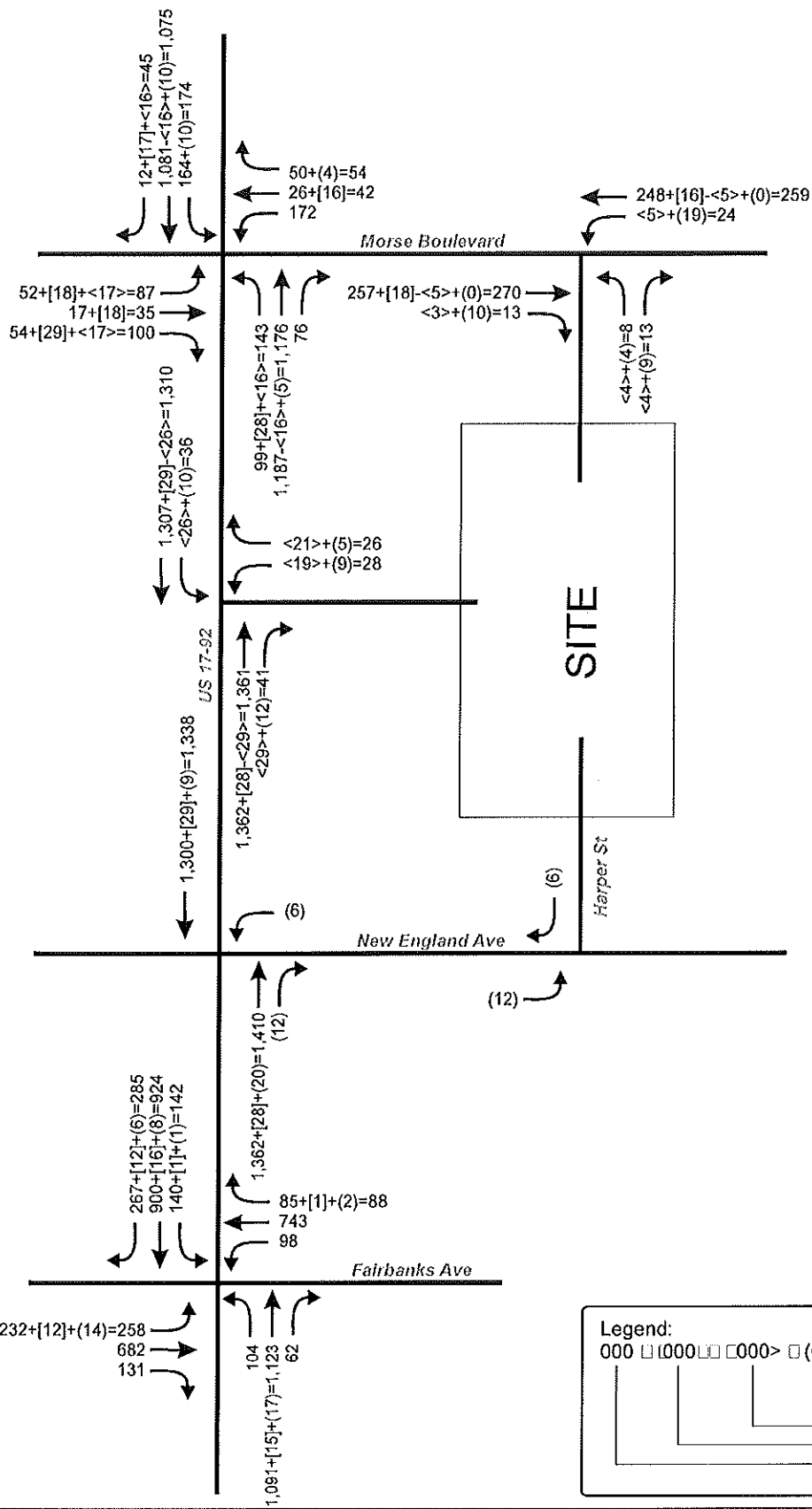
To assess the projected operating conditions at the study intersections, intersection capacity analyses were conducted using projected P.M. peak hour traffic volumes. The intersections were analyzed using the *Highway Capacity Software (HCS)* in accordance with the procedures of the *2010 Highway Capacity Manual*. Projected peak hour volumes were similarly calculated by applying a 1.02 growth factor to existing volumes and adding the approved/not built Lakeside trips plus project trips. **Figure 6** shows the projected peak hour intersection turning volumes for the study intersections. The projected Levels of Service are summarized in **Table 5**.

The analysis shows that the study intersection approaches will continue to operate at LOS E or better, similar to existing conditions in the projected conditions. Detailed printouts of each intersection capacity analysis are included in **Appendix F**.

**Table 5
Projected P.M. Peak Hour Intersection Capacity Analysis**

Intersection	Control	EB		WB		NB		SB		Overall	
		Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay	LOS
US 17-92 & Morse Boulevard	Signal	25.0	C	28.2	C	18.9	B	10.0	B	16.5	B
US 17-92 & Fairbanks Avenue	Signal	73.4	E	70.7	E	65.9	E	45.2	D	62.6	E





The Luxe Winter Park
Project No 4566

Figure 6

Projected P.M. Peak Hour Turning Movements



STUDY CONCLUSIONS

This analysis was conducted in order to assess the traffic impact of the proposed redevelopment of the Mt. Vernon Inn site located in the southeast corner of US 17-92 and Morse Boulevard in Winter Park, Florida. Mt. Vernon Inn is a 143-room hotel which will be demolished to make room for The Luxe, a mixed-use development consisting of 23,500 square feet in restaurant and 7,740 square feet in retail stores/shops, and 53 apartment units. The results of the analysis as documented herein are as follows:

- The proposed redevelopment of the Mt. Vernon Inn site will generate 1,024 new net daily trips and 91 new net P.M. peak hour trips to be added to the area roadways.
- The additional trips to be generated by the proposed uses were distributed and assigned to the area roadways within a one-half mile impact area. The maximum increase in the daily traffic volumes of the state arterial roadways in the area will be slightly less than 2% on US 17-92 adjacent to the site.
- The impacted roadways and intersections within the project's one-half mile impact area are currently operating at Levels of Service E or better and the same Levels of Service will prevail at the project buildout in 2015. This is due to the relatively low traffic volumes generated by the proposed development on the area roadways and intersections.
- The proposed redevelopment is proposed to be served by one driveway on US 17-92 and two driveways on Harper Street. These driveways will provide adequate access to the proposed uses.



APPENDICES

APPENDIX A

FDOT Hourly Traffic Counts

COUNTY: 75
 STATION: 5061
 DESCRIPTION: ON US-17/92, 0.446 MI. S OF SR-423 (UC)
 START DATE: 02/12/2013
 START TIME: 0345

TIME	DIRECTION: N				DIRECTION: S				COMBINED TOTAL		
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD		4TH	TOTAL
0000	32	32	17	24	105	49	36	20	17	122	
0100	16	19	16	13	64	14	13	12	7	46	
0200	7	15	17	14	53	14	7	8	10	39	
0300	9	7	6	8	30	7	2	4	4	17	
0400	12	9	6	8	35	7	15	6	12	40	
0500	10	11	9	12	42	8	19	30	31	88	
0600	24	40	44	60	168	60	55	96	144	355	
0700	76	105	151	158	490	139	163	228	216	746	
0800	193	225	198	230	846	250	251	260	282	1043	
0900	223	234	219	187	863	229	218	258	214	919	
1000	219	207	176	178	780	217	218	202	201	838	
1100	201	249	234	247	931	240	241	232	239	952	
1200	252	289	260	286	1087	254	270	294	247	1065	
1300	268	270	276	248	1062	281	267	254	224	1026	
1400	268	265	255	234	1022	243	266	244	252	1005	
1500	247	243	251	299	1040	218	239	269	256	982	
1600	264	302	257	282	1105	274	268	281	247	1070	
1700	252	272	288	324	1136	249	295	273	271	1088	
1800	285	325	283	272	1165	264	276	267	233	1040	
1900	252	280	227	227	986	251	224	204	132	811	
2000	178	165	152	121	616	170	150	158	144	622	
2100	134	127	116	106	483	139	128	119	118	504	
2200	85	89	62	55	291	95	97	92	73	357	
2300	60	43	37	22	162	67	36	54	71	228	
24-HOUR TOTALS:					14562					15003	29565

PEAK VOLUME INFORMATION			
DIRECTION: N		DIRECTION: S	
COMBINED HOUR	VOLUME	COMBINED HOUR	VOLUME
A.M.	845	800	1043
P.M.	1730	1715	1103
DAILY	1730	1715	1103
TRUCK PERCENTAGE	5.04	6.85	5.96

CLASSIFICATION SUMMARY DATABASE

DIR	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TOTTRK	TOTVOL
N	52	12124	1652	20	113	130	25	73	110	15	100	32	116	0	0	734	14562
S	74	12325	1576	35	93	155	52	82	125	40	101	88	257	0	0	1028	15003

COUNTY: 75
 STATION: 5058
 DESCRIPTION: ON US-17/92, 0.1 MI. N OF SR-527 (UV)
 START DATE: 01/29/2013
 START TIME: 0000

TIME	DIRECTION: N				DIRECTION: S				COMBINED TOTAL		
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD		4TH	TOTAL
0000	18	9	15	15	57	13	15	14	12	54	
0100	6	6	11	4	27	12	9	8	3	32	
0200	8	10	7	5	30	4	11	7	6	28	
0300	7	6	5	7	25	3	4	3	4	14	
0400	3	2	4	9	18	3	8	9	14	40	
0500	12	7	27	22	68	14	24	32	54	124	
0600	34	27	44	65	170	53	87	118	139	397	
0700	92	110	172	206	580	122	200	237	278	837	
0800	232	209	201	247	889	277	289	271	254	1091	
0900	225	209	182	183	799	311	237	219	201	968	
1000	163	171	150	178	662	184	174	195	188	741	
1100	188	212	224	264	888	201	184	206	191	782	
1200	262	235	233	242	972	221	212	246	221	900	
1300	223	234	232	241	930	236	264	239	234	973	
1400	183	222	210	234	849	240	215	214	231	900	
1500	198	237	256	217	908	228	206	188	224	846	
1600	240	268	240	258	1006	234	209	206	241	890	
1700	249	334	289	316	1188	239	292	270	286	1087	
1800	253	258	224	216	951	239	250	219	158	866	
1900	172	179	134	137	622	174	147	141	130	592	
2000	102	101	86	85	374	148	123	131	125	527	
2100	82	82	66	59	289	108	120	92	88	408	
2200	56	60	41	43	200	73	61	47	43	224	
2300	25	27	19	15	86	27	18	21	23	89	
24-HOUR TOTALS:					12588					13410	25998

PEAK VOLUME INFORMATION

DIRECTION: N		DIRECTION: S		COMBINED DIRECTIONS	
DIR	VOLUME	DIR	VOLUME	HR	VOLUME
A.M.	800		815		2007
P.M.	1715		1700		2279
DAILY	1715		815		1715
					2279

COUNTY: 75
 STATION: 0435
 DESCRIPTION: ON SR-426, 0.2 MI. W OF US-17/92
 START DATE: 09/11/2013
 START TIME: 0900

TIME	DIRECTION: E				DIRECTION: W				COMBINED TOTAL		
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD		4TH	TOTAL
0000	30	28	11	17	86	49	43	28	18	138	
0100	14	18	10	12	54	20	20	9	7	56	
0200	6	9	8	5	28	17	12	8	10	47	
0300	7	12	4	6	29	6	10	8	12	36	
0400	8	17	23	21	69	5	13	18	16	52	
0500	16	32	57	85	190	21	27	38	77	163	
0600	94	139	195	219	647	65	98	129	135	427	
0700	228	277	277	285	1029	206	272	279	277	1034	
0800	302	323	312	299	1236	274	277	308	313	1172	
0900	293	266	290	276	1125	261	242	218	235	956	
1000	235	271	243	266	1015	212	231	215	242	900	
1100	237	251	289	292	1069	229	257	240	257	983	
1200	265	327	250	264	1106	247	282	300	286	1115	
1300	249	265	258	243	1015	281	306	314	282	1183	
1400	244	299	311	259	1113	294	313	324	277	1208	
1500	243	253	261	266	1023	296	247	311	295	1149	
1600	265	256	303	297	1119	319	263	275	285	1142	
1700	306	352	296	297	1251	324	293	303	244	1164	
1800	327	310	315	265	1217	267	241	273	239	1020	
1900	211	229	193	196	829	246	192	162	204	854	
2000	156	141	126	118	541	221	191	166	174	752	
2100	113	105	83	90	391	185	224	174	173	756	
2200	72	68	54	41	235	116	112	86	72	386	
2300	39	26	38	40	143	84	66	53	40	243	
24-HOUR TOTALS:					16560					16936	33496

PEAK VOLUME INFORMATION

DIRECTION: E		DIRECTION: W		COMBINED DIRECTIONS	
hour	volume	hour	volume	hour	volume
A.M.	800	800	1172	800	2408
P.M.	1715	1345	1213	1645	2454
DAILY	1715	1345	1213	1645	2454

TRUCK PERCENTAGE 3.04 2.57 2.80

CLASSIFICATION SUMMARY DATABASE

DIR	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	TOTTRK	TOTVOL
E	161	13783	2081	73	268	40	11	52	48	6	0	0	0	0	0	32	503
W	70	14123	2307	63	233	50	3	40	44	3	0	0	0	0	0	436	16936

COUNTY: 75
 STATION: 5071
 DESCRIPTION: ON SR-426, 0.11 MI. E OF US 17/92 (UV)
 START DATE: 01/29/2013
 START TIME: 0000

TIME	DIRECTION: E				DIRECTION: W				COMBINED TOTAL		
	1ST	2ND	3RD	4TH	TOTAL	1ST	2ND	3RD		4TH	TOTAL
0000	33	14	18	19	84	37	19	24	22	102	
0100	12	12	9	8	41	14	15	11	19	59	
0200	8	11	8	7	34	9	14	7	7	37	
0300	7	18	7	8	40	7	11	9	16	43	
0400	9	11	15	19	54	9	8	18	16	51	
0500	8	26	49	64	147	10	31	43	63	147	
0600	78	120	150	185	533	73	92	116	133	414	
0700	171	169	187	261	788	178	208	269	239	894	
0800	206	245	279	215	945	248	221	253	225	947	
0900	205	237	225	233	900	232	207	222	198	859	
1000	212	210	239	226	887	189	183	205	194	771	
1100	221	222	241	264	948	230	238	218	240	926	
1200	291	242	273	286	1092	217	247	256	245	965	
1300	250	247	272	260	1029	265	256	237	239	997	
1400	240	222	246	261	969	204	246	253	260	963	
1500	228	256	251	274	1009	230	257	288	221	996	
1600	237	244	225	252	958	300	245	253	256	1054	
1700	268	265	272	269	1074	246	261	232	215	954	
1800	250	245	276	219	990	261	209	220	170	860	
1900	205	174	144	134	657	267	178	170	139	754	
2000	144	135	133	115	527	155	155	161	146	617	
2100	128	103	82	104	417	161	200	157	101	619	
2200	83	61	68	54	266	101	108	72	61	342	
2300	39	44	26	33	142	58	71	40	33	202	
24-HOUR TOTALS:					14531					14573	29104

PEAK VOLUME INFORMATION			
DIRECTION: E		DIRECTION: W	
HOURLY	VOLUME	HOURLY	VOLUME
A.M.	745	991	977
P.M.	1200	1092	1066
DAILY	1200	1092	1066
COMBINED DIRECTIONS		HOURLY	VOLUME
		745	1952
		1515	2084
		1515	2084

APPENDIX

Existing Intersection Counts /
Signal Timing Data

(Cars and Trucks)

DATE: August 11, 2014 (Monday)

CITY: Orlando

LATITUDE:

LOCATION: US 17-92 & Morse Blvd

COUNTY: Orange Co

LONGITUDE:

Morse Blvd

Morse Blvd

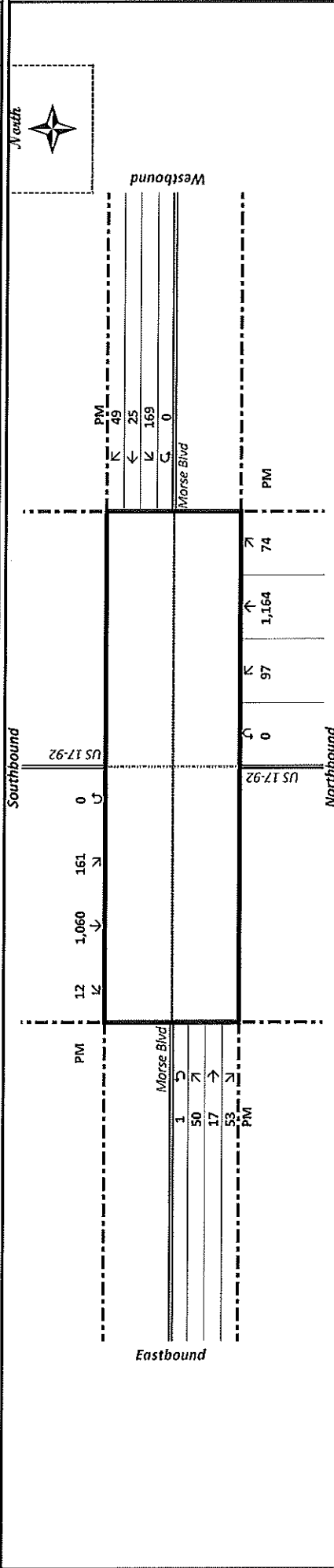
US 17-92

US 17-92

TIME BEGIN	NORTHBOUND			SOUTHBOUND			EASTBOUND			WESTBOUND			E/W TOTAL	GRAND TOTAL									
	L	T	R	L	T	R	L	T	R	L	T	R			L	T	R						
04:00 PM	25	253	17	0	295	43	245	5	1	294	589	12	5	12	0	29	32	11	19	1	63	91	680
04:15 PM	28	275	14	0	317	47	251	4	0	302	619	10	7	21	1	39	42	3	8	0	53	91	710
04:30 PM	28	278	23	0	329	34	259	4	0	297	626	10	1	7	0	18	35	4	6	0	45	63	689
04:45 PM	18	295	17	0	330	42	265	1	0	308	638	11	3	16	0	30	45	5	5	0	55	85	723
TOTAL	99	1,101	71	0	1,271	166	1,020	14	1	1,201	2,472	43	16	56	1	116	154	23	38	1	216	330	2,802
05:00 PM	21	284	26	0	331	34	264	5	0	303	634	15	4	8	0	27	51	6	14	0	71	98	732
05:15 PM	23	289	17	0	329	44	275	5	0	324	653	16	5	18	1	40	48	7	6	0	61	100	753
05:30 PM	35	296	14	0	345	41	256	1	0	298	643	8	5	11	0	24	25	7	24	0	56	80	723
05:45 PM	15	276	11	0	302	48	245	2	0	295	597	25	6	11	0	42	48	4	14	0	66	108	705
TOTAL	94	1,145	68	0	1,307	167	1,040	13	0	1,220	2,527	64	20	48	1	133	172	24	58	0	254	386	2,913

PMI Peak	97	1,164	74	0	1,335	161	1,060	12	0	1,233	2,568	50	17	53	1	121	169	25	49	0	243	363	2,931
04:45 PM to 05:45 PM																							

Peak Hour Factor: 0.973



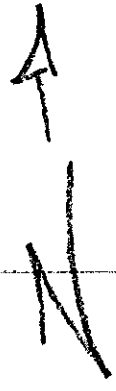
15 MINUTE TURNING MOVEMENT COUNTS
(Trucks Only)

DATE: August 11, 2014 (Monday)
LOCATION: US 17-92 & Morse Blvd

CITY: Orlando
COUNTY: Orange Co

LATITUDE: 00.000000°
LONGITUDE: 00.000000°

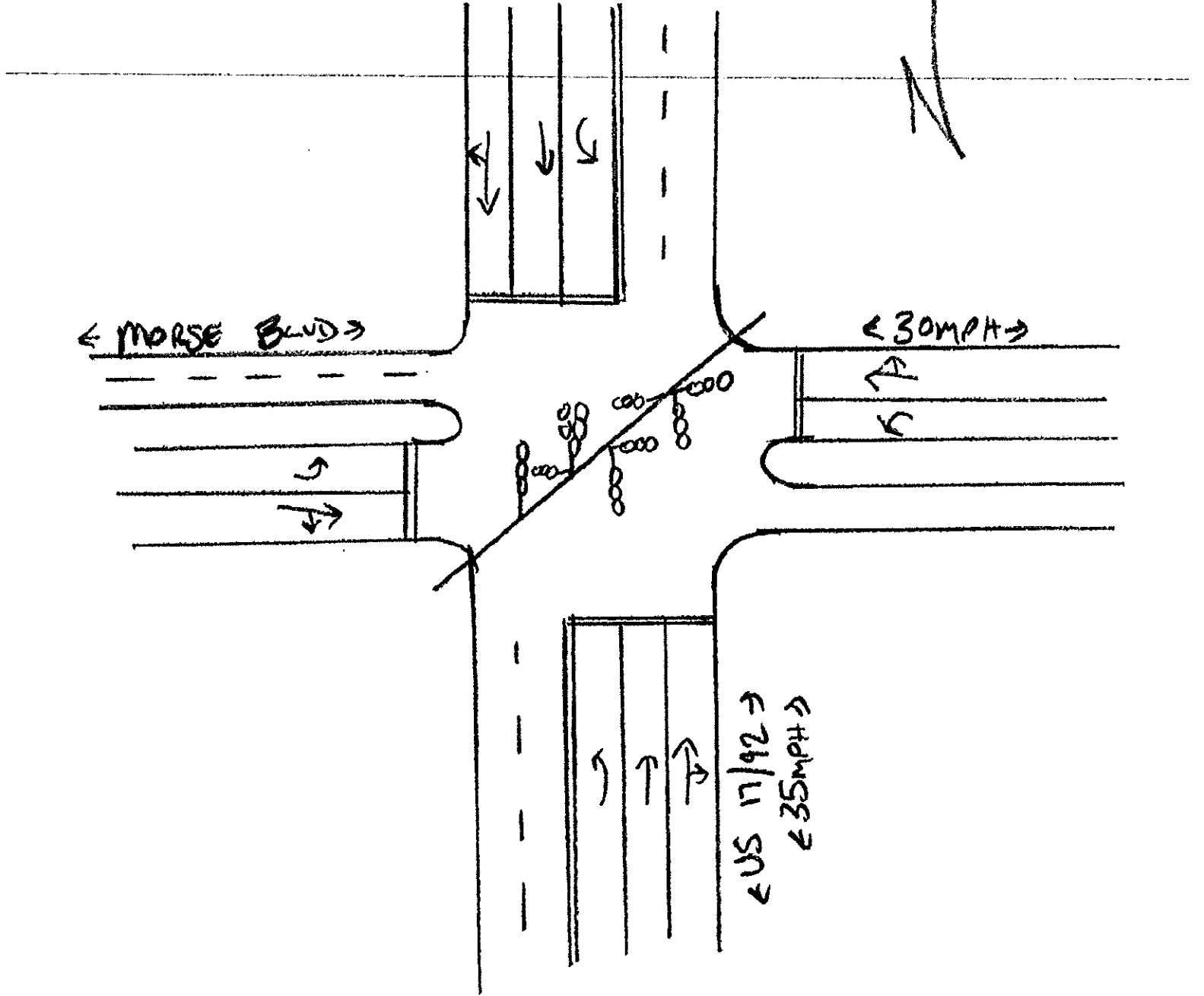
TIME BEGIN	US 17-92					US 17-92					Morse Blvd					Morse Blvd					GRAND TOTAL						
	NORTHBOUND		SOUTHBOUND			N/S		EASTBOUND			WESTBOUND		E/W		TOTAL	TOTAL	TOTAL	TOTAL									
	L	T	R	U-turn	TOTAL	L	T	R	U-turn	TOTAL	L	T	R	U-turn					TOTAL	L		T	R	U-turn	TOTAL		
04:00 PM	0	0	1	0	1	0	0	0	0	0	1	0	0	0	1	0	0	1	0	1	0	0	1	0	1	2	3
04:15 PM	0	0	0	0	0	2	0	0	0	2	3	0	1	0	4	0	0	0	0	0	0	0	0	0	0	1	4
04:30 PM	0	4	0	0	4	1	0	0	0	1	5	0	0	0	5	0	0	0	0	0	0	0	0	0	0	0	5
04:45 PM	0	1	0	0	1	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	1	0	0	0	1	1	2
TOTAL	0	5	1	0	6	2	0	0	0	2	10	1	1	0	2	1	0	1	0	2	1	0	1	0	2	4	14
05:00 PM	0	0	1	0	1	0	0	0	0	0	1	0	0	0	1	0	0	1	0	1	0	0	1	0	1	1	2
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	1	1
05:30 PM	0	2	0	0	2	0	0	0	0	0	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
05:45 PM	0	1	0	0	1	0	1	0	0	1	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2
TOTAL	0	3	1	0	4	0	1	0	0	1	5	0	0	0	5	0	0	1	0	1	1	0	1	0	2	2	7
PM Peak																											
04:45 PM to 05:45 PM																											
0	3	1	0	4	0	0	0	0	0	4	0	0	0	0	0	0	1	0	0	2	0	1	0	3	3	7	



← MORSE BLVD →

← 30MPH →

← US 17/92 →
← 35MPH →



15 MINUTE TURNING MOVEMENT COUNTS

(Cars and Trucks)

DATE: August 11, 2014 (Monday)

CITY: Orlando

LATITUDE: _____

LOCATION: US 17-92 & SR-426

COUNTY: Orange Co

LONGITUDE: _____

US 17-92

US 17-92

SR-426

SR-426

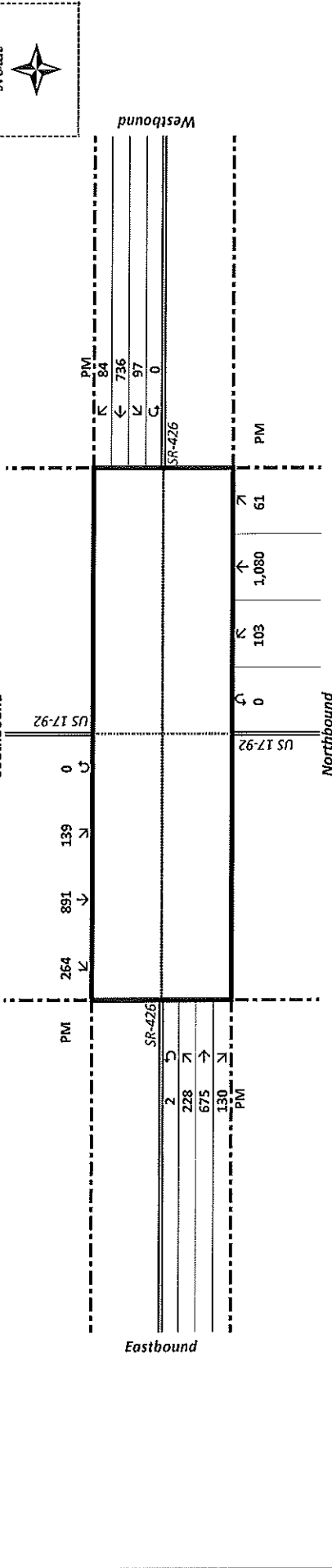
TIME BEGIN	NORTHBOUND			SOUTHBOUND			N/S TOTAL	EASTBOUND			WESTBOUND			E/W TOTAL	GRAND TOTAL								
	L	T	R	L	T	R		L	T	R	L	T	R			L	T	R					
04:00 PM	37	244	15	0	296	40	258	77	0	375	671	45	142	28	0	215	26	147	33	0	206	421	1,092
04:15 PM	24	224	18	0	266	35	183	64	0	282	548	67	156	28	0	251	42	188	35	0	265	516	1,064
04:30 PM	37	288	19	0	344	37	225	66	0	328	672	51	159	27	0	237	27	150	29	0	206	443	1,115
04:45 PM	24	239	16	0	279	31	186	64	0	281	560	63	163	36	0	262	29	190	21	0	240	502	1,062
TOTAL	122	995	68	0	1,185	143	852	271	0	1,266	2,451	226	620	119	0	965	124	675	118	0	917	1,882	4,333
05:00 PM	36	283	19	0	338	34	249	63	0	346	684	49	174	32	0	255	17	154	19	0	190	445	1,129
05:15 PM	21	259	10	0	290	36	214	75	0	325	615	63	188	29	1	281	25	215	17	0	257	537	1,152
05:30 PM	22	299	16	0	337	38	242	62	0	342	679	53	150	33	1	237	26	177	27	0	230	466	1,145
05:45 PM	22	212	6	0	240	29	183	60	0	272	512	57	188	39	1	285	31	168	40	0	239	523	1,035
TOTAL	101	1,053	51	0	1,205	137	888	260	0	1,285	2,490	222	700	133	3	1,058	99	714	103	0	916	1,971	4,461

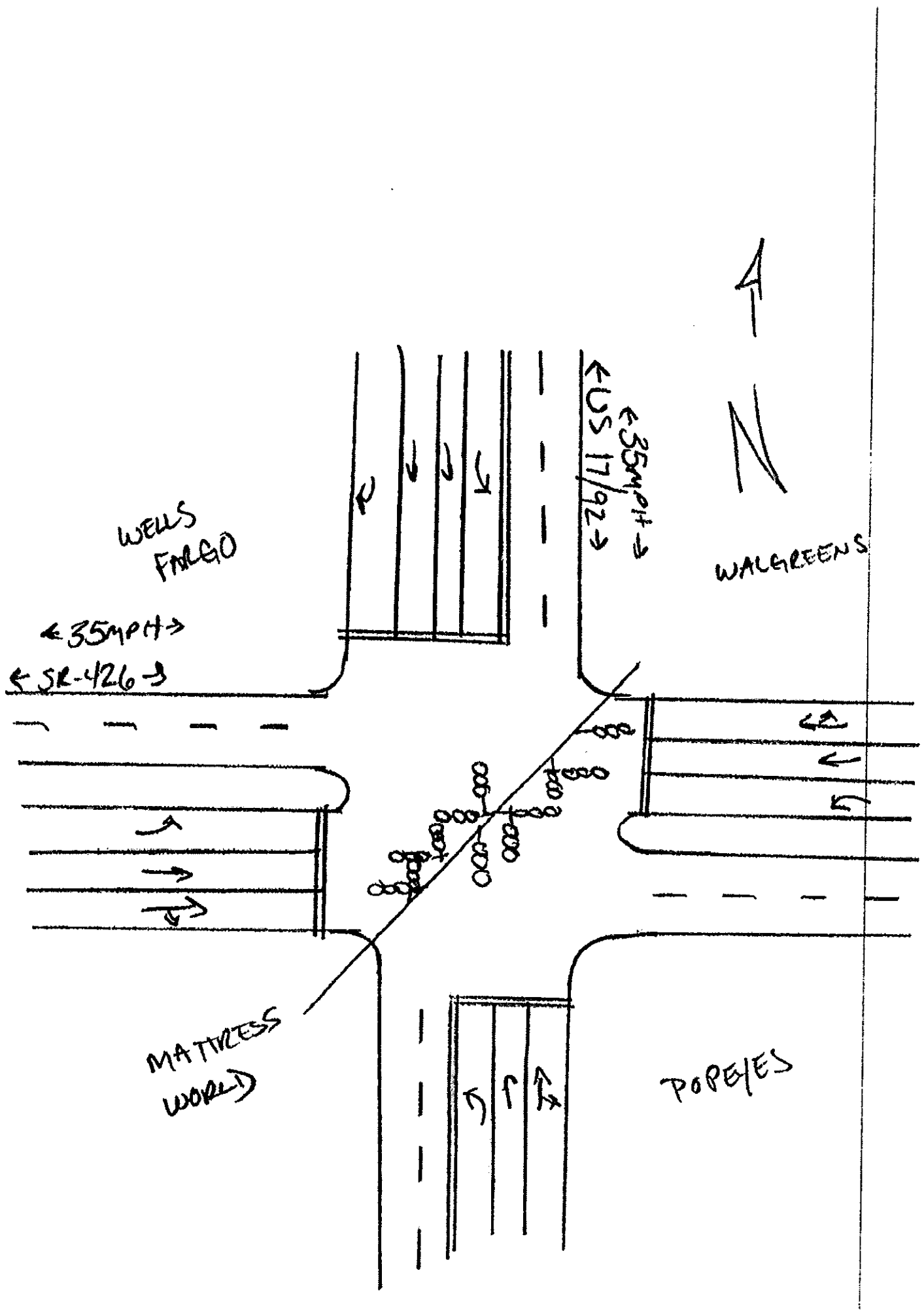
PM Peak
04:45 PM to 05:45 PM

103	1,080	61	0	1,244	139	891	264	0	1,294	2,538	228	675	130	2	1,035	97	736	84	0	917	1,950	4,488
Peak Hour Factor: 0.974															917	1,950	4,488					

Southbound

Northbound





WELLS
FARGO

WALGREENS

MATTRESS
WORLD

POPEYES

← 35MPH →
← US 17/92 →

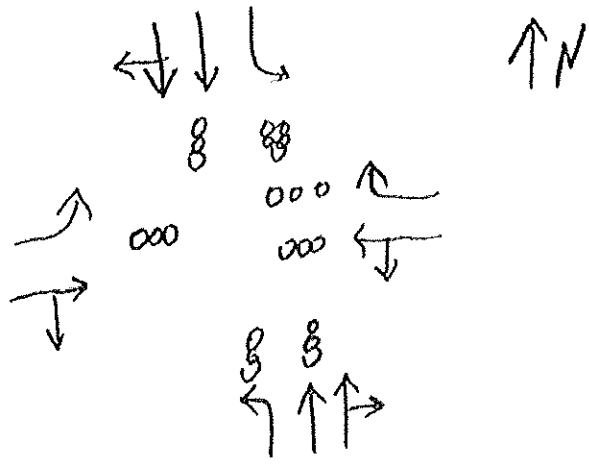
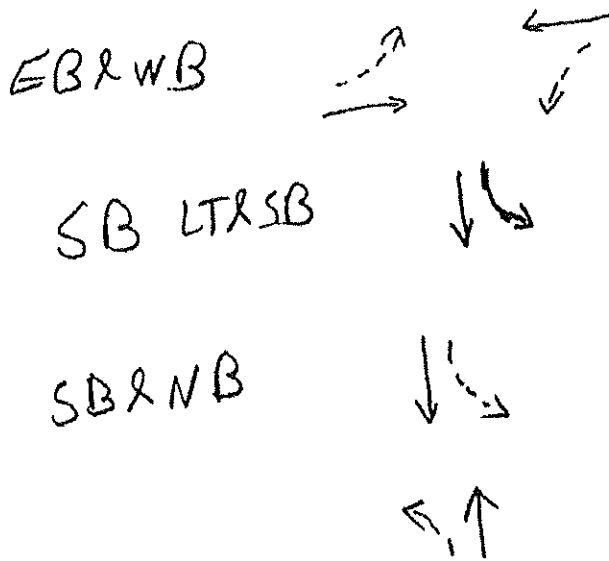
← 35MPH →

← SR-426 →










Us 17/92 & Morse Rd

Cycle	198	198	199	198	199	198	200
NB LT	133	133	134	133	133	132	133
NB TH	133	133	134	133	133	132	133
SB LT	19	20	20	21	20	22	22
SB TH	153	154	153	153	154	154	154
EB LT & RT	31	31	32	30	30	32	33



US 17/92 & Fairbanks Ave

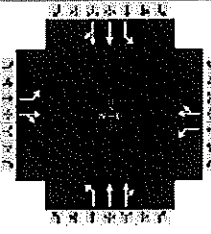
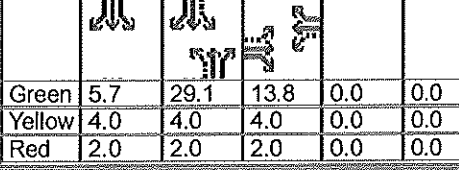
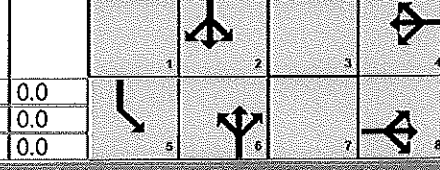

8/13/2014

								7	8	Cycle
		1	2	3	4	5	6			
1		0:23	0:11	0:59	0:22	1:05	0:18			3:18
2		0:24	0:11	1:00	0:30	0:56	0:20			3:21
3		0:23	0:00	1:11	0:25	0:56	0:23			3:18
4		0:23	0:12	1:00	0:27	0:59	0:19			3:20
5		0:24	0:00	1:10	0:26	1:01	0:18			3:19
6		0:24	0:11	1:00	0:22	1:05	0:19			3:21
		0:23	0:07	1:03	0:25	1:00	0:19			3:19

APPENDIX C

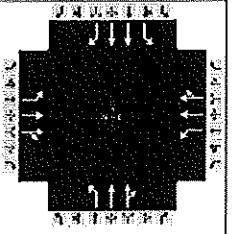
Existing HCS Capacity Analysis

HCS 2010 Signalized Intersection Results Summary

General Information					Intersection Information										
Agency	TPD				Duration, h	0.25									
Analyst	MJA		Analysis Date	Oct 24, 2014		Area Type	Other								
Jurisdiction	Winter Park		Time Period	PM Peak Hour		PHF	0.97								
Intersection	US 17/92 & Morse Boulevard		Analysis Year	2014		Analysis Period	1> 16:45								
File Name	US 17-92 & Morse Boulevard Existing PM Peak.xus														
Project Description	Existing PM Peak Hour														
Demand Information				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h				51	17	53	169	25	49	97	1164	74	161	1060	12
Signal Information															
Cycle, s	66.6	Reference Phase	2												
Offset, s	0	Reference Point	End												
Uncoordinated	Yes	Simult. Gap E/W	On												
Force Mode	Fixed	Simult. Gap N/S	On												
Green	5.7	29.1	13.8	0.0	0.0	0.0									
Yellow	4.0	4.0	4.0	0.0	0.0	0.0									
Red	2.0	2.0	2.0	0.0	0.0	0.0									
Timer Results				EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT				
Assigned Phase					8		4		6	5	2				
Case Number					6.0		6.0		6.3	1.0	4.0				
Phase Duration, s					19.8		19.8		35.1	11.7	46.8				
Change Period, (Y+R ₀), s					6.0		6.0		6.0	6.0	6.0				
Max Allow Headway (MAH), s					3.3		3.3		3.2	3.1	3.2				
Queue Clearance Time (g _s), s					6.9		13.1		21.6	5.0	12.6				
Green Extension Time (g _e), s					0.8		0.7		7.6	0.2	7.6				
Phase Call Probability					1.00		1.00		1.00	0.95	1.00				
Max Out Probability					0.00		0.00		0.00	0.00	0.00				
Movement Group Results				EB			WB			NB			SB		
Approach Movement				L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement				3	8	18	7	4	14	1	6	16	5	2	12
Adjusted Flow Rate (v), veh/h				53	72		174	76		100	644	632	166	554	551
Adjusted Saturation Flow Rate (s), veh/h/ln				1344	1672		1297	1681		518	1881	1841	1810	1900	1892
Queue Service Time (g _s), s				2.3	2.4		8.6	2.5		8.9	19.5	19.6	3.0	10.6	10.6
Cycle Queue Clearance Time (g _c), s				4.9	2.4		11.1	2.5		8.9	19.5	19.6	3.0	10.6	10.6
Green Ratio (g/C)				0.21	0.21		0.21	0.21		0.44	0.44	0.44	0.55	0.61	0.61
Capacity (c), veh/h				332	345		327	346		335	822	805	329	1165	1161
Volume-to-Capacity Ratio (X)				0.158	0.209		0.533	0.220		0.299	0.784	0.785	0.505	0.475	0.475
Available Capacity (c _a), veh/h				682	780		664	784		1145	3765	3685	690	4374	4357
Back of Queue (Q), veh/ln (50th percentile)				0.7	0.9		2.6	1.0		0.9	7.3	7.2	1.0	3.2	3.2
Queue Storage Ratio (RQ) (50th percentile)				0.00	0.00		0.00	0.00		0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh				24.0	21.9		26.5	21.9		13.1	16.0	16.0	12.7	7.0	7.0
Incremental Delay (d ₂), s/veh				0.1	0.1		0.5	0.1		0.2	0.6	0.7	0.4	0.1	0.1
Initial Queue Delay (d ₃), s/veh				0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh				24.1	22.0		27.1	22.1		13.2	16.7	16.7	13.1	7.1	7.1
Level of Service (LOS)				C	C		C	C		B	B	B	B	A	A
Approach Delay, s/veh / LOS				22.9	C	25.5	C	16.4	B	7.9	A				
Intersection Delay, s/veh / LOS				13.9					B						
Multimodal Results				EB			WB			NB			SB		
Pedestrian LOS Score / LOS				2.8	C	2.8	C	2.3	B	2.2	B				
Bicycle LOS Score / LOS				0.7	A	0.9	A	1.6	A	1.5	A				

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	TPD			Duration, h	0.25		
Analyst	MJA	Analysis Date	Oct 24, 2014	Area Type	Other		
Jurisdiction	Winter Park	Time Period	PM Peak Hour	PHF	0.97		
Intersection	Us 17/92 & Fairbanks Ave	Analysis Year	2014	Analysis Period	1> 16:45		
File Name	US 17-92 & Fairbanks Avenue Existing PM Peak.xus						
Project Description	Existing PM Peak Hour						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	230	675	130	97	736	84	103	1080	61	139	891	264

Signal Information				Phase Diagrams									
Cycle, s	157.9	Reference Phase	2										
Offset, s	0	Reference Point	End	Green	10.8	5.9	40.7	11.4	35.4	17.8			
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	4.0	4.0	4.0	4.0	4.0	4.0			
Force Mode	Fixed	Simult. Gap N/S	On	Red	2.0	2.0	2.0	2.0	2.0	2.0			

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	7	4	3	8
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	3.0
Phase Duration, s	28.6	58.6	16.8	46.7	17.4	58.8	23.8	65.1
Change Period, (Y+Rc), s	6.0	6.0	6.0	6.0	6.0	6.0	6.0	6.0
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.0	3.1	3.1
Queue Clearance Time (qs), s	22.5	32.7	10.7	37.0	11.4	50.0	14.2	35.8
Green Extension Time (ge), s	0.0	3.6	0.1	3.5	0.1	2.5	3.4	3.4
Phase Call Probability	1.00	1.00	0.99	1.00	0.99	1.00	1.00	1.00
Max Out Probability	1.00	0.00	0.00	0.03	0.01	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	7	4	14	3	8	18
Adjusted Flow Rate (v), veh/h	237	427	403	100	430	415	106	594	583	143	919	272
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1900	1793	1792	1881	1813	1774	1900	1864	1792	1809	1594
Queue Service Time (gs), s	20.5	30.7	30.7	8.7	34.9	35.0	9.4	48.0	48.0	12.2	33.8	20.4
Cycle Queue Clearance Time (gc), s	20.5	30.7	30.7	8.7	34.9	35.0	9.4	48.0	48.0	12.2	33.8	20.4
Green Ratio (g/C)	0.14	0.33	0.33	0.07	0.26	0.26	0.07	0.33	0.33	0.11	0.37	0.37
Capacity (c), veh/h	260	633	597	123	485	468	128	636	624	201	1356	598
Volume-to-Capacity Ratio (X)	0.913	0.674	0.675	0.816	0.887	0.887	0.826	0.933	0.934	0.711	0.677	0.455
Available Capacity (ca), veh/h	262	838	791	260	699	674	201	994	975	982	1983	874
Back of Queue (Q), veh/ln (50th percentile)	11.8	14.6	13.8	4.2	17.5	16.9	4.6	24.0	23.7	5.7	15.1	8.1
Queue Storage Ratio (RQ) (50th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d1), s/veh	67.0	45.5	45.5	72.9	56.7	56.7	72.6	51.1	51.1	67.9	41.6	37.4
Incremental Delay (d2), s/veh	32.7	0.6	0.6	4.9	7.4	7.7	8.0	8.1	8.3	1.7	0.2	0.2
Initial Queue Delay (d3), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	99.7	46.1	46.2	77.9	64.1	64.4	80.6	59.1	59.4	69.7	41.8	37.6
Level of Service (LOS)	F	D	D	E	E	E	F	E	E	E	D	D
Approach Delay, s/veh / LOS	58.0	E		65.7	E		61.0	E		43.9	D	
Intersection Delay, s/veh / LOS	56.4			E			E			E		

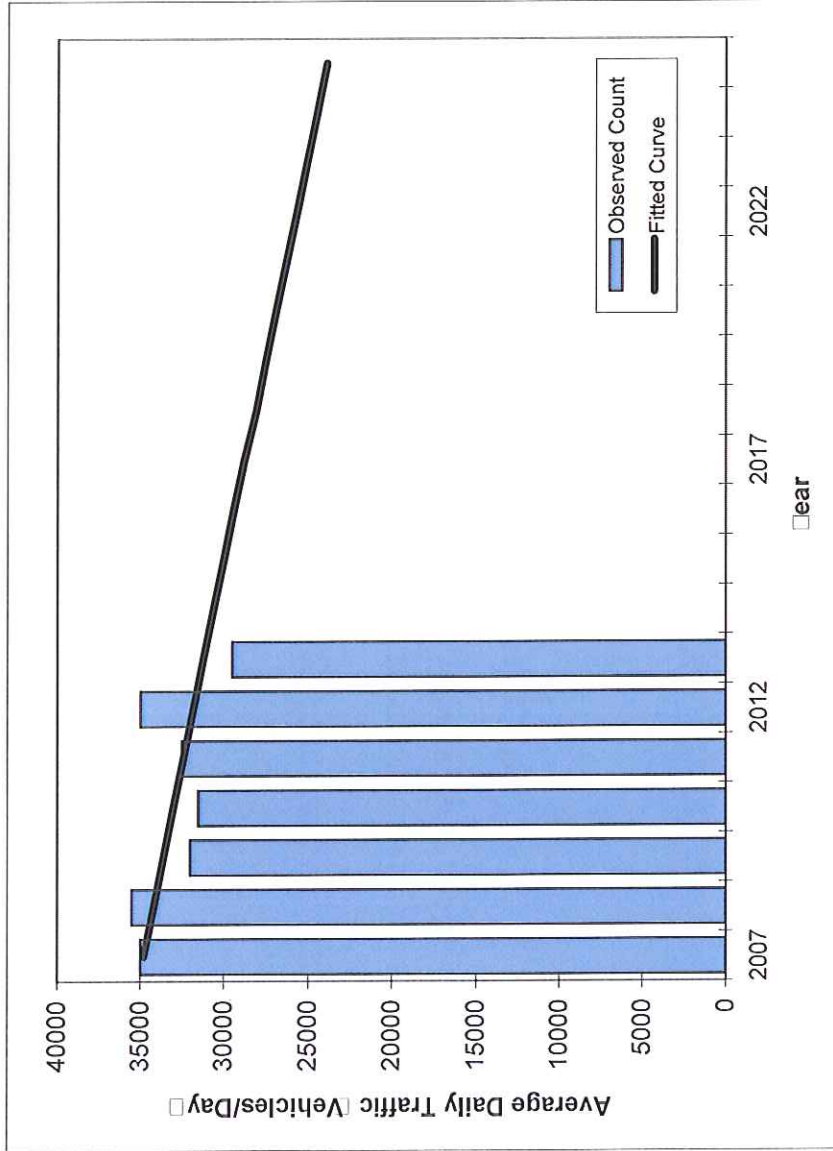
Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.8	C		3.0	C		2.9	C		2.8	C	
Bicycle LOS Score / LOS	1.4	A		1.3	A		1.5	A		1.6	A	

APPENDIX D

Trends Analysis

TRAFFIC TRENDS
 US 1 @ 0.446 Mi S of SR 42

County: Orange
 Station: 5061
 Highway: US 17-92



Year	Traffic Count	ADT/AADT Trend
2007	35000	34800
2008	35500	34200
2009	32000	33600
2010	31500	33000
2011	32500	32400
2012	35000	31800
2013	29500	31200

2014 Opening Year Trend	
2014	N/A 30600
2016 Mid-Year Trend	
2016	N/A 29400
2018 Design Year Trend	
2018	N/A 28100
TRANPLAN Forecasts/Trends	

** Annual Trend Increase: -607
 Trend R-squared: 34.4
 Trend Annual Historic Growth Rate: -1.72
 Trend Growth Rate (2013 to Design Year): -1.99
 Printed: 15-Aug-14

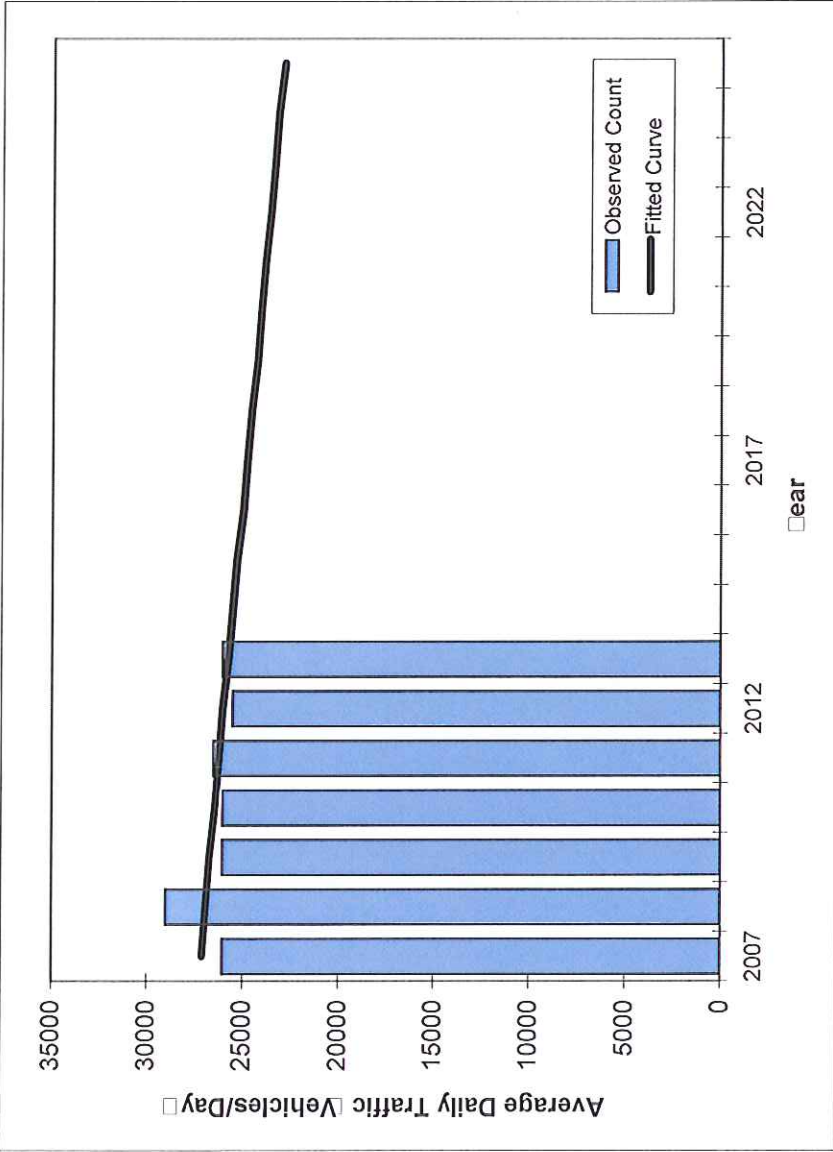
Straight Line Growth Option

*Axle-Adjusted

TRAFFIC TRENDS

US 1 @ 0.1 Mi of SR 52

County: Orange
 Station: 5058
 Highway: US 17-92



** Annual Trend Increase: -232
 Trend R-squared: 18.4
 Trend Annual Historic Growth Rate: -0.86
 Trend Growth Rate (2013 to Design Year): -0.86
 Printed: 15-Aug-14

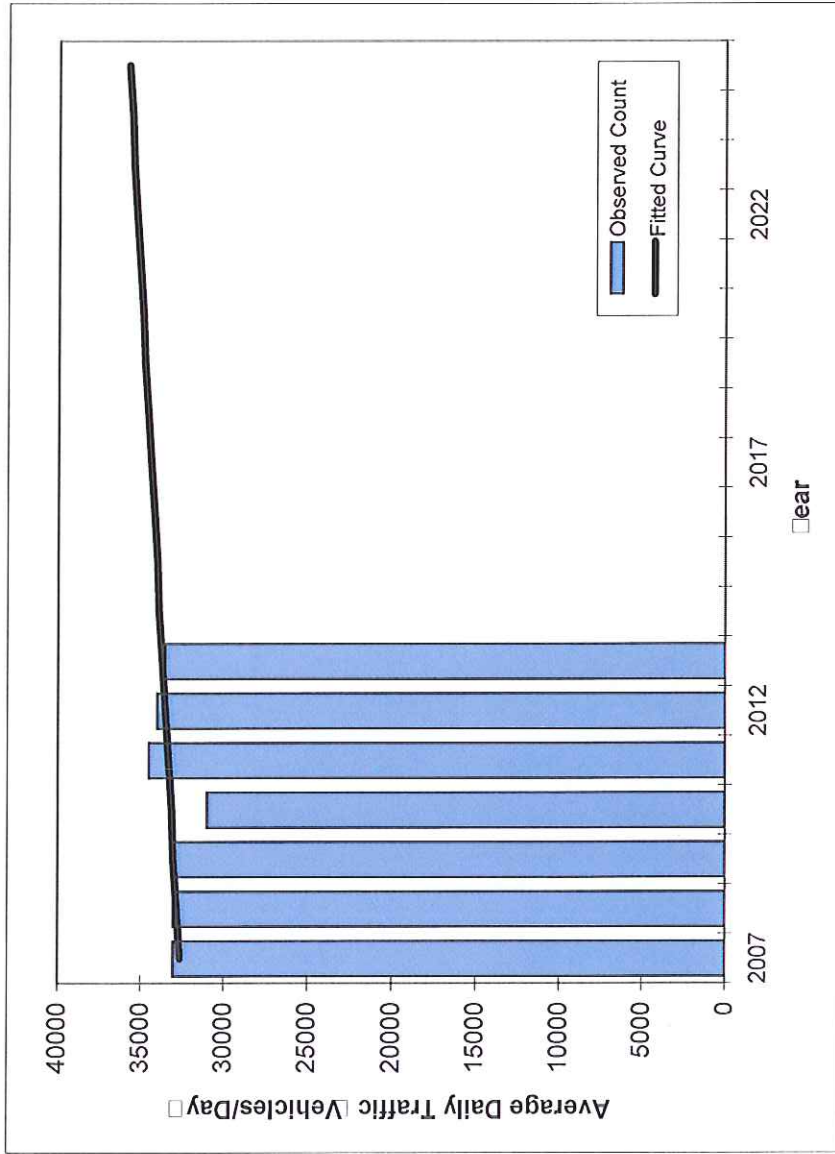
Straight Line Growth Option

Year	Traffic Count	ADT/AADT Trend
2007	26000	27100
2008	29000	26900
2009	26000	26700
2010	26000	26400
2011	26500	26200
2012	25500	26000
2013	26000	25700
2014 Opening Year Trend		
2014	N/A	25500
2016 Mid-Year Trend		
2016	N/A	25000
2018 Design Year Trend		
2018	N/A	24600
TRANPLAN Forecasts/Trends		

*Axle-Adjusted

TRAFFIC TRENDS
 SR 426 0.2 Mi of US 1 92

County: Orange
 Station: 435
 Highway: SR 426



Year	Traffic Count	ADT/AADT Trend
2007	33000	32600
2008	33000	32800
2009	33000	33000
2010	31000	33100
2011	34500	33300
2012	34000	33500
2013	33500	33700

2014 Opening Year Trend	
2014	N/A 33900
2016 Mid-Year Trend	
2016	N/A 34200
2018 Design Year Trend	
2018	N/A 34600
TRANPLAN Forecasts/Trends	

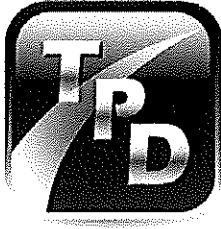
** Annual Trend Increase: 179
 Trend R-squared: 12.1
 Trend Annual Historic Growth Rate: 0.56
 Trend Growth Rate (2013 to Design Year): 0.53
 Printed: 15-Aug-14

Straight Line Growth Option

*Axle-Adjusted

APPENDIX

Lakeside Trip Estimation



LAKESIDE PROJECT

Trip Generation Summary

ITE Code	Land Use	Site	Daily Trips		P.M. Peak Hour Generation			
			Rate	Trips	Rate	Enter	Exit	Total
820	Shopping Center	39,485 S ² F	94.01	3,712	8.16	155	167	322
Total Trips			--	3,712	--	155	167	322
Pass-by Trips (34)			--	1,262	--	53	57	110
Net New Trips			--	2,450	--	102	110	212

Shopping Center Constructed/Occupied	□ 16,000 S ² F	40.52□
Shopping Center Under Construction	□ 23,485 S ² F	59.48□
Total Center Site	39,485 S ² F	100.00□

Trip Generation Under Construction (P.M. Peak Hour)

(Prorated based upon site)

Pass-by Trips □ 32 Enter / 34 Exit

New Net Trips □ 61 Enter / 63 Exit

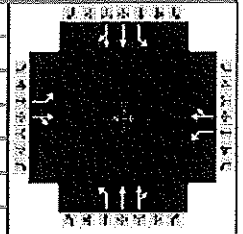
Note: Trip Generation calculations based upon the 9th Edition of the ITE Trip Generation Manual and 2nd Edition of the ITE Trip Generation Handbook.

APPENDIX F

Projected HCS Capacity Analysis

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information		
Agency	TPD			Duration, h	0.25	
Analyst	MJA	Analysis Date	Oct 24, 2014		Area Type	Other
Jurisdiction	Winter Park	Time Period	PM Peak Hour	PHF	0.97	
Intersection	US 17/92 & Morse Boulevard		Analysis Year	2014	Analysis Period	1 > 16:45
File Name	US 17-92 & Morse Boulevard Projected PM Peak.xus					
Project Description	Projected PM Peak Hour					



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	87	35	100	172	42	54	143	1176	76	174	1075	45

Signal Information														
Cycle, s	76.8	Reference Phase	2	Green	6.0	33.9	18.9	0.0	0.0	0.0	1	2	3	4
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	4.0	0.0	0.0	0.0	5	6	7	8
Uncoordinated	Yes	Simult. Gap E/W	On	Red	2.0	2.0	2.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On											

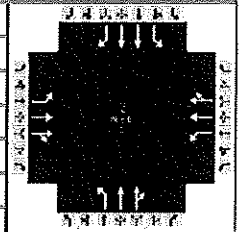
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		8		4		6	5	2
Case Number		6.0		6.0		6.3	1.0	4.0
Phase Duration, s		24.9		24.9		39.9	12.0	51.8
Change Period, (Y+R _c), s		6.0		6.0		6.0	6.0	6.0
Max Allow Headway (MAH), s		3.4		3.4		3.3	3.1	3.3
Queue Clearance Time (g _s), s		10.0		18.0		24.8	5.8	15.6
Green Extension Time (g _e), s		1.1		1.0		9.0	0.2	9.0
Phase Call Probability		1.00		1.00		1.00	0.98	1.00
Max Out Probability		0.00		0.01		0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	3	8	18	7	4	14	1	6	16	5	2	12
Adjusted Flow Rate (v), veh/h	90	139		177	99		147	652	639	179	581	573
Adjusted Saturation Flow Rate (s), veh/h/ln	1317	1676		1221	1708		494	1881	1841	1810	1900	1873
Queue Service Time (g _s), s	4.5	5.2		10.7	3.6		18.8	22.7	22.8	3.8	13.6	13.6
Cycle Queue Clearance Time (g _c), s	8.0	5.2		16.0	3.6		20.1	22.7	22.8	3.8	13.6	13.6
Green Ratio (g/C)	0.25	0.25		0.25	0.25		0.44	0.44	0.44	0.55	0.60	0.60
Capacity (c), veh/h	357	413		311	420		304	831	813	299	1136	1119
Volume-to-Capacity Ratio (X)	0.251	0.337		0.571	0.235		0.485	0.784	0.786	0.600	0.512	0.512
Available Capacity (c _a), veh/h	564	676		503	689		941	3256	3186	605	3783	3729
Back of Queue (Q), veh/ln (50th percentile)	1.4	2.0		3.1	1.4		2.0	8.9	8.8	1.4	4.6	4.6
Queue Storage Ratio (RQ) (50th percentile)	0.00	0.00		0.00	0.00		0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	26.4	23.8		30.4	23.2		18.1	18.3	18.3	15.2	9.0	9.0
Incremental Delay (d ₂), s/veh	0.1	0.2		0.6	0.1		0.4	0.6	0.6	0.7	0.1	0.1
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	26.5	24.0		31.0	23.3		18.5	18.9	19.0	15.9	9.1	9.1
Level of Service (LOS)	C	C		C	C		B	B	B	B	A	A
Approach Delay, s/veh / LOS	25.0	C		28.2	C		18.9	B		10.0	B	
Intersection Delay, s/veh / LOS	16.5						B					

Multimodal Results	EB	WB	NB	SB
Pedestrian LOS Score / LOS	2.8 / C	2.8 / C	2.3 / B	2.2 / B
Bicycle LOS Score / LOS	0.9 / A	0.9 / A	1.7 / A	1.6 / A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency	TPD			Duration, h	0.25
Analyst	MJA	Analysis Date	Oct 24, 2014	Area Type	Other
Jurisdiction	Winter Park	Time Period	PM Peak Hour	PHF	0.97
Intersection	Us 17/92 & Fairbanks Ave	Analysis Year	2014	Analysis Period	1 > 16:45
File Name	US 17-92 & Fairbanks Avenue Projected PM Peak.xus				
Project Description	Projected PM Peak Hour				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	258	682	131	98	743	88	104	1123	62	142	924	285

Signal Information				EB		WB		NB		SB	
Cycle, s	166.4	Reference Phase	2								
Offset, s	0	Reference Point	End	Green	11.4	5.6	43.1	12.0	39.4	18.9	
Uncoordinated	Yes	Simult. Gap EW	On	Yellow	4.0	4.0	4.0	4.0	4.0	4.0	
Force Mode	Fixed	Simult. Gap N/S	On	Red	2.0	2.0	2.0	2.0	2.0	2.0	

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	7	4	3	8
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	3.0
Phase Duration, s	29.0	60.7	17.4	49.1	18.0	63.4	24.9	70.3
Change Period, (Y+R _c), s	6.0	6.0	6.0	6.0	6.0	6.0	6.0	6.0
Max Allow Headway (MAH), s	3.1	3.1	3.1	3.1	3.1	3.0	3.1	3.1
Queue Clearance Time (g _s), s	25.0	34.9	11.3	39.4	12.0	54.5	15.2	38.6
Green Extension Time (g _e), s	0.0	3.6	0.1	3.5	0.1	2.6	3.6	3.6
Phase Call Probability	1.00	1.00	0.99	1.00	0.99	1.00	1.00	1.00
Max Out Probability	1.00	0.00	0.00	0.05	0.02	0.00	0.00	0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	7	4	14	3	8	18
Adjusted Flow Rate (v), veh/h	266	431	407	101	436	420	107	616	605	146	953	294
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1900	1793	1792	1881	1811	1774	1900	1865	1792	1809	1594
Queue Service Time (g _s), s	23.0	32.9	32.9	9.3	37.4	37.4	10.0	52.5	52.5	13.2	36.6	23.1
Cycle Queue Clearance Time (g _c), s	23.0	32.9	32.9	9.3	37.4	37.4	10.0	52.5	52.5	13.2	36.6	23.1
Green Ratio (g/C)	0.14	0.33	0.33	0.07	0.26	0.26	0.07	0.35	0.35	0.11	0.39	0.39
Capacity (c), veh/h	249	625	589	123	488	470	128	657	644	204	1399	617
Volume-to-Capacity Ratio (X)	1.067	0.690	0.691	0.824	0.894	0.895	0.835	0.939	0.940	0.719	0.681	0.477
Available Capacity (c _a), veh/h	249	796	752	247	665	640	191	944	927	933	1885	831
Back of Queue (Q), veh/ln (50th percentile)	15.9	15.8	14.9	4.5	19.0	18.4	5.0	26.8	26.4	6.2	16.4	9.2
Queue Storage Ratio (RQ) (50th percentile)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00
Uniform Delay (d ₁), s/veh	72.0	48.7	48.7	76.8	59.6	59.6	76.5	52.9	53.0	71.4	42.6	38.5
Incremental Delay (d ₂), s/veh	75.9	1.0	1.1	5.2	9.6	9.9	11.7	10.8	11.2	1.8	0.2	0.2
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	147.9	49.7	49.7	82.0	69.2	69.6	88.2	63.8	64.1	73.2	42.9	38.7
Level of Service (LOS)	F	D	D	F	E	E	F	E	E	E	D	D
Approach Delay, s/veh / LOS	73.4	E		70.7	E		65.9	E		45.2	D	
Intersection Delay, s/veh / LOS	62.6						E					

Multimodal Results	EB			WB			NB			SB		
Pedestrian LOS Score / LOS	2.9	C		3.0	C		2.9	C		2.8	C	
Bicycle LOS Score / LOS	1.4	A		1.3	A		1.6	A		1.6	A	

**CITY OF WINTER PARK
PLANNING AND ZONING BOARD**

**Staff Report
November 4, 2014**

REQUEST OF ENGLISH AND SWOOPE INVESTMENT LLC AND VILLAGE PARK SENIOR HOUSING PARTNERS LTD. TO: AMEND THE CONDITIONAL USE FOR THE VILLAGE PARK SENIOR HOUSING PROJECT AT 550 N. DENNING DRIVE SO AS TO PERMIT THE CONSTRUCTION OF FOUR NEW, TWO STORY SINGLE FAMILY DETACHED HOMES ON THE PROPERTY AT 796 W. SWOOPE AVENUE, ZONED R-3.

This is a request by English and Swoope Investment LLC and Village Park Senior Housing Partners Ltd. (Atlantic Housing Partners) to amend their Conditional Use approval for the redevelopment on the property at 796 W. Swoope Avenue. This request is for the construction of four new detached single family homes on the 796 W. Swoope property.

Back in February, 2014 the Planning Board and City Commission permitted an increase in the density for the Village Park senior housing project at 550 N. Denning from 105 units to 108 units based on the acquisition of this property, zoned R-3, at 796 W. Swoope. The Development Agreement approved at that time states that "any new development on that site (796 W. Swoope) must receive a Conditional Use permit".

Current Development Request: The 796 W. Swoope property now holds four residential units. There is a concrete block home in the front and three wood frame buildings in the rear. Those units are habitable and occupied but have been provided with minimal upkeep over the years.

The proposal is to demolish the existing structures and redevelop the property into four new single family detached homes, as shown on the attached plans. While all these numbers are not on the plans, the developer has committed that the two front homes would be a maximum of 2,000 square foot of living area plus a two car garage of 496 sq. ft. (22 x22). The two rear homes would be a maximum of 2,450 square feet plus two car garage of the same dimensions. All four homes are two story and a maximum of 30 feet tall, within the permitted 35 feet of building height. The total size of the four homes is 10,884 sq. ft. which is an FAR of 54.4%. The R-3 maximum FAR is 110% and the R-2 maximum FAR is 55%. Impervious coverage is shown at 13,187 or 65.9%. Impervious maximum in R-3 is 75% and in R-2 is 65%. So this is actually an R-2 development in terms of density (both units, impervious coverage and FAR).

There are two visitor parking spots shown on the "site survey" so that meets code of 2.5 spaces per unit. While not shown, the development must meet the City's storm water retention requirement either in swales or via underground exfiltration.

These four homes meet the setbacks for single family homes in R-3 zoning with 25 foot front and rear setbacks and a 10 foot side setback on the east side. As this is all technically one property (as a Condo) there is no required setback to the west, to the Village Park Apt. side, because technically there is no interior property line.

Background Project/Property History: In 2006, the City approved the Denning Drive apartment project at 550 N. Denning Drive. It was a three story project of 105 units. The parking garage was constructed first but when the real estate economy declined in 2008, the construction halted. The original 550 N. Denning property and 861 W. Canton property was then sold to Atlantic Housing Partners in late 2012, who revised the plans with City Commission approval into a four story, 105 unit senior housing project. In December 2012 and January 2013 the City Commission approved the revised project via Conditional Use and Development Agreement.

In February, 2014 based on the acquisition of the adjacent property at 796 W. Swoope, which is zoned R-3, the City approved converting the storage locker amenity on the 2nd, 3rd and 4th floors into an apartment unit on each of those three floors thereby increasing the density of the building/project by three units from 105 units to 108 units. Based on the R-3 maximum density of 17 units/acre, this 0.46 acres at 796 W. Swoope could then potentially hold seven units. So the property at 796 W. Swoope was capped via the amended Development Agreement at a density of four units.

Staff Summary and Recommendation:

The property at 796 W. Swoope Avenue has four somewhat deteriorated rental units. There has been some minimal repairs undertaken over the years in order to make the units rentable but they are still somewhat deteriorated rental units. This will be an upgrade with new construction.

Based on the lengthy public hearing experience for the Canton/Capen properties, directly to the south, it is clear that the neighborhood prefers single family detached housing. These developers could just as easily be proposing a single 4 unit condo building. Instead the plan is four condo units as single family detached homes. Staff assumes this is more in keeping with the desires of the neighbors and other citizens who have spoken to the City at the previous public hearings and community meetings.

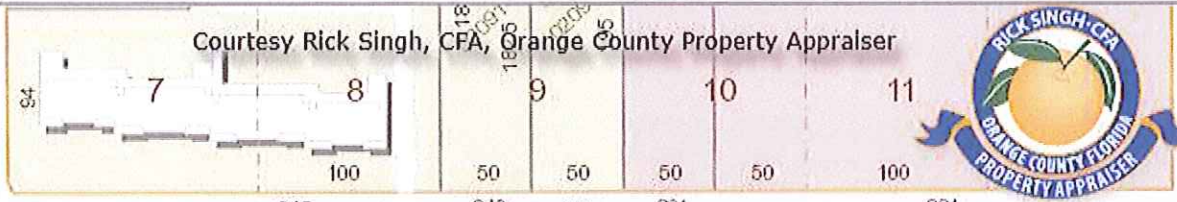
STAFF RECOMMENDATION IS FOR APPROVAL.

OCPA Web Map

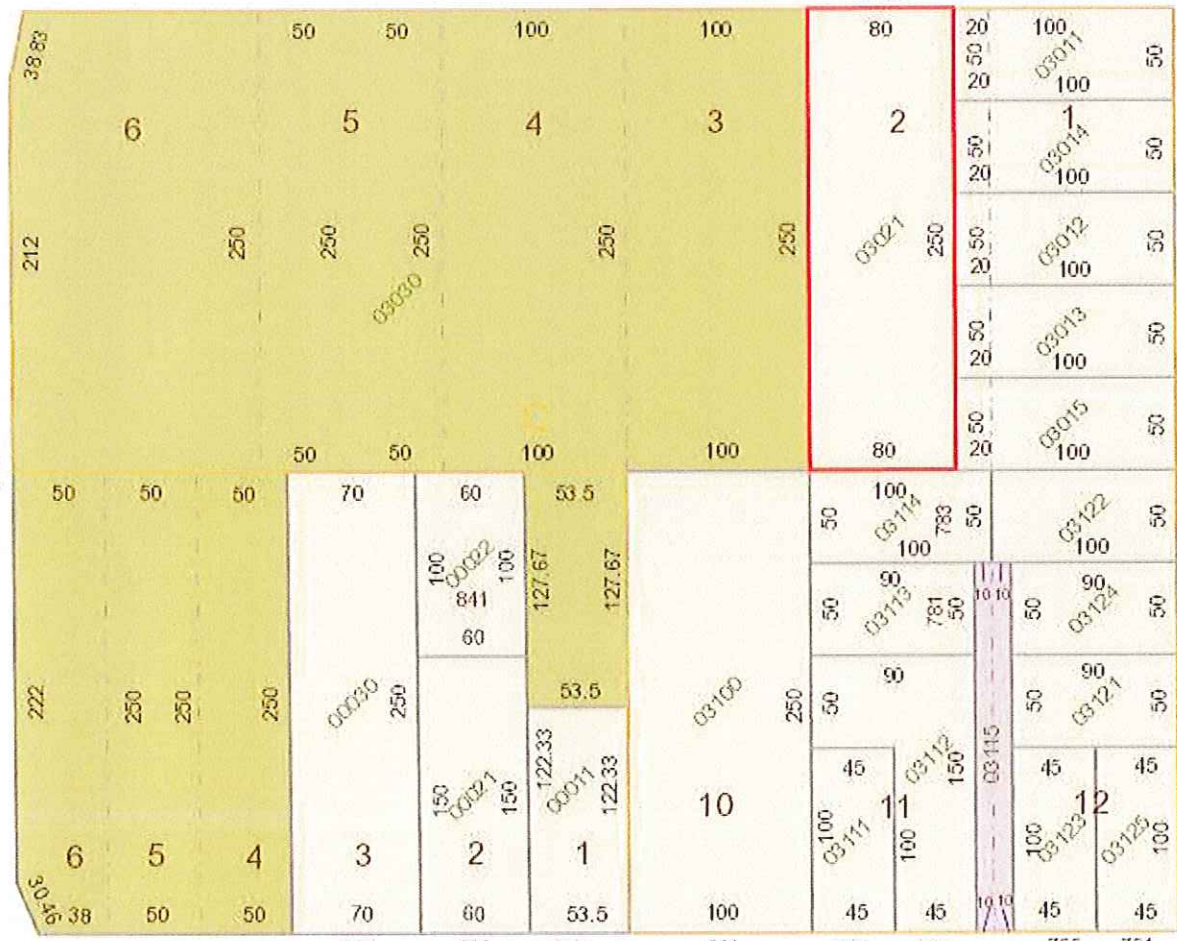
- Major Road
- Proposed Road
- Residential
- Commercial/Industrial/Vacant Land
- Park
- Florida Turnpike
- Public Road
- Brick Road
- Agriculture
- Agricultural Curflage
- Lakes and Rivers
- Interstate 4
- Galad Roads
- Block Line
- Commercial/Institutional
- Hydro
- Building
- Toll Road
- Road Under Construction
- Lot Line
- Governmental/Institutional/AMIA
- Waste Land
- Block Number
- Lot Number
- Parcel Number
- Parcel Address
- Parcel Dimension



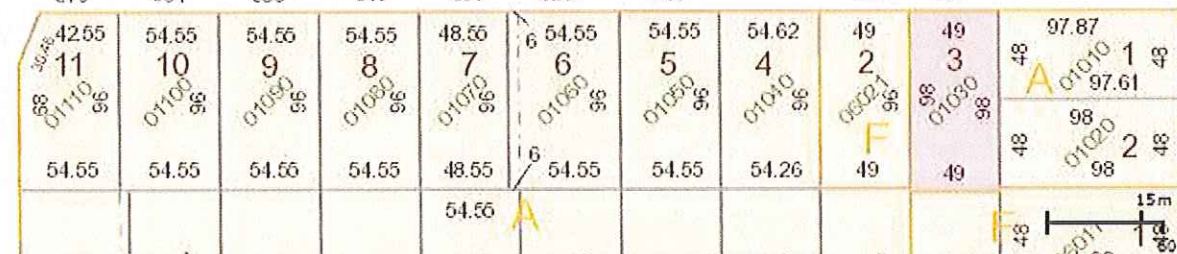
Courtesy Rick Singh, CFA, Orange County Property Appraiser



W SWOOPE AVE



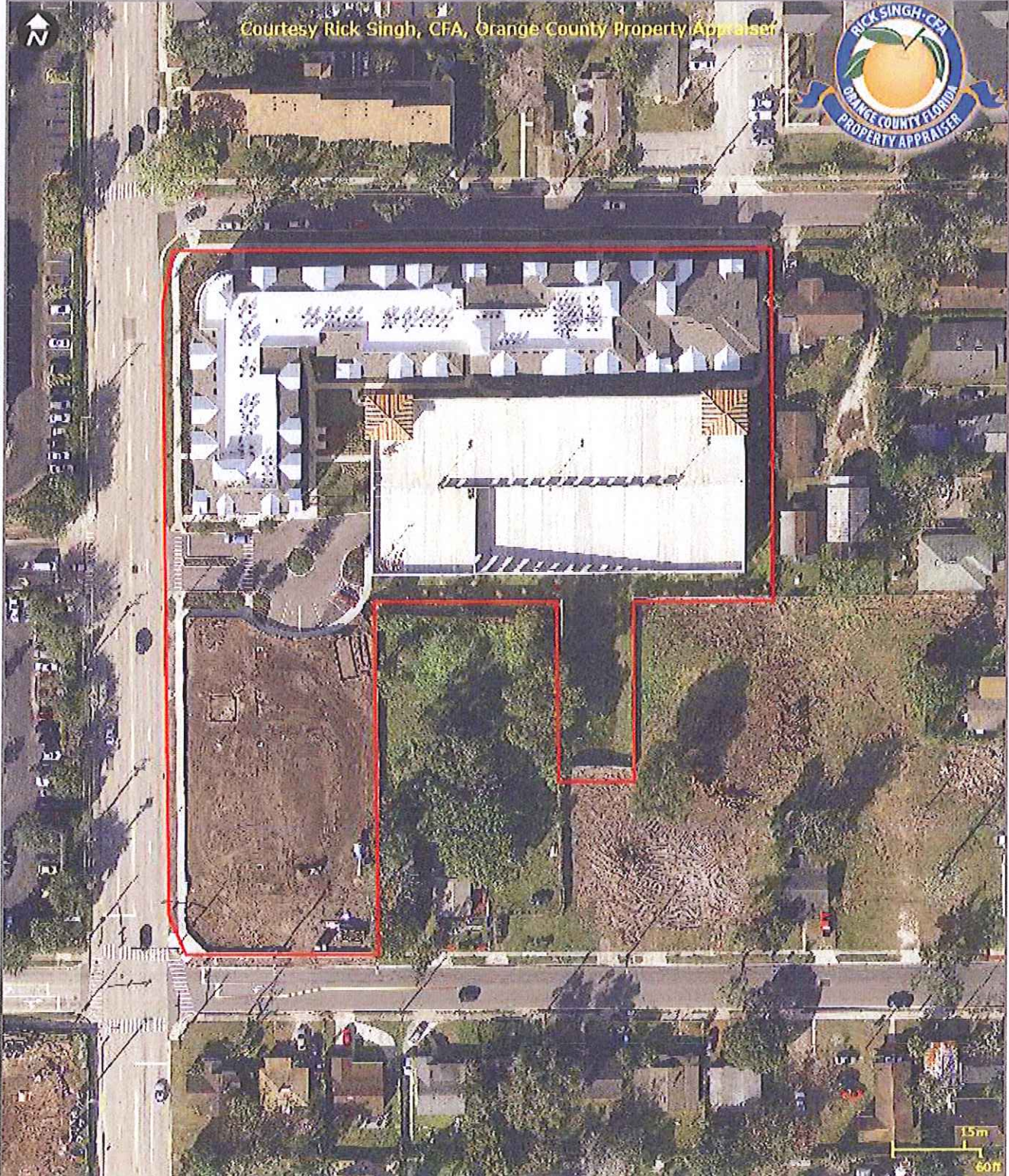
W CANTON AVE



OCPA Web Map

Major Roads	Proposed Road	Residential	Commercial/Industrial/Vacant Land	Parks	6 Lot Number
Florida Turnpike	Brick Road	Agriculture	Agricultural Curtilage	Lakes and Rivers	06060 Parcel Number
Interstate 4	Block Line	Commercial/Institutional	Hydro	Building	3106 Parcel Address
Toll Road	Road Under Construction	Governmental/Institutional/Misc.	Waste Land	E Block Number	111.9 Parcel Dimension

Courtesy Rick Singh, CFA, Orange County Property Appraiser

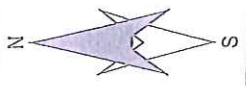
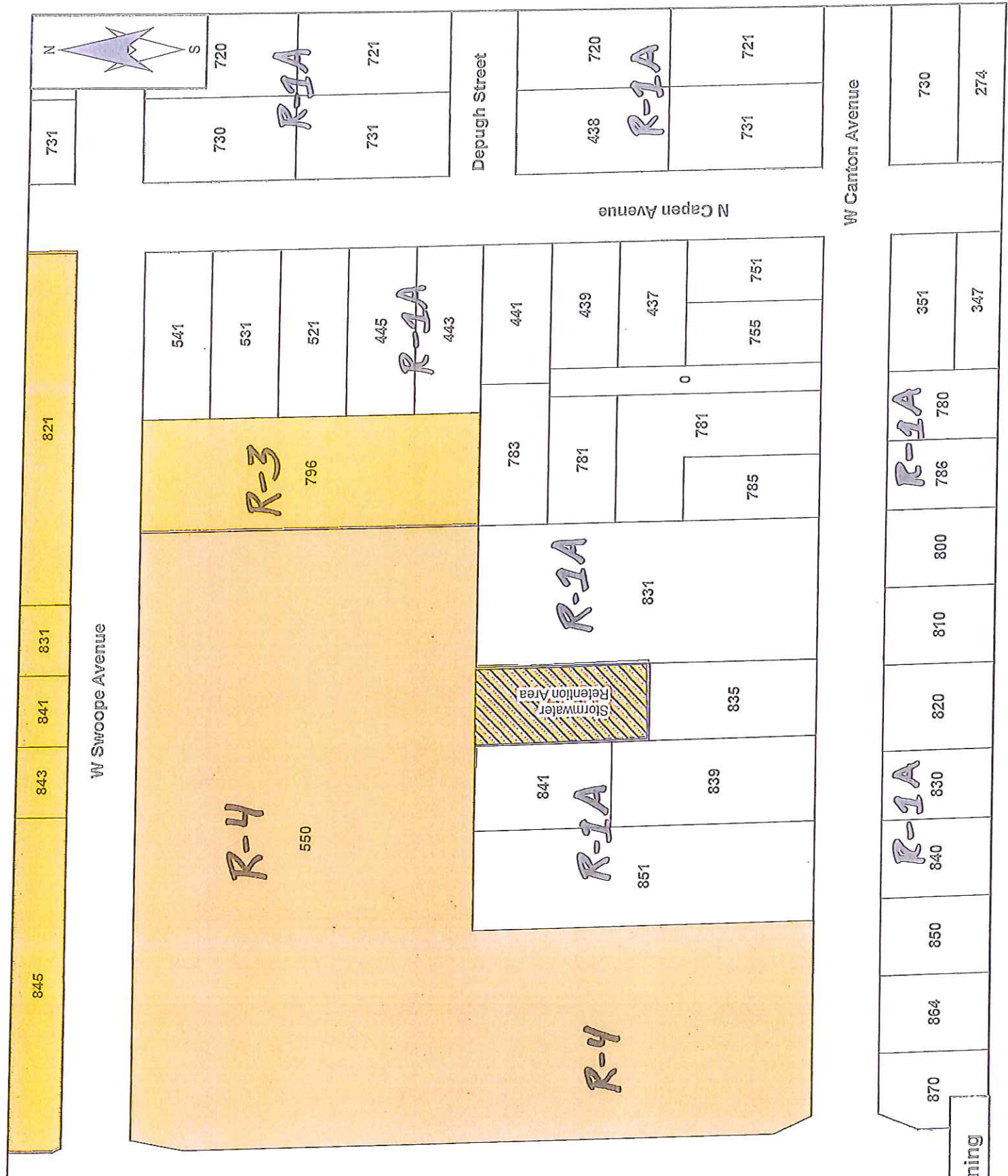


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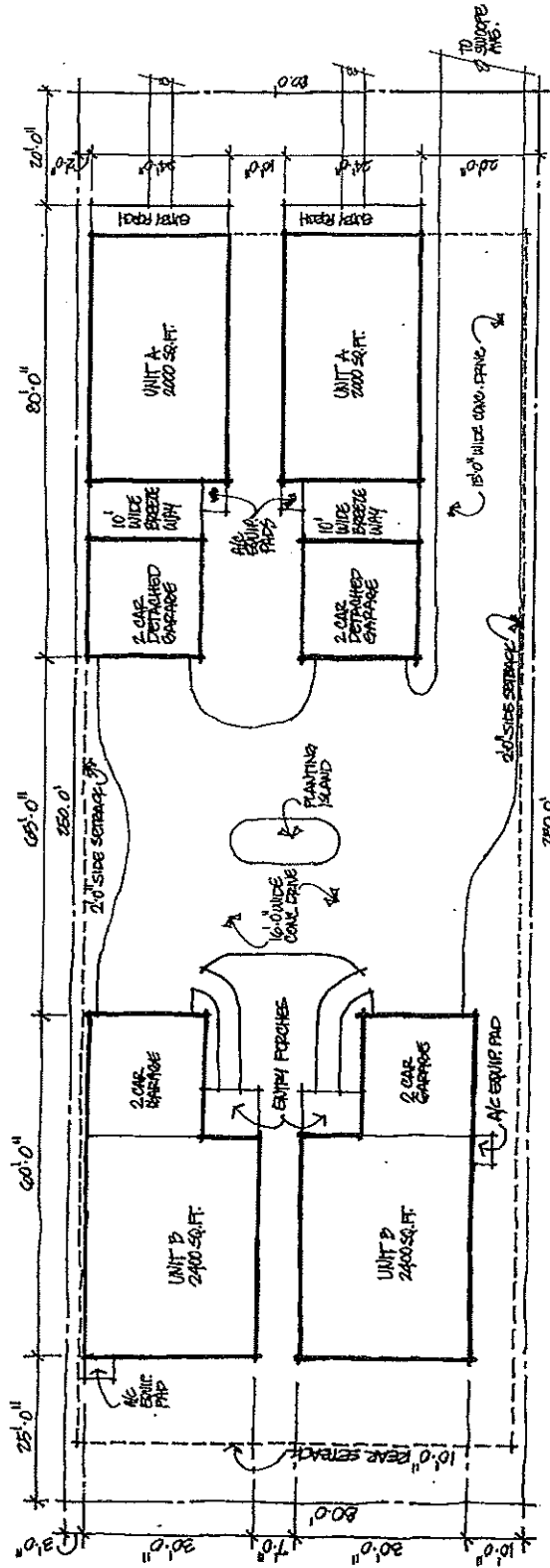
This map is for reference only and is not a survey.

Zoning
C-1
C-2
C-3
C-3A
I-1
O-1
O-2
PL
PQP
PR
PURD
R-1A
R-1AA
R-1AAA
R-2
R-3
R-4

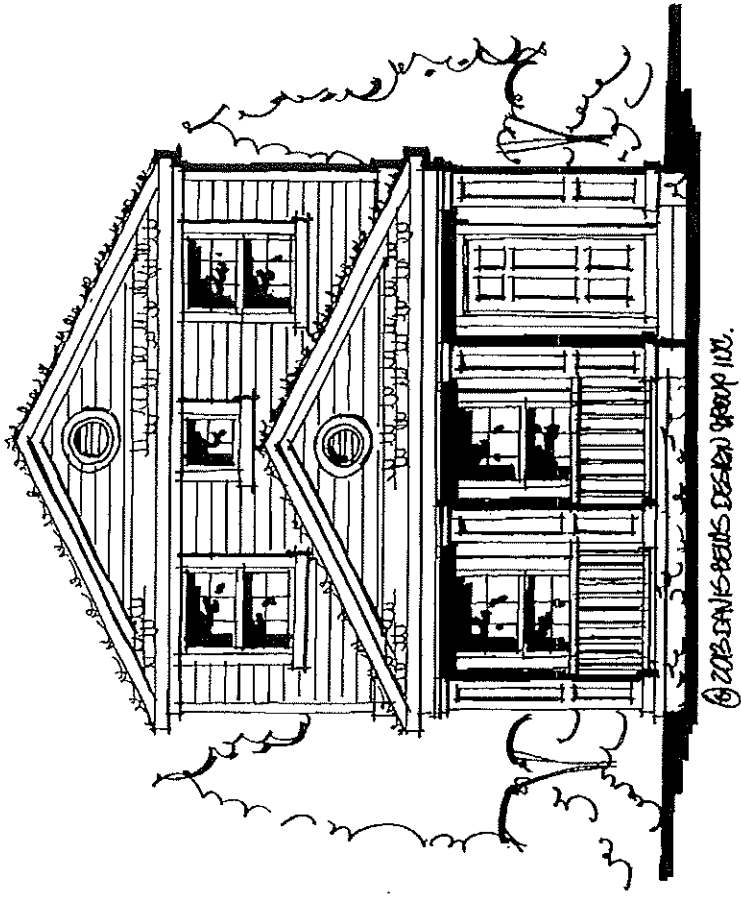
Existing Zoning



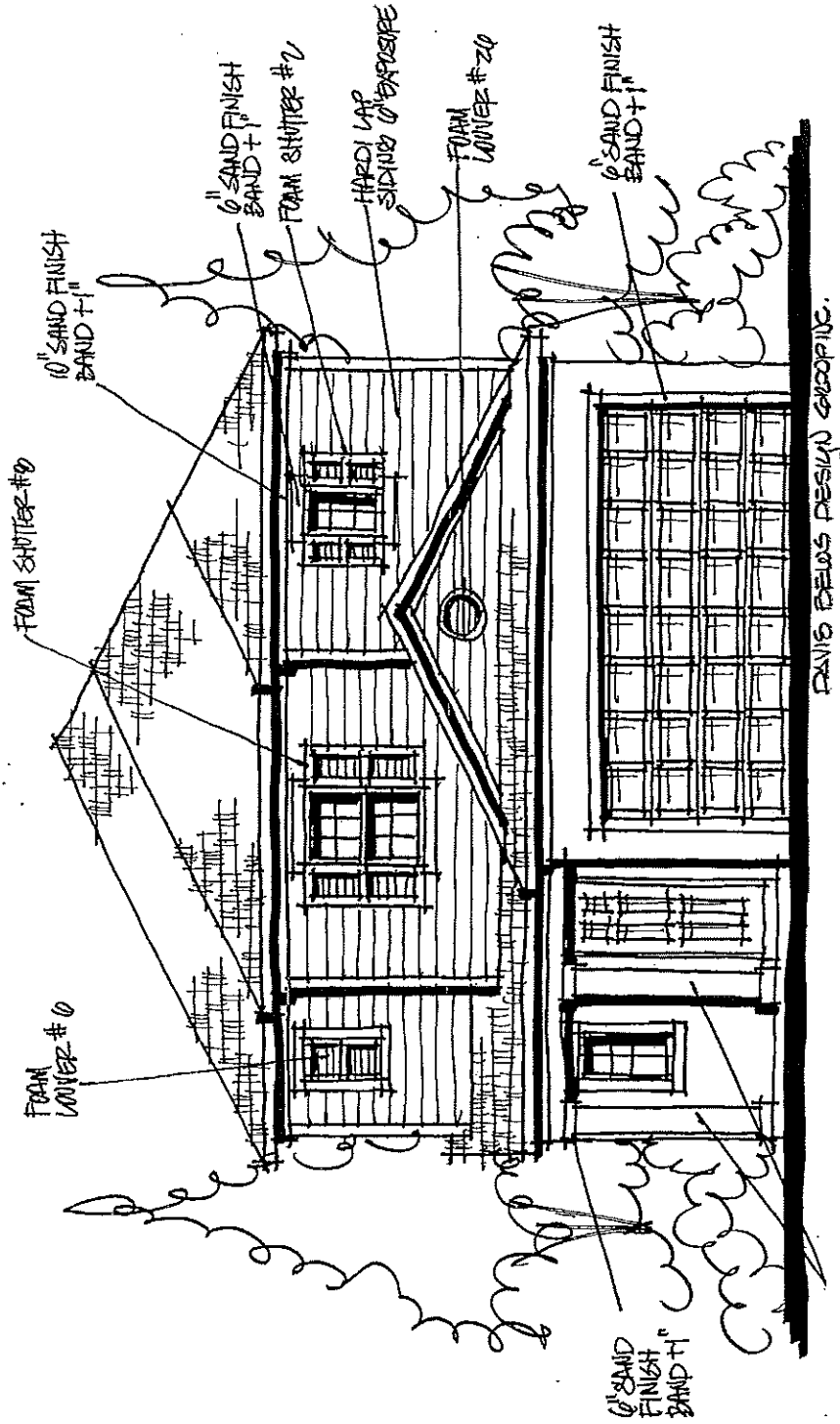
796 W. Swape Proposed Plan



PRELIMINARY SITE PLAN
 796 SWAPE AVE. WINTER PARK CO. 29114



© 2015 DANIEL PERINS DESIGN GROUP LLC.



**CITY OF WINTER PARK
PLANNING AND ZONING BOARD**

**Staff Report
November 4, 2014**

SPR 5:14 Request of Phil Kean Designs for approval of a new two-story single-family home located at 2751 Wright Avenue on Lake Forest.

Phil Kean Designs, representing the owners, (Katherine Phillips) is requesting approval for a new two-story single-family home at 2715 Wright Avenue on Lake Forest. This is a replacement house that will be built adjacent to the existing home and then the existing home demolished following completion. 46,608 sq. ft. lot is vacant. It is the last lakefront lot in this section because it was the one used for the sales office.

The property is approximately 67,714 square feet in size (1.55 acres). The new proposed two-story home will be 5,250 sq. ft. per the City's FAR calculations which is inclusive of the living area, garages, etc. which on this lot is a minimal FAR of 7.75%. This new home will have impervious lot coverage of 6,183 sq. ft. or again a minimal 9.13% of the property.

TREE PRESERVATION: This property has approximately 81 trees on-site. The new construction will remove eight oak trees and six camphor trees but there are still many, many trees still on-site.

VIEW FROM THE LAKE: This home is located 115 to 145 feet from Lake Forest. While there is some grade drop throughout the length of the home there is sufficient space for a future swimming pool which is not shown at this time. It appears that there will be no major issue with conformance to the maximum three foot height of the swimming pool deck above existing grade.

VIEW OF NEIGHBORS: From the size, location and orientation of this property, there are no other lakefront owners who have lake views affected by this project.

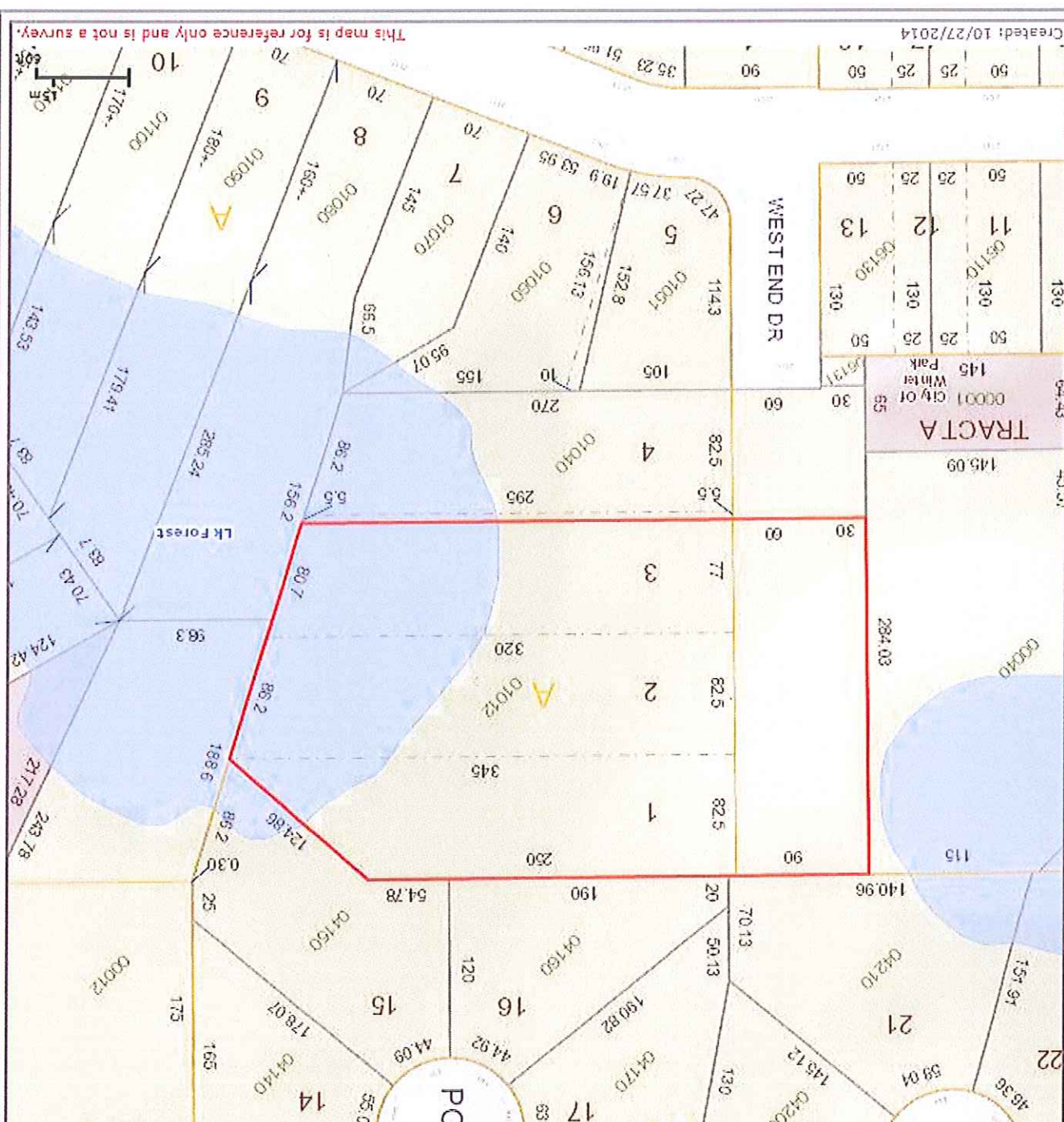
STORM WATER RETENTION: There is obviously lots of land area to work with and little chance of significant runoff into the lake even if nothing were done. However, per Code there is an area planned to meet the City's storm water retention requirements. Staff will further explore the layout of this retention so that adjacent trees are not negatively affected.

SUMMARY: Staff does not see any concerns with the new home as presented on this very large property.

STAFF RECOMMENDATION IS FOR APPROVAL.

	Major Roads		Proposed Road		Residential		Lot Line		Governmental Institutional		Waste Land		Block Number		Parcel Dimension
	Florida Turnpike		Public Roads		Agriculture		Brick Road		Commercial Institutional		Hydro		Building		Parcel Address
	Interstate 4		Road Under Construction		Vacant Land		Block Line		Agriculture Curtilage		Lakes and Rivers		Parcel Number		Parcel Address
	Toll Road				Industrial				Agriculture		Parks		Parcel Number		Parcel Address

OCPA Web Map
 Courtesy Rick Singh, CFA, Orange County Property Appraiser
 RICK SINGH, CFA
 ORANGE COUNTY PROPERTY APPRAISER
 06060 Parcel Number
 3106 Parcel Address
 1119 Parcel Dimension
 6 Lot Number



This map is for reference only and is not a survey.

Created: 10/27/2014



Courtesy Rick Singh, CFA, Orange County Property Appraiser

<p>6 Lot Number</p> <p>05050 Parcel Number</p> <p>3106 Parcel Address</p> <p>111.9 Parcel Dimension</p>	<p>Parke</p> <p>Lakes and Rivers</p> <p>Building</p> <p>Block Number</p>	<p>Commercial</p> <p>Industrial</p> <p>Vacant Land</p> <p>Agricultural</p> <p>Curlage</p> <p>Hydro</p> <p>Waste Land</p>	<p>Residential</p> <p>Agriculture</p> <p>Commercial</p> <p>Institutional</p> <p>Governmental</p> <p>Misc.</p>	<p>Proposed Road</p> <p>Brick Road</p> <p>Block Line</p> <p>Lot Line</p>	<p>Major Roads</p> <p>Public Roads</p> <p>Galad Roads</p> <p>Road Under Construction</p>	<p>Florida Turnpike</p> <p>Interstate 4</p> <p>Toll Road</p>	<p>OCPA Web Map</p>
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2751 Wright Ave. Winter Park, FL 32789

Zoning R-1A Lakefront

Max Building Height 35'

Designer: Phil Kean Design Group

Client: Kathy Phillips

Our client would like to build a new residential property on her existing lot at 2751 Wright Ave Winter Park, FL. The site sits south of Quail Hollow and east of Phillips Place. Lake Forest is to the east and an unnamed body of water to the west. A log cabin currently sits on this site and will be demolished after the construction of the new house. Our client would like to live in the log cabin during construction to save expense on moving, storage, and rent if she was to be displaced during construction.

The proposed location for the new residential property is at the north end of the site. There is an approximate 6'0" masonry wall at the north end of the site, separating Quail Hollow from our client's lot. There are relatively 81 trees located on this site. The proposed building footprint and driveway would require the removal of eight Oak and six Camphor trees. These trees are bold on the AS Sheet.

The site sits on top of a crest between two small bodies of water. Naturally, rainwater will flow into these "lakes". The required storm water retention for this site is 900.17 Cubic Feet. Area A provides 958 cubic feet.

The ISR and storm water retention calculations for this site are on the following page.



SETBACK / COVERAGE WORKSHEET

For Single Family Zoning Districts (R-1A, R-1AA & R-1AAA)¹

Address: 2751 Wright Ave. Winter Park, FL 32789

Lot width²: 242ft

Submitted by: Tommy Watkins, Phil Kean Design Group

Lot area³: 67,714sq ft

	Maximum % Allowed ⁴	Existing Area ¹⁰	Additional Proposed Area ¹⁰	New Total Area	Maximum Allowed Area
IMPERVIOUS LOT COVERAGE Include bldg footprint, driveways, sidewalks, patios, swimming pools, A/C pads, etc.	2 story - 50% 1 story - 60%	6,090	6,578	12,668	33,857
FLOOR AREA RATIO (F.A.R.)^{5,6} For one and two story bldgs (include 1st & 2nd floors, garages/carpools, stair areas on both floors, areas on 2nd floors which are open to the 1st floor ⁷ , and accessory bldgs. EXCLUDE - pool screen enclosure areas and certain open front, side & rear porches ⁸ .	Lots < 11,600 sf: Use 38% Base FAR or w/ increased side setbacks: 43% Max FAR				
	Lots 11,600 sf to 13,600 sf Use 4.500 sf Base area & 5,200 sf Maximum area				
SCREEN POOL ENCLOSURE	Lots > 13,600 sf Use 33% Base FAR or w/ increased side setbacks: 38% Max FAR	2,585	4,591	7,176	22,346
	8% ⁹				
	Minimum % Required	Existing Area ¹⁰	Landscape Area Reduced ¹⁰	New Total Area	Minimum Required Area
FRONT YARD LANDSCAPE COVERAGE Count all landscaped green areas - exclude hard surfaces and all driveway surfaces (pervious & impervious).	50%				

Front Lot Area:
NOTES:

1. Windsong & Waterbridge may use these standards, except lot types A, B, & C in Windsong. Provisions on side articulations & accessory bldgs are mandatory.
2. Lot width measured at the front bldg line across lot. The bldg line is located at the required front setback for vacant lots or front bldg wall closest to the street of existing homes. For unusual (pie) shaped lots, an average lot width may be utilized as measured between the front setback line and the required rear setback line or shall be determined by the Building Director. For a proposed home, determine the front setbacks described on page 2.
3. Submerged lands or land across the street shall not be included.
4. Percentage based on the lot area.
5. One story homes with a sloping roof, 12:12 or less, may utilize the maximum F.A.R. and may provide roof dormers, 8 ft maximum width and 2.5 ft back from the required setback, occupying 45% of roof area within the same roof plane.
6. See page 3 on how to achieve maximum F.A.R.
7. Vaulted and cathedral ceiling areas count twice if the height from the floor to the ceiling is 17.5 feet or greater.
8. The area of open front porches and entries may be excluded from the gross floor area subject to a maximum area of 400 square feet. The area within an open or screened rear and/or side porches, lanai, porte cochere or other covered areas may be excluded from the gross floor area up to 500 sf of floor area. On 2nd floor, rear and/or side porches shall have an exterior sides that are 75% open in order to utilize up to 300 sf of the total 500 sf excludable gross floor area. Utilizing this exemption requires a deed covenant to be recorded, outlining the restrictions precluding the enclosing of side and/or rear porches; and enclosing and screening of front porches.
9. Any area not already used in the permitted floor area ratio (FAR) may be added to this 8% for additional screened pool enclosure area.
10. These columns only apply to existing homes.

2751 WRIGHT AVE. ISR and Retention Calculations
CALCULATIONS:

Zoning: R-1A Lakefront
 Construction: 2-Story
 MAX. Building Height: 35'

IMPERVIOUS Calculations:

Lot Size: **67,714 SF**

<u>Description</u>		
Building Footprint	3,252	
Covered Entry	289	
Lanai	448	
Covered Side Porch	128	
Fountain	67	
Driveway/Entry Walk	3,359	
AC/Generator Pads	32	
Existing house footprint	3,227	
TOTAL	10,802	SF

Impervious Allowed:	33,857 SF	OR	50.00%
Impervious Provided:	10,802 SF	OR	15.95%
Impervious Differential:	(23,055) SF	OR	-34.05%

Retention Calculations

Required Retention	900.17 Cubic Feet		
	Dimensions	Area	Avg. Depth
Area A	958 Irregular	3830	3"
Retention Provided	958 Cubic Feet		

A SINGLE FAMILY RESIDENCE



2751 WRIGHT AVE. WINTER PARK, FLORIDA

DESIGNER:
PHIL KEAN DESIGN GROUP
ARCHITECTURE CONSTRUCTION INTERIORS
519 W. Fairbanks Avenue
Winter Park, Florida 32789
PH: (407)825-7111 (TOLL FREE)
WWW.PHILKEANDSIGN.COM

ENGINEER:

PROJECT INFO:

2751 WRIGHT AVE.
WINTER PARK, FL
32789

NOTES

BASED ON THE FLORIDA RESIDENTIAL BUILDING CODE 2010

TO THE BEST OF THE ENGINEER'S INTERPRETATION, THESE DRAWINGS, BUILDING CODE, PERmits, AND CONTRACTS, ARE DEVELOPED AND MAINTAINED BY THE ENGINEER AND ARE NOT TO BE REPRODUCED, COPIED, REPRODUCED, OR TRANSMITTED IN ANY MANNER WITHOUT THE WRITTEN PERMISSION, AND ADMINISTERED AND ENFORCED BY THE LOCAL JURISDICTIONS.

CONSTRUCTION NOTES:
1. ALL DIMENSIONS SHALL BE SHOWN IN FEET AND INCHES UNLESS OTHERWISE NOTED.
2. THE FLORIDA BUILDING CODE, 2010 EDITION IS APPLICABLE TO THIS PROJECT.
3. ALL CONSTRUCTION SHALL BE BRANDED AND CHECKED BY THE CONTRACTOR AS REQUIRED TO SAFELY PERFORM THE WORK.
4. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
5. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

ROOF COVERINGS:
1. ALL ROOF COVERINGS SHALL BE NOTED ON PLANS AND COMPLY WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL ROOF COVERINGS SHALL BE NOTED ON PLANS AND COMPLY WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

WINDOWS AND DOORS:
1. ALL WINDOWS AND DOORS SHALL BE DESIGNED TO MEET AT LEAST THE MINIMUM REQUIREMENTS OF THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL WINDOWS AND DOORS SHALL BE DESIGNED TO MEET AT LEAST THE MINIMUM REQUIREMENTS OF THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

STAIRS AND RAILING NOTES:
1. ALL STAIRS AND RAILINGS SHALL BE DESIGNED TO MEET AT LEAST THE MINIMUM REQUIREMENTS OF THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL STAIRS AND RAILINGS SHALL BE DESIGNED TO MEET AT LEAST THE MINIMUM REQUIREMENTS OF THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

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2. ALL MEANS OF EGRESS SHALL BE DESIGNED TO MEET AT LEAST THE MINIMUM REQUIREMENTS OF THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

MECHANICAL AND HVAC:
1. ALL MECHANICAL AND HVAC SYSTEMS SHALL BE DESIGNED TO MEET AT LEAST THE MINIMUM REQUIREMENTS OF THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2. ALL MECHANICAL AND HVAC SYSTEMS SHALL BE DESIGNED TO MEET AT LEAST THE MINIMUM REQUIREMENTS OF THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

TERMITE

SECTION 910 PROTECTION AGAINST TERMITES
1. TERMITE PROTECTION SHALL BE PROVIDED BY REGISTERED TERMITES, AND SHALL BE INSTALLED IN ACCORDANCE WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
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GENERAL:
1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
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10. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.

1) METHOD OF TREATMENT SHALL BE APPROVED BY THE GOVERNING AGENCIES AND SHALL BE INSTALLED IN ACCORDANCE WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
2) PRESURE TREATED LUMBER THAT HAS BEEN CUT OR DRILLED THAT IS TO BE USED IN CONTACT WITH THE GROUND SHALL BE INSTALLED IN ACCORDANCE WITH THE FLORIDA BUILDING CODE AND ALL APPLICABLE LOCAL ORDINANCES AND REGULATIONS.
3) BORGATE APPLIED TO ALL FRAME MEMBERS WITHIN 2" A.F.F.

INDEX

GENERAL

ARCHITECTURAL

ENGINEERING

COVER SHEET

CS

SHEET NO.	SHEET TITLE	DATE	DESCRIPTION
01	GENERAL	06-20-14	SCHEMATIC DESIGN
02	ARCHITECTURAL	08-04-14	SCHEMATIC DESIGN
03	ENGINEERING	08-19-14	SCHEMATIC DESIGN
04	COVER SHEET	10-27-14	WP LAKEFRONT REVIEW
05	COVER SHEET		
06	COVER SHEET		
07	COVER SHEET		
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SHEET DATA:
DESIGNED BY: TOMMY WAYTENS
DRAWN BY: ERIC LUCIA

SHEET DESCRIPTION:
COVER SHEET

SHEET:
CS

DESIGNER:
 PHIL KEAN DESIGN GROUP
 ARCHITECTURE / CONSTRUCTION / INTERIORS
 3000 Lakeside Drive
 Winter Park, Florida 32789
 PH: (407) 947-1100
 WWW.PHILKEANDSIGN.COM

ENGINEER:

PROJECT INFO:
 2751 WRIGHT AVE.
 WINTER PARK, FL
 32789

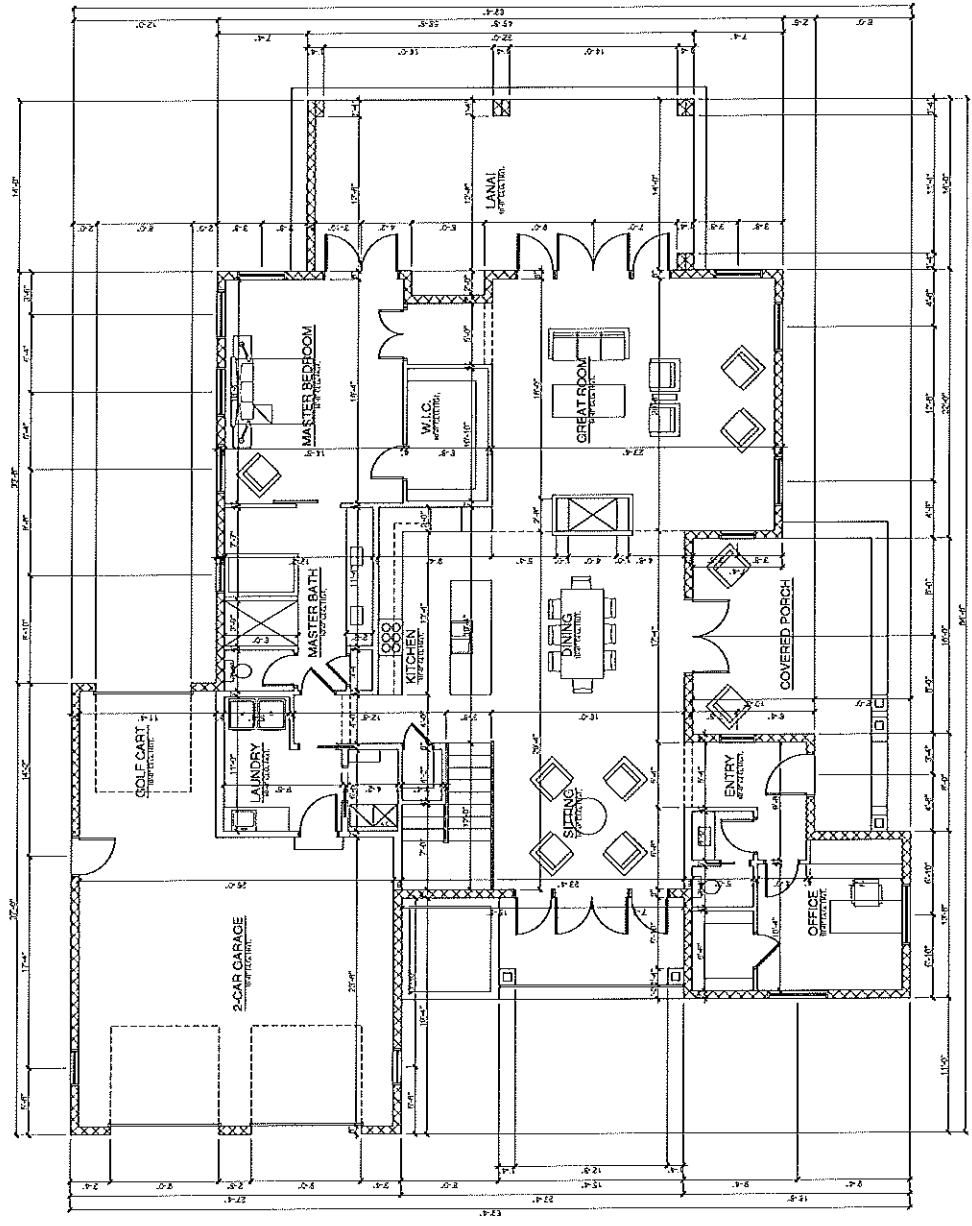
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DATE	DESCRIPTION
06-20-14	SCHEMATIC DESIGN
09-04-14	SCHEMATIC DESIGN
09-16-14	SCHEMATIC DESIGN
10-10-14	WP LAKEFRONT REVIEW
10-27-14	WP LAKEFRONT REVIEW

SHEET DATA:
 DESIGNED BY: TOMMY WATKINS
 DRAWN BY: ERIC LUCIA

SHEET DESCRIPTION:
 MAIN LEVEL FLOOR PLAN
 DIMENSIONED

SHEET:
 1



MAIN LEVEL	
AC	262
LANNI	448
COVERED ENTRY	31
SECOND LEVEL	
AC	1056
BEDROOM	206
TOTALS	
AC	1661
GARAGE	814
COVERED ENTRY	210
TOTAL UNDER ROOF	2085
BALCONY	270

MAIN LEVEL FLOOR PLAN
 SCALE: 1/8" = 1'-0"

DESIGNER:

PHIL KEAN DESIGN GROUP
 ARCHITECTURE / INTERIOR DESIGN / INTERIORS
 100 N. FORT MEADE BLVD.
 WINTER PARK, FLORIDA 32789
 PH: (407) 839-1100
 WWW.PHILKEANDSIGN.COM

ENGINEER:

PROJECT INFO:

2751 WRIGHT AVE.
 WINTER PARK, FL
 32789

PLAN HISTORY:

DATE	DESCRIPTION
09-29-14	SCHEMATIC DESIGN
09-04-14	SCHEMATIC DESIGN
09-16-14	SCHEMATIC DESIGN
10-16-14	W/P LAKEFRONT REVIEW
10-27-14	W/P LAKEFRONT REVIEW

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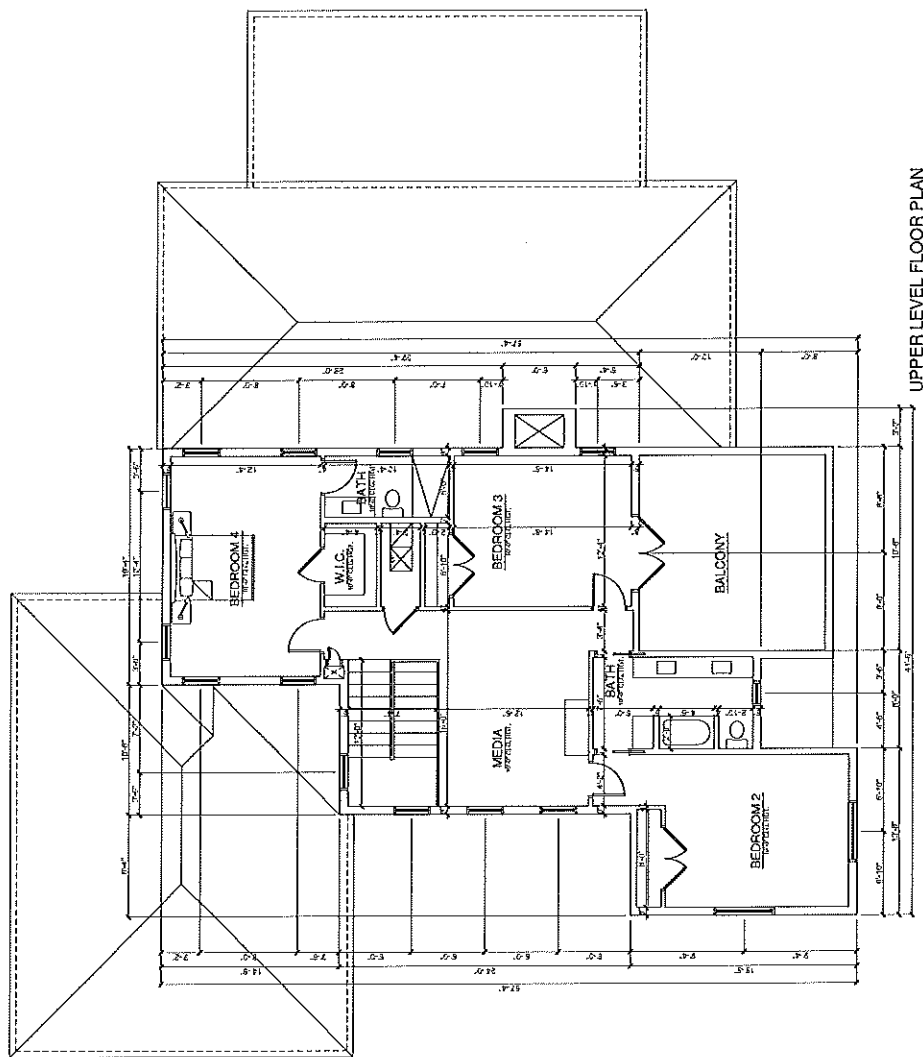
DESIGNED BY: TOMMY WATKINS
 DRAWN BY: ERIC LUCIA

SHEET DESCRIPTION:

UPPER LEVEL FLOOR PLAN
 DIMENSIONED

SHEET:

2



MAIN LEVEL	
AREA	543
GARAGE	448
LANAI	448
COVERED ENTRY	312
SECOND LEVEL	
AREA	1225
BALCONY	270
TOTALS	
AREA	1961
GARAGE	614
COVERED ENTRY	312
TOTAL UNDER ROOF	6225
BALCONY	270

UPPER LEVEL FLOOR PLAN
 SCALE: 1/8" = 1'-0"

DESIGNER:

PHIL KEAN DESIGN GROUP
ARCHITECTURAL ARCHITECTURE / INTERIORS
100 W. Central Avenue
Winter Park, Florida 32789
PH: (407) 939-1100
WWW.PHILKEANDESIGN.COM

ENGINEER:

PROJECT INFO:

2751 WRIGHT AVE.
WINTER PARK, FL
32789

PLAN HISTORY:

DATE	DESCRIPTION
04-20-14	SCHEMATIC DESIGN
08-04-14	SCHEMATIC DESIGN
08-18-14	SCHEMATIC DESIGN
10-16-14	WP LAKEFRONT REVIEW
10-27-14	WP LAKEFRONT REVIEW

SHEET DATA:

DESIGNED BY: TOMMY WATKINS
DRAWN BY: ERIC LUCIA

SHEET DESCRIPTION:

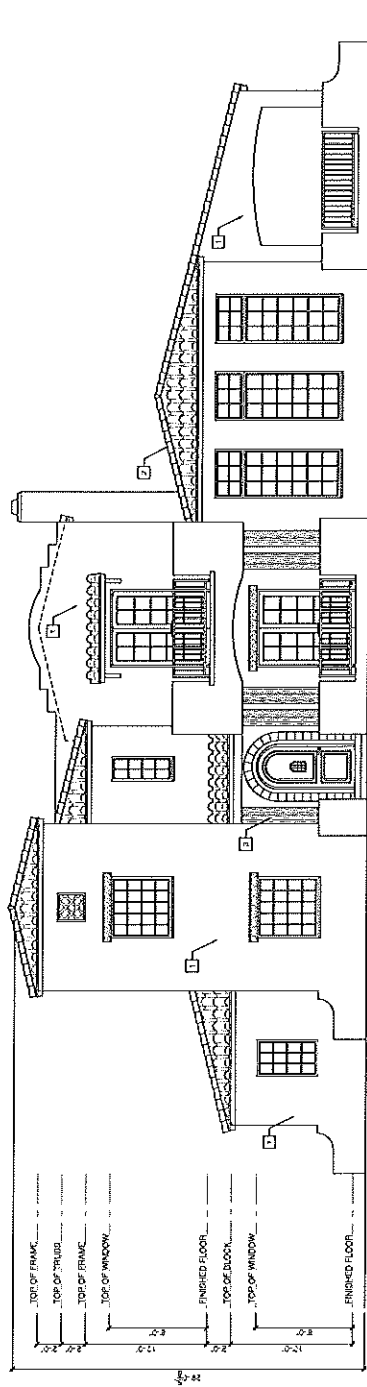
FRONT AND REAR
ELEVATIONS

SHEET:

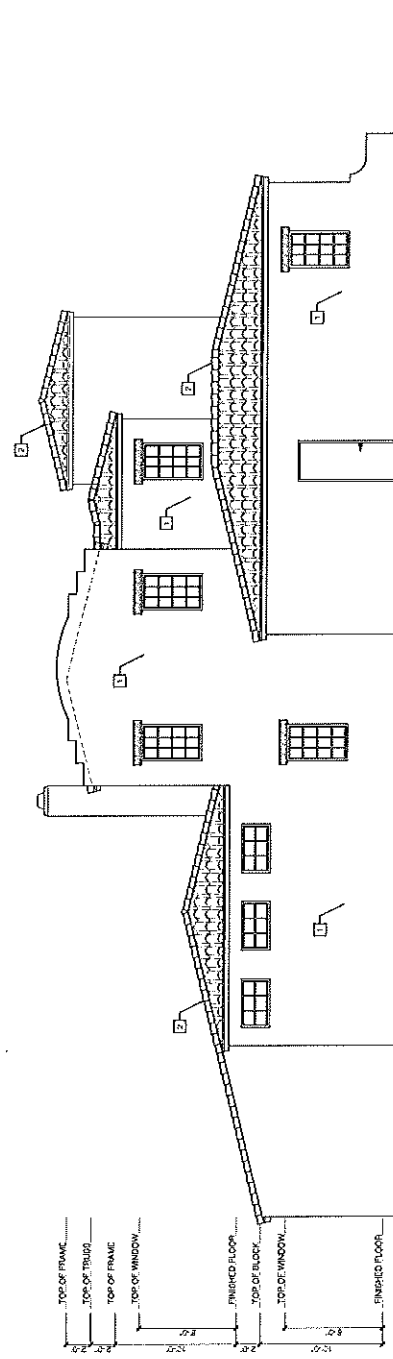
3

ELEVATION NOTES:

- 1. FINISH FLOOR
- 2. CLAY TILE ROOF
- 3. PAIR WOOD



FRONT ELEVATION
SCALE: 1/8" = 1'-0"



REAR ELEVATION
SCALE: 1/8" = 1'-0"

ELEVATION NOTES:

- 1. STUCCO FINISH
- 2. CLAY TILE ROOF
- 3. PAIR WOOD

DESIGNER:

PHIL KEAN DESIGN GROUP
 ARCHITECTURE / CONSTRUCTION / INTERIORS
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 Winter Park, Florida 32789
 PH: (407) 939-1111 FAX: (407) 939-1112
 WWW.PHILKEANDSIGN.COM

ENGINEER:

PROJECT INFO:

2751 WRIGHT AVE.
 WINTER PARK, FL
 32789

PLAN HISTORY:

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05-20-14	SCHEMATIC DESIGN
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08-19-14	SCHEMATIC DESIGN
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10-27-14	WP LAKEFRONT REVIEW

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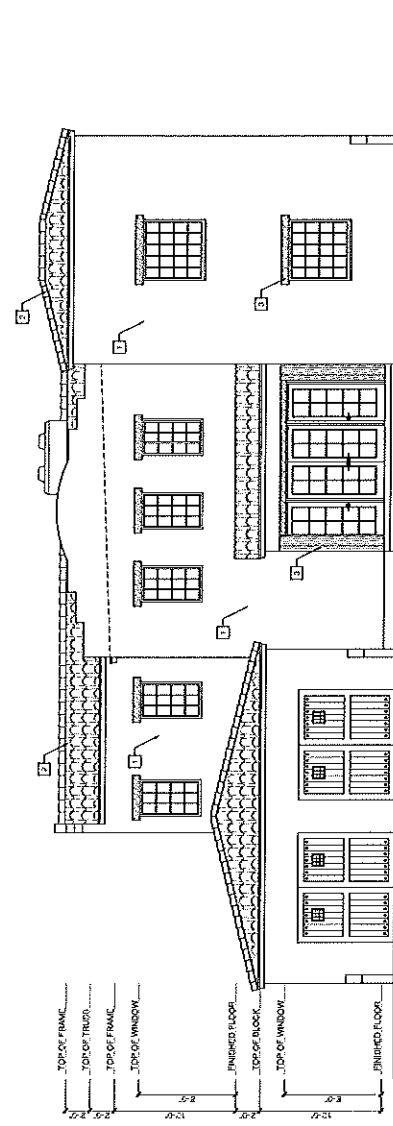
DESIGNED BY: TOMMY WATKINS
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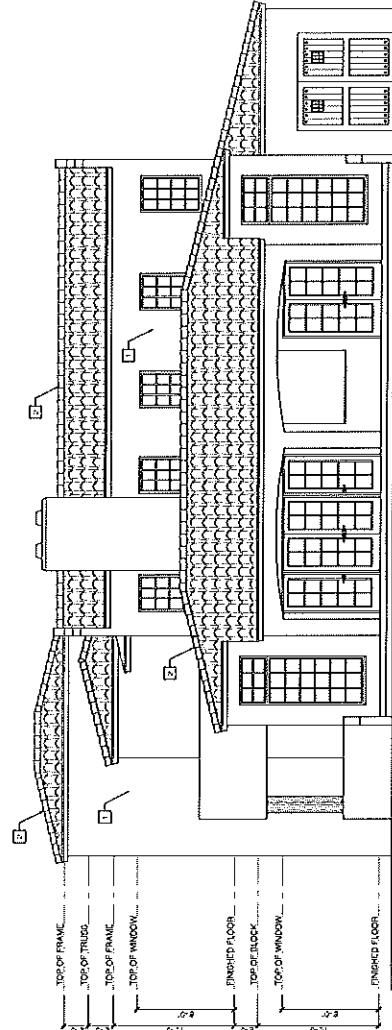
LEFT AND RIGHT
 ELEVATIONS

SHEET:

4



LEFT ELEVATION
 SCALE: 1/8" = 1'-0"



RIGHT ELEVATION
 SCALE: 1/8" = 1'-0"

DESIGNER:

PHIL KEAN DESIGN GROUP
ARCHITECTURE / INTERIOR DESIGN / LANDSCAPE ARCHITECTURE
100 W. Central Avenue
Winter Park, Florida 32789
PH: (407) 937-1000
WWW.PHILKEANDSIGN.COM

ENGINEER:

PROJECT INFO:

2751 WRIGHT AVE.
WINTER PARK, FL
32789

PLAN HISTORY:

DATE	DESCRIPTION
08-20-14	SCHEMATIC DESIGN
08-04-14	SCHEMATIC DESIGN
09-10-14	SCHEMATIC DESIGN
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10-27-14	WP LAKEFRONT REVIEW

SHEET DATA:

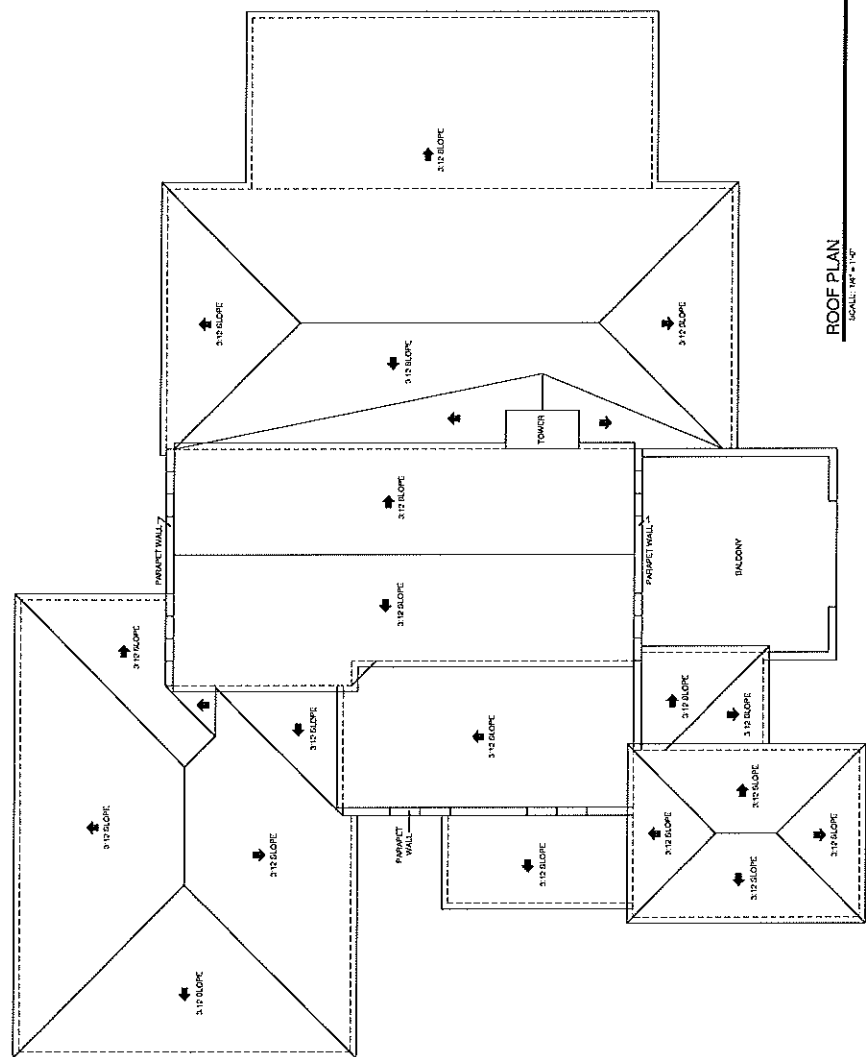
DESIGNED BY: TOMMY WATKINS
DRAWN BY: ERIC LUCIA

SHEET DESCRIPTION:

BIRDSEYE
LOW ROOF

SHEET:

5



DESIGNER:

PHIL KLAN DESIGN GROUP
ARCHITECTURE / CONSTRUCTION / INTERIORS
19741 Lakeside Avenue
Winter Park, Florida 32789
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ENGINEER:

PROJECT INFO:

2751 WRIGHT AVE.
WINTER PARK, FL
32789

PLAN HISTORY:

DATE	DESCRIPTION
06-20-14	SCHEMATIC DESIGN
08-04-14	SCHEMATIC DESIGN
09-16-14	SCHEMATIC DESIGN
10-16-14	WP LAKEFRONT REVIEW
10-27-14	WP LAKEFRONT REVIEW

SHEET DATA:

DESIGNED BY: TOMMY WATKINS
DRAWN BY: ERIC LUCIA

SHEET DESCRIPTION:

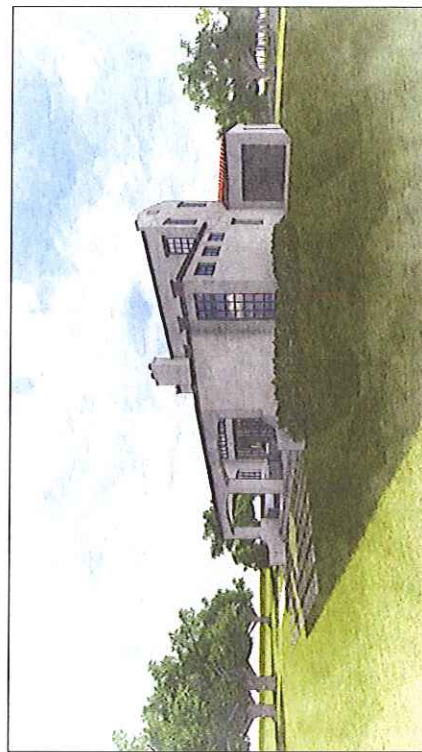
RENDERINGS

SHEET:

6



FRONT PERSPECTIVE



REAR PERSPECTIVE



LEFT LAKEFRONT



RIGHT LAKEFRONT