Planning & Zoning Board
Regular Meeting

January 5, 2021 at 6:00 p.m.

721 W. New England Avenue or Hybrid Virtual Meeting
Winter Park, Florida
https://cityofwinterpark.org/government/live-broadcasts/

Agenda Items

1. Call to Order

2. Approval of December 1, 2020 meeting minutes

3. Public Hearings

- SPR #20-18 Request of Native Homes, Inc. for approval to construct a new two-story single-family home at 530 Country Club Drive on Lake Killarney.

- SPR #20-14 Request of Lazarus Development Group, LLC for approval to construct a new two-story home at 520 Country Club Drive on Lake Killarney.

- CPA #20-06; RZ #20-06; CU #20-07 Request of Winter Park Historic Hotels Group to vacate portions of Killarney Drive and Fairview Avenue, and in return, creating open space/park areas, amend the Comprehensive Plan Future Land Use policy text to add a new policy for this specific property, amend the Zoning Code Commercial (C-3) and Open Space Recreational (PR) text to provide for the allowances above for only this specific property, amend the Future Land Use/Zoning Map from Single Family (R-1A) and Office on 2.09 acres of this site to Commercial (C-3) and 0.88 acres to Open Space Recreational (PR), and Conditional Use approval for the specific hotel project of a 5-story, 132-room hotel with associated restaurant, ballroom/meeting space, and 235-space parking garage with one floor of underground parking.
  FOR SUPPORTING DOCUMENT LETTERS PLEASE VISIT:
  https://cityofwinterpark.org/docs/government/boards/agendas/PZB-agd-2021-01-05-los.pdf

- ANNEX #21-01; CPA #21-01; RZ #21-01; CU #21-01 Request of the Magruder Eye Institute to annex the properties at 279/283 Orange Terrace Drive; 450/460/470 Cambridge Blvd. and 2310 Devon Court; to amend the Comprehensive Plan and Official Zoning Map to establish Parking Lot future land use and Parking Lot (PL) zoning on those annexed properties and for Conditional Use approval to construct a two-story, 32,000 sq. ft. medical office building on these properties combined with 2245 W. Fairbanks Avenue.

- ZTA# 20-08 Request of the City of Winter Park for: An Ordinance amending Article III, Zoning of Chapter 58 of the Land Development Code providing regulations electric vehicle charging stations in multi-family and non-residential parking lots and parking garages.

- CPA #21-02; RZ #21-02 Request to establish Commercial future land use and Commercial (C-3) zoning on seven lots annexed into the City at **2269/2267/2265/2263/6221/2259/2257** Kentucky Avenue.
  THIS ITEM HAS BEEN CONTINUED TO A FUTURE DATE. PLEASE CHECK HTTPS://CITYOFWINTERPARK.ORG/GOVERNMENT/BOARD-PUBLIC-MEETINGS/ FOR FUTURE UPDATES.

Agenda Items

4. New Business

5. Planning Director's Report

6. Board Updates & Comments

7. Upcoming Meeting Schedule

Next P&Z Work Session: Tuesday, January 26, 2020 at 12:00 p.m.

Next P&Z Regular Meeting: Tuesday, February 2, 2021 at 6:00 p.m.

appeals & assistance

"If a person decides to appeal any decision made by the Board with respect to any matter considered at such meeting or hearing, he/she will need a record of the proceedings, and that, for such purpose, he/she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based." (F.S. 286.0105).

"Persons with disabilities needing assistance to participate in any of these proceedings should contact the City Clerk's Office (407-599-3277) at least 48 hours in advance of the meeting."

Planning & TRANSPORTATION

401 South Park Avenue • Winter Park, Florida 32789

407-599-3324 • planning@cityofwinterpark.org
cityofwinterpark.org

Planning & Zoning Board
Staff Report for January 5, 2021 Meeting

SPR #20-18. Request of Native Homes, Inc. for: Approval to construct a new, two-story, 5,277 square foot, single-family home located at 530 Country Club Drive on Lake Killarney, zoned R-2.

Background

Native Homes, Inc. is requesting site plan approval to construct a new, two-story, 5,277 square-foot, single-family home at 530 Country Club Drive, which is located on Lake Killarney, and is zoned R-2. This property measures 9,212 square feet and is also known as Lot 8 of the Lake Killarney Shores replat that was approved by the Commission on April 9, 2018. Below is a table summarizing this request in comparison to the R-2 zoning/lakefront lot requirements.

Table 1:

	R-2/Lakefront Lot Requirements	Proposed
Floor Area Ratio	Max 55%	4,560 square feet/49.5%*
Impervious Lot Coverage	Max 65%	3,894 square feet/42%
Lakefront Setback	72.3 ft. avg./50 ft. min.	70 feet

*Total size is 5,277 square feet but the FAR excludes entry/balcony and outdoor living areas.

Lakefront Lot Review Criteria:

Tree Preservation

The purpose and intent of the lakefront lots section of the code states that existing trees shall be preserved to the degree reasonably possible, and the appearance of the property and the shore when viewed from the water will be kept as natural as reasonably possible. As there are currently no trees located on this lot, the applicant is not proposing to remove any trees as part of this request.

View From the Lake

The code limits walls and swimming pool decks facing the lake in excess of three feet in height. The issues that staff typically deals with about grades and pool decks/retaining walls are not present in this case due to the fact that the lot is relatively flat. The existing grade is 87.3 feet while the grade of the pool deck is 87.7 feet (0.4-foot difference), well below the three-foot height maximum.

View of Neighbors

Another purpose of this review is to ensure that the views of the lake from adjoining properties will not be unduly impaired by new houses, additions, second story additions, etc. In order to achieve this, the code states that the lakefront setback shall be the average established by the adjacent lakefront properties within 200 feet of the subject property, or a 50-foot setback, whichever is greater. However, the P&Z Board does have the authority to approve setbacks less than the average determined, or impose more restrictive setbacks.

During the replat of this Lake Killarney Shores subdivision, there was significant discussion about the lakefront views of the adjoining neighbors to this subdivision at both the P&Z Board and City Commission meetings. At the Commission meeting, it was decided that the lakefront average setback will be established by the setback of the existing homes along the lake within 200 feet of each request (even though they have subsequently been demolished).

Staff has determined that the average lakefront setback for this lot (known as Lot 8 of the attached plat) is approximately 72.3 feet from the lake (see attached analysis). The applicant/builder is proposing a 70-foot setback from the lake to the home, which is slightly over the average setback determined by staff. Additionally, a 50-foot setback to the pool deck is requested.

The applicant has provided a letter signed by the developer/owner of the Lake Killarney replat properties nearest to this lot and also signed by the nearest affected (non-developer owned property) neighbor to this lot (owner of 518/612 Country Club Drive), that states an agreed-upon building setback of 70 feet from the lake, and a 50-foot pool/deck setback. Staff does not see a concern with the plan as submitted.

Stormwater Retention

The code requires retention of stormwater so that stormwater flowing over a waterfront lawn does not carry any fertilizers, herbicides, or other chemicals that upon entrance into the canal or chain of lakes that may affect the water quality of our community's most precious assets. The amount of impervious surface on the lot determines the depth/size of the retention needed. The applicant is proposing a swale retention area along the lakefront that will meet all stormwater retention criteria.

Summary

The applicant is proposing a home that is compatible with the natural grade of the property, is ensuring that water pollution from stormwater runoff and other sources will be minimized, and is ensuring that the appearance of the property and the shore when viewed from the water, will be kept as natural as reasonably possible. Overall, the plans meet the intent of the canal front review criteria defined in the code and no variances are requested.

Staff recommendation is for approval.

LOCATION MAP

530 Country Club Drive

City of Winter Park
Florida

LOCATION MAP

530 Country Club Drive

City of Winter Park
Florida

Existing Buildings Inside and Within 200' of Killarney Shores

Property Address	Existing Setback (ft)
638 Country Club Drive	52
632 Country Club Drive	50
624/626 Country Club Drive	51.8
618/620 Country Club Drive	75.6
612 Country Club Drive	79.7
604/606 Country Club Drive	71.4
600/602 Country Club Drive	55
532/534 Country Club Drive	85.8
524 Country Club Drive	99.5
518 Country Club Drive	85.5
605 Lakefront Boulevard	50
595 Lakefront Boulevard	45

Existing Lakefront Setback Averages within 200' Per Lot

Lot 1

Property Address	Existing Setback (ft)
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
624/626 Country Club Drive	51.8
632 Country Club Drive	50
638 Country Club Drive	52
Average	**61.82**

Lot 2

Property Address	Existing Setback (ft)
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
624/626 Country Club Drive	51.8
632 Country Club Drive	50
638 Country Club Drive	52
Average	**63.4**

Lot 3

Property Address	Existing Setback (ft)
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
624/626 Country Club Drive	51.8
632 Country Club Drive	50
Average	**65.7**

Lot 4

Property Address	Existing Setback (ft)
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
Average	**70.4**

Lot 5

Property Address	Existing Setback (ft)
524 Country Club Drive	99.5
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
Average	**76.2**

Lots 6 & 7

Property Address	Existing Setback (ft)
518 Country Club Drive	85.5
524 Country Club Drive	99.5
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
Average	**78.2**

Lot 8

Property Address	Existing Setback (ft)
605 Lakefront Boulevard	50
518 Country Club Drive	85.5
524 Country Club Drive	99.5
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
Average	**72.3**

Lot 9

Property Address	Existing Setback (ft)
605 Lakefront Boulevard	50
518 Country Club Drive	85.5
532/534 Country Club Drive	85.8
600/602 Country Club Drive	55
595 Lakefront Boulevard	45
Average	**64.3**

LAKE KILLARNEY SHORES

BEING A REPLAT OF PORTIONS OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST CITY OF WINTER PARK, ORANGE COUNTY, FLORIDA

LEGAL DESCRIPTION:

PARCELS OF LAND BEING A PORTION OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, LYING WITHIN SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

(1ST DESCRIBED)

BEGIN AT A IRON PIPE (NO ID) MARKING THE NORTHEAST CORNER OF THAT PARTICULAR PROPERTY DESCRIBED IN OFFICIAL RECORDS BOOK 6449, PAGE 2972, (FOR A POINT OF REFERENCE, SAID POINT IS THE FOLLOWING THREE (3) COURSES FROM THE NORTHEAST CORNER OF SAID LOT 1, LORD'S SUBDIVISION: (1) SOUTH 88°48'25"WEST, A DISTANCE OF 192.85 FEET (2) SOUTH 00°08'22"EAST, A DISTANCE OF 270.14 FEET, AND (3) NORTH 88°48'25"EAST, A DISTANCE OF 165.00 FEET). THENCE, FROM SAID **POINT OF BEGINNING**, RUN SOUTH 00°51'00"EAST, ALONG THE WESTERLY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOK 4184, PAGE 1708 AND BOOK 10684, PAGE 7897, FOR A DISTANCE OF 115.36 FEET; THENCE RUN SOUTH 00°00'00"EAST, ALONG THE WESTERLY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOKS 10684, PAGE 7897; BOOK 7711, PAGE 4825; AND BOOK 9605, PAGE 2646, FOR A DISTANCE OF 199.59 FEET; THENCE CONTINUE SOUTH 20°28'36"EAST, ALONG SAID WESTERLY LINE, FOR A DISTANCE OF 12.83 FEET; THENCE CONTINUE SOUTH 69°08'54"WEST, FOR A DISTANCE OF 4.80 FEET; THENCE RUN SOUTH 00°00'00"EAST ALONG THE WESTERLY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOKS 9605, PAGE 2646; BOOK 10658, PAGE 3768; BOOK 10450, PAGE 32; BOOK 9838, PAGE 8700; AND BOOK 9247, PAGE 4865, FOR A DISTANCE OF 506.96 FEET TO A POINT ON THE NORTH LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 4425, PAGE 366; THENCE RUN NORTH 89°32'17"WEST, ALONG SAID NORTH PROPERTY LINE, FOR A DISTANCE OF 27.80 FEET TO THE NORTHWEST CORNER OF SAID PROPERTY; THENCE SOUTH 00°01'28"WEST, ALONG THE WESTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 119.05 FEET TO A POINT ON THE NORTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378 (KNOWN AS COUNTRY CLUB DRIVE); THENCE RUN ALONG THE NORTHERLY AND EASTERLY LINES OF SAID PROPERTY (COUNTRY CLUB DRIVE) THE FOLLOWING EIGHT (8) COURSES: (1) NORTH 82°56'13"WEST, FOR A DISTANCE OF 43.61 FEET; (2) NORTH 67°02'46"WEST, FOR A DISTANCE OF 32.85 FEET; (3) NORTH 44°29'39"WEST, FOR A DISTANCE OF 29.31 FEET; (4) NORTH 19°26'25"WEST, FOR A DISTANCE OF 36.63 FEET; (5) NORTH 10°13'52"WEST, FOR A DISTANCE OF 160.05 FEET; (6) NORTH 00°07'50" EAST, FOR A DISTANCE OF 260.46 FEET; (7) NORTH 21°40'44"WEST, FOR A DISTANCE OF 80.00 FEET; (8) NORTH 19°29'40"WEST, FOR A DISTANCE OF 100.16 FEET TO THE MOST SOUTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9750, PAGE 4747; THENCE DEPARTING SAID EASTERLY PROPERTY LINE (COUNTRY CLUB DRIVE), RUN NORTH 63°30'30"EAST, ALONG THE SOUTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9750, PAGE 4747, FOR A DISTANCE OF 150.00 FEET TO THE SOUTHEAST CORNER OF SAID PROPERTY; THENCE NORTH 24°01'54"WEST, ALONG THE EASTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 138.85 FEET TO THE MOST NORTHERLY CORNER OF SAID PROPERTY; THENCE NORTH 55°10'58"WEST, FOR A DISTANCE OF 23.45 FEET TO A POINT ON THE EAST LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 2925, PAGE 1746; THENCE RUN NORTH 00°08'22"WEST, ALONG THE EAST PROPERTY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOK 2925, PAGE 1746 AND OFFICIAL RECORDS BOOK 10658, PAGE 4875, FOR A DISTANCE OF 83.56 FEET TO THE SOUTHWEST CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9126, PAGE 2378 (KNOWN AS ELLEN DRIVE), SAID POINT DESIGNATED HEREIN AS REFERENCE POINT 'A'; THENCE RUN NORTH 88°48'25"EAST, ALONG THE SOUTH LINE OF SAID PROPERTY (ELLEN DRIVE) AND THE SOUTH LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORD BOOK 10056, PAGE 9292, FOR A DISTANCE OF 165.00 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 145,116 SQUARE FEET OR 3.331 ACRES, MORE OR LESS.

ALONG WITH (2ND DESCRIBED):

COMMENCING AT THE AFOREMENTIONED REFERENCE POINT 'A', BEING THE SOUTHWEST CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9126, PAGE 2378 (KNOWN AS ELLEN DRIVE); THENCE RUN SOUTH 63°27'11"WEST, FOR A DISTANCE OF 75.47 FEET TO THE MOST NORTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 10948, PAGE 2596, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, FOR A **POINT OF BEGINNING**, THENCE RUN SOUTH 48°02'57"WEST, ALONG THE NORTHWESTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 135.24 FEET TO THE NORTHEASTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9126, PAGE 2378 (KNOWN AS COUNTRY CLUB DRIVE), SAID POINT DESIGNATED HEREIN AS REFERENCE POINT 'B'; THENCE NORTH 49°27'42"WEST, ALONG SAID NORTHEASTERLY PROPERTY LINE (COUNTRY CLUB DRIVE), FOR A DISTANCE OF 129.41 FEET, TO THE MOST SOUTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9744, PAGE 4872; THENCE NORTH 47°05'24"EAST, ALONG THE SOUTHEASTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 158.91 FEET TO THE MOST EASTERLY CORNER OF SAID PROPERTY, SAID POINT ALSO LYING ON THE WESTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 10658, PAGE 4875; THENCE SOUTH 37°51'42"EAST, ALONG SAID WESTERLY PROPERTY LINE, FOR A DISTANCE OF 112.20 FEET; THENCE SOUTH 47°41'20"EAST, CONTINUING ALONG SAID WESTERLY PROPERTY LINE, FOR A DISTANCE OF 19.19 FEET, TO THE **POINT OF BEGINNING**.

CONTAINING 18,880 SQUARE FEET, OR 0.433 ACRES, MORE OR LESS.

ALONG WITH (3RD DESCRIBED):

COMMENCING AT A 3/4-INCH IRON PIPE (NO ID) BEING THE AFOREMENTIONED REFERENCE POINT 'B', THENCE RUN SOUTH 53°05'10" WEST, CROSSING SAID COUNTRY CLUB DRIVE (AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378), FOR A DISTANCE OF 37.19 FEET, TO A POINT ON THE SOUTHWESTERLY LINE OF SAID PARCEL AND THE **POINT OF BEGINNING**, SAID POINT ALSO BEING THE MOST NORTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 6642, PAGE 2836; THENCE RUN SOUTH 53°00'00"WEST, ALONG THE NORTHERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 164.27 FEET, MORE OR LESS, TO THE ORDINARY HIGH WATER LINE OF LAKE KILLARNEY; THENCE RUN NORTHWESTERLY ALONG SAID ORDINARY HIGH WATER LINE, THE FOLLOWING TWELVE (12) COURSES: (1) NORTH 27°05'50"WEST, FOR A DISTANCE OF 13.65 FEET; (2) NORTH 34°46'51"WEST, FOR A DISTANCE OF 18.32 FEET; (3) NORTH 32°27'29"WEST, FOR A DISTANCE OF 15.62 FEET; (4) NORTH 47°13'16"WEST, FOR A DISTANCE OF 33.98 FEET; (5) NORTH 43°42'48"WEST, FOR A DISTANCE OF 8.91 FEET; (6) NORTH 20°25'43"WEST, FOR A DISTANCE OF 4.18 FEET; (7) NORTH 05°55'13"WEST, FOR A DISTANCE OF 5.45 FEET; (8) NORTH 16°07'19"WEST, FOR A DISTANCE OF 4.13 FEET; (9) NORTH 66°35'18"WEST, FOR A DISTANCE OF 3.37 FEET; (10) NORTH 51°55'47"WEST, FOR A DISTANCE OF 21.23 FEET; (11) NORTH 53°20'21"WEST, FOR A DISTANCE OF 15.41 FEET; (12) NORTH 58°23'34"WEST, FOR A DISTANCE OF 9.98 FEET TO A POINT ON THE SOUTHEASTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 7445, PAGE 911; THENCE NORTH 38°11'00"EAST, ALONG SAID SOUTHEASTERLY PROPERTY LINE FOR A DISTANCE OF 142.84 FEET, MORE OR LESS, TO A POINT ON THE SOUTHWESTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378 (KNOWN AS COUNTRY CLUB DRIVE);THENCE SOUTH 48°42'32"EAST, ALONG SAID SOUTHWESTERLY LINE, FOR A DISTANCE OF 190.16 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 25,008 SQUARE FEET, OR 0.574 ACRES, MORE OR LESS.

ALONG WITH (4TH DESCRIBED):

COMMENCING AT A 3/4-INCH IRON PIPE (NO ID) BEING THE AFOREMENTIONED REFERENCE POINT 'B', THENCE RUN SOUTH 53°05'10" WEST, CROSSING SAID COUNTRY CLUB DRIVE (AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378), FOR A DISTANCE OF 37.19 FEET, TO A POINT ON THE SOUTHWESTERLY LINE OF SAID PARCEL; THENCE SOUTH 43°49'00"EAST, ALONG SAID SOUTHWESTERLY PROPERTY LINE, FOR A DISTANCE OF 102.04 FEET; THENCE SOUTH 22°14'49"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 33.39 FEET TO THE **POINT OF BEGINNING**; THENCE RUN SOUTH 22°14'49"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 180.00 FEET TO AN ANGLE BREAK; THENCE SOUTH 26°43'08"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 45.70 FEET TO AN ANGLE BREAK; THENCE SOUTH 02°25'21"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 124.20 FEET TO A POINT ON THE NORTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 6538, PAGE 4203; THENCE DEPARTING SAID RIGHT OF WAY LINE, RUN NORTH 90°00'00"WEST, ALONG THE NORTHERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 186.31 FEET, MORE OR LESS, TO THE ORDINARY HIGH WATER LINE OF LAKE KILLARNEY; THENCE RUN NORTHWESTERLY ALONG SAID ORDINARY HIGH WATER LINE THE FOLLOWING SIXTEEN (16) COURSES: (1) NORTH 14°46'36"EAST, FOR A DISTANCE OF 26.86 FEET; (2) NORTH 04°48'08"EAST, FOR A DISTANCE OF 13.31 FEET; (3) NORTH 02°52'30"WEST, FOR A DISTANCE OF 11.03 FEET; (4) NORTH 10°29'47"WEST, FOR A DISTANCE OF 8.06 FEET; (5) NORTH 22°47'37"WEST, FOR A DISTANCE OF 12.48 FEET; (6) NORTH 01°03'57"WEST, FOR A DISTANCE OF 10.96 FEET; (7) NORTH 13°32'39" WEST, FOR A DISTANCE OF 15.09 FEET; (8) NORTH 18°51'49"WEST, FOR A DISTANCE OF 40.94 FEET; (9) NORTH 25°09'04"WEST, FOR A DISTANCE OF 14.95 FEET; (10) NORTH 30°03'30"WEST, FOR A DISTANCE OF 23.57 FEET; (11) NORTH 32°19'44"WEST, FOR A DISTANCE OF 24.01 FEET; (12) NORTH 20°34'10"WEST, FOR A DISTANCE OF 19.67 FEET; (13) NORTH 24°51'44"WEST, FOR A DISTANCE OF 29.29 FEET; (14) NORTH 23°52'10"WEST, FOR A DISTANCE OF 17.25 FEET; (15) NORTH 31°28'23"WEST, FOR A DISTANCE OF 24.16 FEET; (16) NORTH 27°05'50"WEST, FOR A DISTANCE OF 6.15 FEET TO A POINT ON THE SOUTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 6642, PAGE 2836; THENCE RUN NORTH 71°31'12"EAST, ALONG SAID SOUTHERLY PROPERTY LINE, FOR A DISTANCE OF 190.51 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 58,782 SQUARE FEET, OR 1.349 ACRES, MORE OR LESS.

FOR AN AGGREGATE TOTAL OF 247,786 SQUARE FEET, OR 5.688 ACRES, MORE OR LESS.

TOGETHER WITH THAT CERTAIN EASEMENT RECORDED IN O.R. BOOK 3282, PAGE 2096, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA.

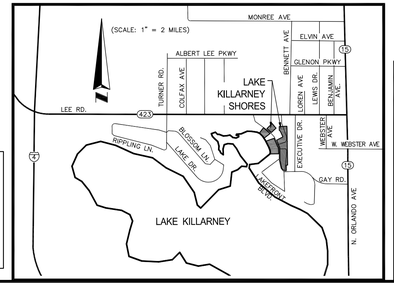
AND

TOGETHER WITH THAT DECLARATION OF ACCESS AND UTILITY EASEMENT RECORDED IN O.R. BOOK 7464, PAGE 1949, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA.

SURVEYOR'S NOTES:

1. THE BEARINGS SHOWN HEREON ARE BASED ON A PORTION OF THE MONUMENTED WESTERLY LINE OF COUNTRY CLUB DRIVE (SEE MAP SHEET 2 OF 3), PER OFFICIAL RECORDS BOOK 9126, PAGE 2378, AS HAVING A BEARING OF S 48°42'32" E, AND AS PER SURVEY MAP PROVIDED BY THE CITY OF WINTER PARK, PREPARED BY: HENRICH, TROTTER, CARTER & AYERS, INC., DATED 12/17/99, CERTIFIED BY: G.L.CARTER, REGISTERED LAND SURVEYOR NO. 2435, DATE: 5/15/01.
2. ALL PLATTED UTILITY EASEMENTS SHALL ALSO BE EASEMENTS FOR THE CONSTRUCTION, INSTALLATION, MAINTENANCE, AND OPERATION OF CABLE TELEVISION SERVICES PROVIDED, HOWEVER, NO SUCH CONSTRUCTION, INSTALLATION, MAINTENANCE, AND OPERATION OF CABLE TELEVISION SERVICES SHALL INTERFERE WITH THE FACILITIES AND SERVICES OF AN ELECTRIC, TELEPHONE, GAS OR OTHER PUBLIC UTILITY. IN THE EVENT A CABLE TELEVISION COMPANY DAMAGES THE FACILITIES OF A PUBLIC UTILITY, IT SHALL BE SOLELY RESPONSIBLE FOR THE DAMAGES. THIS SECTION SHALL NOT APPLY TO THOSE PRIVATE EASEMENTS GRANTED TO OR OBTAINED BY A PARTICULAR ELECTRIC, TELEPHONE, GAS OR OTHER PUBLIC UTILITY. SUCH CONSTRUCTION, INSTALLATION, MAINTENANCE, AND OPERATION SHALL COMPLY WITH THE NATIONAL ELECTRICAL SAFETY CODE AS ADOPTED BY THE FLORIDA PUBLIC SERVICE COMMISSION. (SECTION 177.091 (28), FLORIDA STATUTES).
3. DRAINAGE EASEMENTS SHOWN HEREON ARE HEREBY DEDICATED TO THE LAKE KILLARNEY SHORES HOMEOWNERS' ASSOCIATION, INC. FOR THE OPERATION, MAINTENANCE AND REPAIR OF THE SUBDIVISION'S STORMWATER MANAGEMENT AND DRAINAGE SYSTEM.
4. UTILITY EASEMENTS SHOWN HEREON ARE HEREBY DEDICATED TO THE CITY OF WINTER PARK AND TO OTHER PUBLIC UTILITY SERVICE PROVIDERS FOR THE OPERATION, INSTALLATION, MAINTENANCE AND REPAIR OF UTILITIES OF EVERY TYPE. THE CITY OF WINTER PARK SHALL ONLY BE RESPONSIBLE FOR THE MAINTENANCE OF UTILITIES IT INSTALLS OR ACCEPTS WITHIN ANY OF THE UTILITY EASEMENTS DEPICTED ON THIS PLAT, AND THE CITY SHALL NOT HAVE ANY RESPONSIBILITY FOR MAINTENANCE OF THE EASEMENTS FOR ANY OTHER PURPOSE, INCLUDING IN REGARD TO THE SURFACE IMPROVEMENTS, SOD AND LANDSCAPING AND UTILITIES OWNED BY OTHERS. THE CITY OF WINTER PARK'S UTILITY EASEMENT RIGHTS SHALL BE SUPERIOR TO THAT OF OTHER UTILITY OWNERS IN THE EVENT OF A CONFLICT.
5. THE CITY OF WINTER PARK AND THE ST. JOHNS RIVER WATER MANAGEMENT DISTRICT SHALL HAVE THE RIGHT, BUT NOT THE OBLIGATION, TO ACCESS, MAINTAIN, REPAIR, REPLACE OR OTHERWISE CARE FOR OR CAUSE TO BE CARED FOR, TRACT "A" AND DRAINAGE EASEMENTS INCLUDING, WITHOUT LIMITATION THE DRAINAGE SYSTEMS CONSTRUCTED THEREON. A BLANKET INGRESS/EGRESS EASEMENT IS GRANTED IN FAVOR OF THE CITY OF WINTER PARK AND/OR THE ST. JOHNS RIVER WATER MANAGEMENT DISTRICT FOR SAID PURPOSE OVER SAID DRAINAGE EASEMENTS, AND OVER TRACT "A".
6. SIDEWALK EASEMENTS SHOWN HEREON ARE HEREBY DEDICATED TO THE CITY OF WINTER PARK FOR THE OPERATION, REPAIR AND MAINTENANCE OF PUBLIC SIDEWALKS AND FOR THE GENERAL PUBLIC'S PEDESTRIAN ACCESS AND USE OF SIDEWALK IMPROVEMENTS THERIN. THE CITY OF WINTER PARK SHALL ONLY BE RESPONSIBLE FOR THE MAINTENANCE OF THE SIDEWALK IMPROVEMENTS IT INSTALLS OR ACCEPTS WITHIN ANY OF THE SIDEWALK EASEMENTS DEPICTED ON THIS PLAT, AND THE CITY SHALL NOT HAVE ANY RESPONSIBILITY FOR MAINTENANCE OF THE EASEMENTS FOR ANY OTHER PURPOSE.
7. LOT OWNERS SHALL BE RESPONSIBLE TO MAINTAIN THEIR LOTS AS SET FORTH IN THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS FOR LAKE KILLARNEY.
8. TRACT "E" (PUBLIC RIGHT-OF-WAY), AN EXTENSION OF THE ELLEN DRIVE RIGHT-OF-WAY, IS HEREBY DEDICATED TO THE CITY OF WINTER PARK FOR THE OPERATION, REPAIR AND MAINTENANCE OF THE RIGHT-OF-WAY FOR THE GENERAL PUBLIC'S VEHICULAR AND PEDESTRIAN ACCESS AND USE OF THE ROADWAY IMPROVEMENTS THEREON.
9. THIS SUBDIVISION IS SUBJECT TO AND GOVERNED BY THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS FOR LAKE KILLARNEY SHORES.
10. TRACT "A" (STORMWATER MANAGEMENT) AND TRACT "B" (LAKE ACCESS) SHALL BE OWNED AND MAINTAINED BY THE LAKE KILLARNEY SHORES HOMEOWNERS' ASSOCIATION, INC. FOR THE BENEFIT OF THE OWNERS OF LOTS 1-30 AND FOR THE OTHER PURPOSES AS SET FORTH IN THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS TO BE RECORDED IN THE PUBLIC RECORDS. A NON-EXCLUSIVE DRAINAGE EASEMENT IS HEREBY GRANTED TO THE CITY OF WINTER PARK OVER, UNDER, AND THROUGH TRACT "A" AND THE DRAINAGE EASEMENTS ADJACENT TO THE PUBLIC RIGHTS-OF-WAY. THE CITY OF WINTER PARK SHALL NOT HAVE ANY OPERATION, MAINTENANCE AND REPAIR OBLIGATIONS CONCERNING TRACT "A", TRACT "B" OR ANY DRAINAGE EASEMENTS DEDICATED HEREON.
11. EACH DRAINAGE EASEMENT AREA ON LOTS 1-11 AND 26-30 MUST BE MAINTAINED BY THE INDIVIDUAL LOT OWNERS.
12. THE CITY OF WINTER PARK AND ITS OFFICERS, EMPLOYEES AND AGENTS ARE HEREBY DEDICATED A NON-EXCLUSIVE, PERPETUAL EASEMENT OVER AND THROUGH TRACT "B" (20' WIDE LAKE ACCESS) AND IMPROVEMENTS THEREON FOR PEDESTRIAN, VEHICULAR AND BOAT ACCESS TO AND FROM LAKE KILLARNEY AND PUBLIC RIGHTS-OF-WAY FOR MAINTENANCE, SEARCH AND RESCUE, PATROLLING AND RELATED MATTERS. THIS DOES NOT CREATE A PUBLIC RIGHT OF USE AND ACCESS IN TRACT "B". THE CITY SHALL HAVE NO MAINTENANCE AND REPAIR OBLIGATIONS CONCERNING TRACT "B".
13. DOCKS/BOATHOUSES SHALL BE LOCATED ONLY WHERE INDICATED ON THIS PLAT (BY ASTERISK SYMBOL, SEE LEGEND). PURSUANT TO CITY COMMISSION APPROVAL, SUCH DOCKS/BOATHOUSES MAY BE LOCATED WITH ZERO SIDE SETBACKS AND NO MORE THAN THREE FEET. SIZE, HEIGHT AND DIMENSIONS SHALL BE PERMITTED BY THE CITY OF WINTER PARK LAKES AND WATERWAYS BOARD.
14. ACCESS EASEMENT TRACT "C" AND "D" SHALL BE CONVEYED TO THE LAKE KILLARNEY SHORES HOMEOWNER'S ASSOCIATION, INC. AND OPERATED, REPAIRED AND MAINTAINED FOR INGRESS AND EGRESS TO AND FROM CERTAIN LOTS AND THE PUBLIC RIGHT-OF-WAY AS FURTHER SET FORTH IN THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS TO BE RECORDED IN THE PUBLIC RECORDS.
15. THE DEDICATOR/PROPERTY OWNER FOR ITSELF AND ITS SUCCESSORS AND ASSIGNS HEREBY DISCLAIMS, RELEASES AND QUIT CLAIMS TO THE CITY OF WINTER PARK AND ANY AND ALL RIGHTS, TITLE AND INTERESTS DEDICATOR/PROPERTY OWNER MAY HAVE IN COUNTRY CLUB DRIVE AND TO ANY GAPS AND GORES THAT MAY EXIST BETWEEN THE PROPERTY BEING SUBDIVIDED BY THIS PLAT AND COUNTRY CLUB DRIVE, WHETHER BEING INSIDE OR OUTSIDE THE BOUNDARIES OF THIS PLAT.
16. REGARDLESS OF THE TRACT AND LOT BOUNDARIES ADJACENT TO LAKE KILLARNEY SHOWN HEREON, THE STATE OF FLORIDA INTERNAL IMPROVEMENT TRUST FUND OWNS TITLE, AS SOVEREIGN LANDS, TO THOSE LANDS OF LAKE KILLARNEY LYING WATERWARD OF THE ORDINARY HIGH WATER LINE (OHWL). ORDINARY HIGH WATER ELEVATION IS BASED ON A PUBLICATION BY CITY OF WINTER PARK, DEPARTMENT OF PUBLIC WORKS (LAKES DIVISION), WHICH LISTS THE ORDINARY HIGH WATER ELEVATION AS: 82.0 NAVD-1988 (82.8 NGVD-1929).
17. ALL SITE CONSTRUCTION AND TREE REMOVAL ACTIVITIES MUST BE IN ACCORDANCE WITH APPLICABLE CITY OF WINTER PARK CODES OF ORDINANCES AND THE CONDITIONS OF APPROVAL FOR THIS PLAT AND ALL OTHER DEVELOPMENT ORDERS AND BUILDING PERMITS ISSUED CONCERNING THIS SUBDIVISION. AMONG OTHER THINGS, THERE ARE SPECIFIC CONDITIONS OF APPROVAL FOR THIS SUBDIVISION CONCERNING BUILDING SETBACKS, TREE PRESERVATION AND RESTRICTING THE REMOVAL OF TREES FROM THE LOTS WHICH SHALL BE BINDING UPON THE LOTS AND MUST BE COMPLIED WITH BY ALL LOT OWNERS, AS SET FORTH IN THE CONDITIONS OF PLAT APPROVAL, WHEN DEEMED NECESSARY BY CITY'S URBAN FORESTRY PERSONNEL IN ORDER TO PRESERVE TREE(S) AND TREE CANOPY, THE BUILDING SETBACK TO AN INTERIOR (SAME SUBDIVISION) PROPERTY LINE CONCERNING LOTS #10, 11, 13, 14, 15, 19, 20 22, 23, 26, AND 28 MAY BE REDUCED TO A 5 FOOT ONE STORY AND 8 FOOT TWO STORY SIDE SETBACK IN LIEU OF THE REQUIRED 7 FOOT ONE STORY AND 10 FOOT TWO STORY SIDE SETBACKS AND A 15 FOOT SECOND STORY REAR IN LIEU OF THE REQUIRED 25 FOOT REAR SETBACK. ALL OTHER LOTS SHALL MEET THE NORMAL R-2 ZONING DISTRICT SETBACK REQUIREMENTS. NO PERMIT FOR REMOVAL OF TREES ON THE "TREES TO BE SAVED" PLAN ON FILE WITH THE CITY WILL BE GRANTED BY THE CITY FOR A PERIOD OF AT LEAST 25 YEARS FROM THE DATE OF RECORDING OF THIS PLAT UNLESS THE CITY ARBORIST DETERMINES THAT THEIR CONDITION, HEALTH OR OTHER NON-ECONOMIC FACTORS WARRANT THEIR REMOVAL.

VICINITY MAP



ASM
AMERICAN SURVEYING & MAPPING INC.
CERTIFICATION OF AUTHORIZATION NUMBER LB#6393
3191 MAGUIRE BOULEVARD, SUITE 200
ORLANDO, FLORIDA 32803
(407) 426-7979

NOTICE

THIS PLAT, AS RECORDED IN ITS GRAPHIC FORM, IS THE OFFICIAL DEPICTION OF THE SUBDIVIDED LANDS DESCRIBED HEREIN AND WILL IN NO CIRCUMSTANCES BE SUPPLANTED IN AUTHORITY BY ANY OTHER GRAPHIC OR DIGITAL FORM OF THE PLAT. THERE MAY BE ADDITIONAL RESTRICTIONS THAT ARE NOT RECORDED ON THIS PLAT THAT MAY BE FOUND IN THE PUBLIC RECORDS OF THIS COUNTY, AS PER 177.091 (27) F.A.C.

SURVEYOR'S CERTIFICATE

I HEREBY CERTIFY THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE HEREON DESCRIBED LAND WHICH WAS RECENTLY SURVEYED AND PLATTED UNDER MY DIRECTION AND SUPERVISION, AND THAT PERMANENT REFERENCE MONUMENTS AND PERMANENT CONTROL POINTS HAVE BEEN SET IN ACCORDANCE WITH CHAPTER 177, FLORIDA STATUTES, AND THAT SURVEY DATA COMPLIES WITH ALL REQUIREMENTS OF CHAPTER 177, FLORIDA STATUTES.

SURVEYOR'S NAME: JAMES D. FLICK, PSM DATE
Registration Number: LS 6088
Florida Registration Number of Legal Entity: LB 6393
3191 Maguire Boulevard, Suite 200, Orlando FL 32803

PLAT BOOK PAGE

LAKE KILLARNEY SHORES DEDICATION

KNOW ALL MEN BY THESE PRESENTS, That the limited liability company named below, being the owner in fee simple of the lands shown hereon, does hereby dedicate said lands and plat for the uses and purposes therein expressed, including as set forth in the Surveyor's Notes.

IN WITNESS WHEREOF, has caused these presents to be signed and attested to by the officers named below on the_____day of_____, 2018.
Owner:
Turner Real Property Investments, LLC,
a Florida limited liability company

BY: _____ _____
TITLE

SIGNED AND SEALED IN THE PRESENCE OF:

_____ _____
SIGNATURE PRINTED NAME

_____ _____
SIGNATURE PRINTED NAME

ACKNOWLEDGEMENT

STATE OF FLORIDA
ORANGE COUNTY

THIS IS TO CERTIFY, That on _____, 2018, before me, an officer duly authorized to take acknowledgments in the State and County aforesaid, personally appeared

_____, _____
(title)

of Turner Real Property Investments, LLC, a limited liability company under the laws of the State of Florida, to me know to be the individual and officer described in and who executed the foregoing dedication and severally acknowledged the execution thereof to be his free act and deed as such officer there unto duly authorized; and that the said dedication for the uses and purposes therein expressed is the act and deed of said limited liability company.

IN WITNESS WHEREOF, I have hereto set my hand and seal on the above date

_____
NOTARY PUBLIC
My Commission Expires _____

CERTIFICATE OF APPROVAL, BY THE CITY OF WINTER PARK

THIS IS TO CERTIFY, That on _____, the City Commission of the City of Winter Park, Florida approved the foregoing plat.

_____
Steve Leary Mayor

Attest:

_____
Cindy Bonham City Clerk

CERTIFICATE OF APPROVAL BY CITY ENGINEER

Examined & Approved: _____ Date _____

City Engineer _____

Florida Registration No: _____

CERTIFICATE OF REVIEW BY CITY SURVEYOR

I have reviewed this plat and find it be in conformity with Chapter 177, Florida Statutes.

_____ _____
Signed Date
Florida Registration Number

CERTIFICATE OF COUNTY COMPTROLLER

I HEREBY CERTIFY that the foregoing plat was recorded in the Orange County Official Records on _____ as File No. _____.

County Comptroller in and for Orange County, Florida

_____

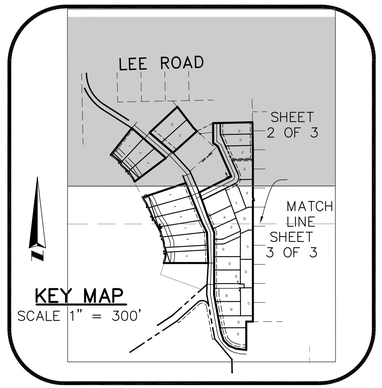
LAKE KILLARNEY SHORES

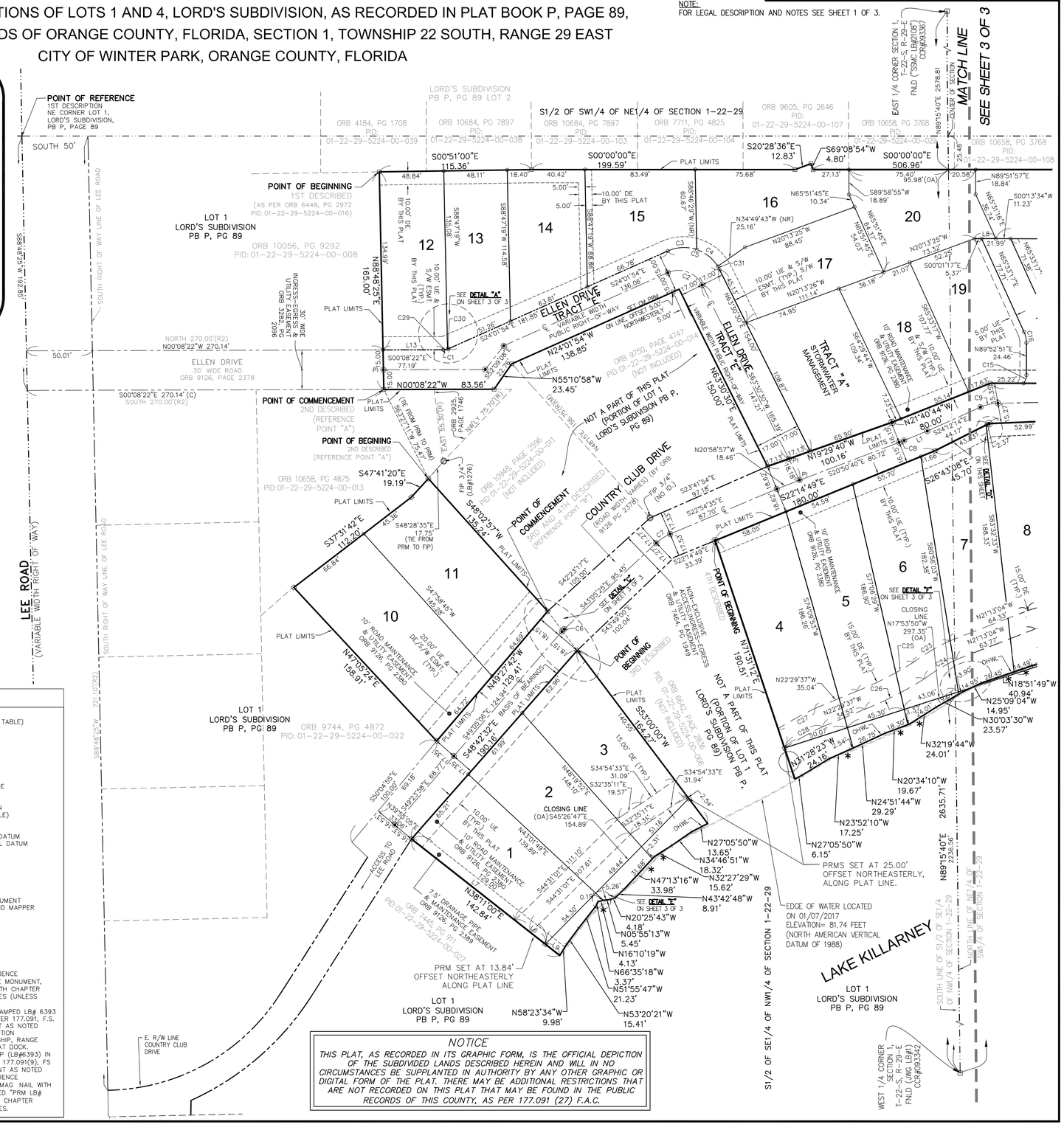
SHEET 2 OF 3

PLAT BOOK ____ PAGE ____

BEING A REPLAT OF PORTIONS OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST CITY OF WINTER PARK, ORANGE COUNTY, FLORIDA

NOTE:
FOR LEGAL DESCRIPTION AND NOTES SEE SHEET 1 OF 3.





Curve Table					
Curve #	Length	Radius	Delta	Chord Bearing	Chord
C1	4.17'	10.00'	023°53'31"	S12°05'08"E	4.14'
C2	30.56'	20.00'	087°32'23"	S19°44'18"W	27.67'
C3	22.46'	30.00'	042°53'42"	S02°35'03"E	21.94'
C4	20.65'	30.00'	039°25'45"	S38°34'40"W	20.24'
C5	45.84'	30.00'	087°32'23"	S19°44'18"W	41.51'
C6	2.61'	25.00'	005°59'07"	N46°06'31"W	2.61'
C7	12.60'	36.00'	020°02'52"	N33°03'59"W	12.53'
C8	4.11'	200.00'	001°10'42"	S21°26'01"E	4.11'
C23	31.78'	160.18'	011°21'59"	N30°43'49"W	31.72'
C24	28.83'	145.18'	011°22'36"	N30°55'42"W	28.78'
C25	47.26'	254.94'	010°37'21"	N27°00'29"W	47.20'
C26	50.50'	269.94'	010°43'06"	N27°04'43"W	50.42'
C27	31.73'	288.20'	006°18'28"	N28°47'38"W	31.71'
C28	33.27'	273.20'	006°58'38"	N29°12'50"W	33.25'
C29	3.24'	10.00'	018°35'05"	S09°25'55"E	3.23'
C30	0.93'	10.00'	005°18'27"	S21°22'40"E	0.93'
C31	2.73'	30.00'	005°12'57"	N60°54'01"E	2.73'

1" = 40'
GRAPHIC SCALE
0 20' 40' 80'

Line Table		
Line #	Direction	Length
L1	S22°01'22"E	18.87'
L5	N86°49'37"W	26.07'
L6	N38°11'00"E	29.39'
L7	N38°11'00"E	99.61'
L8	S00°01'17"E	16.12'
L9	N38°11'00"E	13.84'
L13	S00°08'22"E	45.70'

LEGEND

- (C) CALCULATED DATA
- C# CURVE NUMBER (SEE CURVE TABLE)
- CB CHORD BEARING
- CCR CERTIFIED CORNER RECORD
- CHD CHORD LENGTH
- ℄ CENTER LINE
- CM CONCRETE MONUMENT
- DE DRAINAGE EASEMENT
- ID. IDENTIFICATION
- EW EDGE OF WATER
- F.A.C FLORIDA ADMINISTRATIVE CODE
- FIP FOUND IRON PIPE
- FNLD FOUND NAIL & DISK
- FPC FLORIDA POWER CORPORATON
- L# LINE NUMBER (SEE LINE TABLE)
- LB # LICENSE BUSINESS NUMBER
- LS LICENSED SURVEYOR
- NAVD NORTH AMERICAN VERTICAL DATUM
- NGVD NATIONAL GEODETIC VERTICAL DATUM
- (NR) NON RADIAL
- OHWL ORDINARY HIGH WATER LINE
- ORB OFFICIAL RECORDS BOOK
- PB PLAT BOOK
- PC POINT OF CURVATURE
- PG PAGE
- PI POINT OF INTERSECTION
- PRM PERMANENT REFERENCE MONUMENT
- PSM PROFESSIONAL SURVEYOR AND MAPPER
- PT POINT OF TANGENCY
- (R2) PER ORB 9126, PAGE 2378
- R/W RIGHT OF WAY
- SEC SECTION
- S/W SIDEWALK
- ESMT. EASEMENT
- TYP. TYPICAL
- UE UTILITY EASEMENT
- SET PRM (PERMANENT REFERENCE MONUMENT) 4"X4" CONCRETE MONUMENT, LB#6393 IN ACCORDANCE WITH CHAPTER 177.091(7), FLORIDA STATUTES (UNLESS OTHERWISE NOTED)
- SET PCP NAIL AND DISK, STAMPED LB# 6393 IN ACCORDANCE WITH CHAPTER 177.091, F.S.
- FOUND CONCRETE MONUMENT AS NOTED
- INDICATES CHANGE OF DIRECTION
- 29-27-27 SECTION, TOWNSHIP, RANGE
- PROPOSED LOCATION OF BOAT DOCK.
- SET 1/2" IRON ROD AND CAP (LB#6393) IN ACCORDANCE WITH CHAPTER 177.091(9), FS
- FOUND IRON PIPE MONUMENT AS NOTED
- SET PRM (PERMANENT REFERENCE MONUMENT) 1/8" DIAMETER MAG NAIL WITH 1 1/4" BRASS DISK INSCRIBED "PRM LB# 6393" IN ACCORDANCE WITH CHAPTER 177.091(8), FLORIDA STATUTES.

ASM
AMERICAN SURVEYING & MAPPING INC.
CERTIFICATION OF AUTHORIZATION NUMBER LB#6393
3191 MAGUIRE BOULEVARD, SUITE 200
ORLANDO, FLORIDA 32803
(407) 426-7979

NOTICE

THIS PLAT, AS RECORDED IN ITS GRAPHIC FORM, IS THE OFFICIAL DEPICTION OF THE SUBDIVIDED LANDS DESCRIBED HEREIN AND WILL IN NO CIRCUMSTANCES BE SUPPLANTED IN AUTHORITY BY ANY OTHER GRAPHIC OR DIGITAL FORM OF THE PLAT. THERE MAY BE ADDITIONAL RESTRICTIONS THAT ARE NOT RECORDED ON THIS PLAT THAT MAY BE FOUND IN THE PUBLIC RECORDS OF THIS COUNTY, AS PER 177.091 (27) F.A.C.

LAKE KILLARNEY SHORES

SHEET 3 OF 3

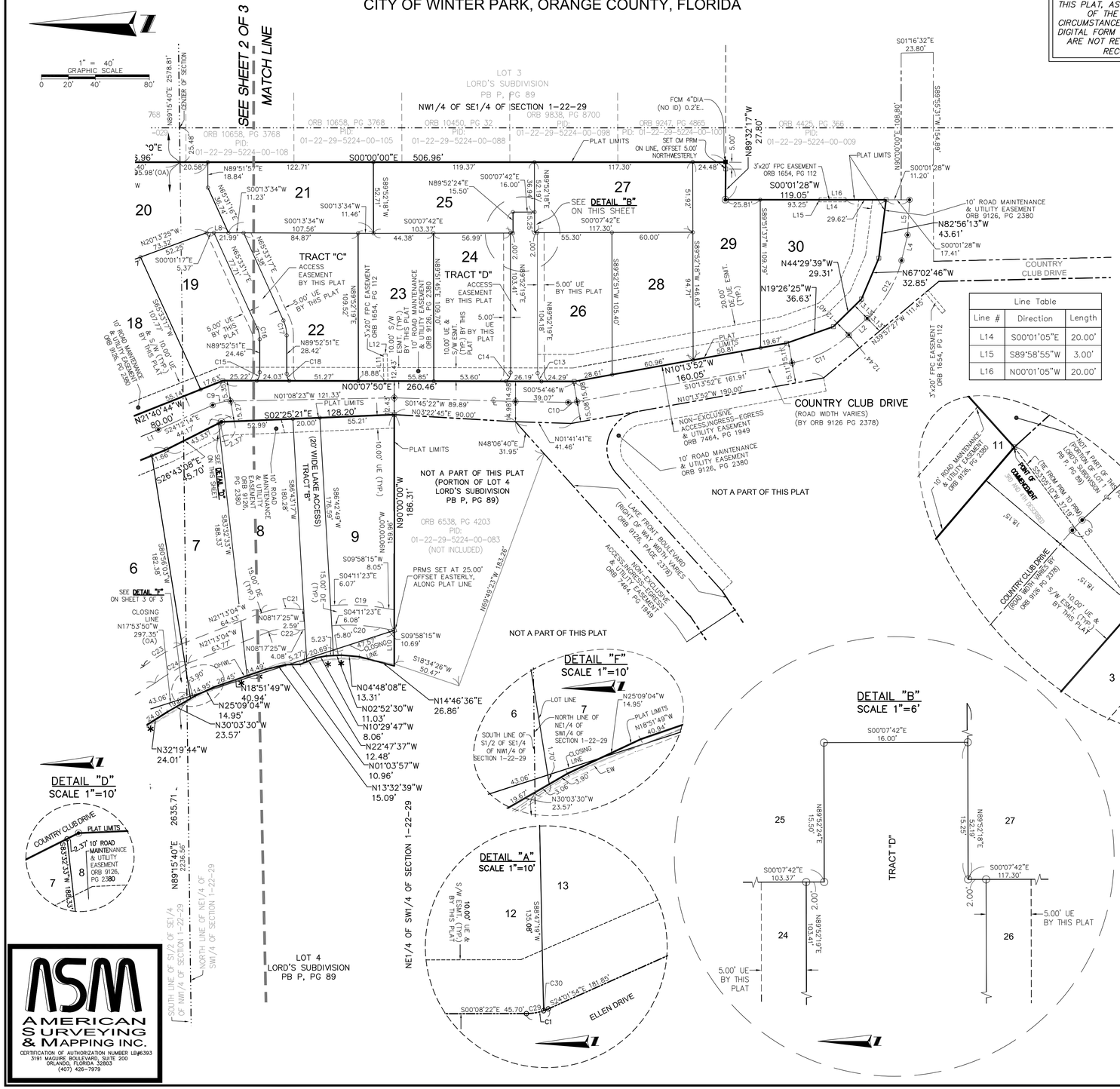
PLAT BOOK ____ PAGE ____

BEING A REPLAT OF PORTIONS OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST CITY OF WINTER PARK, ORANGE COUNTY, FLORIDA

NOTE:
FOR LEGAL DESCRIPTION AND NOTES SEE SHEET 1 OF 3.
FOR LEGEND SEE SHEET 2 OF 3.

NOTICE

THIS PLAT, AS RECORDED IN ITS GRAPHIC FORM, IS THE OFFICIAL DEPICTION OF THE SUBDIVIDED LANDS DESCRIBED HEREIN AND WILL IN NO CIRCUMSTANCES BE SUPPLANTED IN AUTHORITY BY ANY OTHER GRAPHIC OR DIGITAL FORM OF THE PLAT. THERE MAY BE ADDITIONAL RESTRICTIONS THAT ARE NOT RECORDED ON THIS PLAT THAT MAY BE FOUND IN THE PUBLIC RECORDS OF THIS COUNTY, AS PER 177.091 (27) F.A.C.



Curve Table					
Curve #	Length	Radius	Delta	Chord Bearing	Chord
C1	4.17'	10.00'	023°53'31"	S12°05'08"E	4.14'
C6	2.61'	25.00'	005°59'07"	N46°06'31"W	2.61'
C9	13.69'	34.00'	023°03'50"	N12°40'18"W	13.59'
C10	6.61'	34.00'	011°08'38"	S04°39'33"E	6.60'
C11	47.47'	85.00'	031°59'54"	S26°13'49"E	46.86'
C12	51.01'	85.00'	034°22'53"	S59°25'12"E	50.24'
C13	6.97'	7.94'	050°20'50"	S65°09'53"W	6.75'
C14	7.63'	7.99'	054°44'04"	S62°49'27"E	7.35'
C15	6.79'	9.99'	038°54'32"	S70°39'53"E	6.66'
C16	8.49'	20.03'	024°17'24"	N77°43'04"E	8.43'
C17	11.46'	27.00'	024°19'34"	N77°43'04"E	11.38'
C18	5.64'	8.00'	040°25'01"	S70°43'46"W	5.53'
C19	33.33'	152.51'	012°31'13"	N03°44'48"E	33.26'
C20	29.83'	137.51'	012°25'52"	N03°47'28"E	29.78'
C21	29.27'	52.26'	032°05'10"	N04°48'58"W	28.89'
C22	23.19'	37.26'	035°39'47"	N02°57'42"W	22.82'
C29	3.24'	10.00'	018°35'05"	S09°25'55"E	3.23'
C30	0.93'	10.00'	005°18'27"	S21°22'40"E	0.93'

Line Table		
Line #	Direction	Length
L14	S00°01'05"E	20.00'
L15	S89°58'55"W	3.00'
L16	N00°01'05"W	20.00'

Line Table		
Line #	Direction	Length
L2	N42°13'46"W	18.37'
L4	N76°36'39"W	19.06'
L5	N86°49'37"W	26.07'
L6	N38°11'00"E	29.39'
L8	S00°01'17"E	16.12'
L10	N90°00'00"W	26.35'
L11	N88°18'01"E	20.00'
L12	S01°41'59"E	3.00'

AMERICAN SURVEYING & MAPPING INC.
CERTIFICATION OF AUTHORIZATION NUMBER LB#6393
3191 MAGUIRE BOULEVARD, SUITE 200
ORLANDO, FLORIDA 32803
(407) 426-7979

Letter of Agreement

Lake Killarney, LLC (the Developer of Lake Killarney Shores) and David Robold (owner of 612 and 518 Country Club Drive, Winter Park, 32789) agree to the following regarding the setbacks and characteristics of the homes to be built on Lake Killarney as part of the Lake Killarney Shores development:

- Lot 1: Rear Setback 60'
- Lot 2: Rear Setback 61'
- Lot 3: Rear setback of 70' on the side adjacent to 612 Country Club Drive. Front setback reduced to 20'. Pool will be located furthest from 612 Country Club Drive and shall be at an elevation even to or recessed below ground level to avoid obstruction of views from 612 Country Club Drive.
- Lot 4: Rear setback 70'. Front setback reduced to 20'. Pool will be placed furthest from 612 Country Club Drive to avoid obstruction of views from 612 Country Club Drive.
- Lots 5-8: Rear setback of 70'.
- Lot 9: Rear setback of 85'. Front setback reduced to 20'. Pool will be located furthest from 518 Country Club Drive and shall be even to or recessed below ground level to avoid obstruction of views from 518 Country Club Drive.
- Pools can be placed between the agreed rear setback and the 50' lakefront setback.
- Pools on lots 3, 4, 8, and 9 will be at an elevation even to or recessed below ground level as necessary to insure those backyards are not at higher elevation compared to 612 and 518 Country Club Drive.
- Solid fence or privacy fence shall not extend beyond the rear corners of the house indoor living area at rear setback line. Beyond that point, the fence will be see-through picket fence.
- There will be no pool enclosures, cabanas, summer-kitchens, or similar type construction in the pool area or beyond the rear corners of the house indoor living area.
- Underground utilities will be attached to 518 and 612 Country Club Drive at no additional cost.
- All curbing will be replaced along right-of-way.
- Speed humps will be located in right-of-way north of the intersection of Country Club Drive and Lake Front Drive. However, they will not obstruct ingress/egress to the driveways located at 518 and 612 Country Club Drive.
- This agreement is subject to final approval by the Winter Park City Commission.

Date: 9/6/2018

Anil Deshpande
Lake Killarney, LLC
5401 S. Kirkman Road, Suite 640
Orlando, Florida 32819

Date: 9-11-18

David Robold
612 Country Club Drive
Winter Park, Florida 32789

FLORIDA BUILDING CODE, 2017 6TH EDITION. NFPA 70A-02 AND NATIONAL ELECTRICAL CODES BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318-11) SPECIFICATIONS FOR STRUCTURAL CONCRETE BUILDINGS (ACI 301-02) BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES (ACI 530-02) NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION, 2005 EDITION WOOD FRAMED CONSTRUCTION SPECIFICATION MINIMUM DESIGN LOADS ASCE 7-16

NOTE: THIS STRUCTURE HAS BEEN DESIGNED TO MEET OR EXCEED THE WIND LOAD REQUIREMENTS OF THE 2017 FLORIDA BUILDING CODE RESIDENTIAL SIXTH EDITION, SECTION R301 DESIGN CRITERIA AND INCLUDING ASCE 7-10.
1. WIND SPEED = 139 MPH ULTIMATE WIND SPEED (Vult) AND 108 MPH (Vasd)
2. RISK CATEGORY 2
CONSTRUCTION TYPE = SINGLE FAMILY RESIDENCE (V)
3. WIND EXPOSURE = CATEGORY C
4. INTERNAL PRESSURE COEFFICIENT FOR ENCLOSED BUILDINGS IS .18 AND HEIGHT & EXPOSURE ADJUSTMENT COEFFICIENT IS 1.40

THE DUNN RESIDENCE
LOT 8, LAKE KILLARNEY SHORES

ABBREVIATIONS

A.B.	Anchor Bolt
Abv.	Above
A/C	Air-Conditioner
Adj.	Adjustable
A.F.F.	Above Finished Floor
A.H.U.	Air Handler Unit
ALT.	Alternate
B.C.	Base Cabinet
B.F.	Bifold Door
Bk Sh	Book Shelf
Bm.	Beam
BOT.	Bottom
B.P.	Bypass door
Brg.	Bearing
Cir.	Circle
Clg.	Ceiling
CJ	Control Joint
Col.	Column
Comp.	A/C Compressor
C.T.	Ceramic Tile
D	Dryer
Dec.	Decorative
Ded.	Dedicated Outlet
Dbl.	Double
Dia.	Diameter
Disp.	Disposal
Dist.	Distance
D.S.	Drawer Stack
D.V.	Dryer Vent
D.W.	Dishwasher
Ea.	Each
E.W.	Each Way
Elec.	Electrical
Elev.	Elevation
Ext.	Exterior
Exp.	Expansion
F.B.C.	Florida Bldg. Code
Fin. Flr.	Finished Floor
F.G.	Fixed Glass
Flr.	Floor
Fdn.	Foundation
Flr. Sys.	Floor System
F.Pl.	Fireplace
F.O.M.	Face Of Masonry
Ft.	Foot / Feet
Ftg.	Footing
FX	Fixed
Galv.	Galvanized
G.C.	General Contractor
G.F.I.	Ground Fault Interrupter
G.T.	Girder Truss
Hdr.	Header
Hgt.	Height
HB	Hose Bibb
Int.	Interior
K/Wall	Kneewall
K.S.	Knee Space
Laun.	Laundry
Lav.	Lavatory
L.F.	Linear Ft.
L.T.	Laundry Tub
Mas.	Masonry
Max.	Maximum
M.C.	Medicine Cabinet
Mfgr.	Manufacturer
Micro.	Microwave
Min	Minimum
M.L.	Microlam
Mir.	Mirror
Mono	Monolithic
N.T.S.	Not to Scale
Opn'g.	Opening
Opt.	Optional
Pc.	Piece
Ped.	Pedestal
P.L.	Parallam
PLF	Pounds per linear foot
Plt. Ht.	Plate Height
Plt Sh.	Plant Shelf
PSF	Pounds per square foot
P.T.	Pressure Treated
Pwd.	Powder Room
Rad.	Radius
Ref.	Refrigerator
Req'd.	Required
Rm.	Room
Req'd.	Required
Rm.	Room
Rnd.	Round
R/SH	Rod and Shelf
SD.	Smoke Detector
S.F.	Square Ft.
Sh.	Shelves
SHT	Sheet
S.L.	Side Lights
S.P.F.	Spruce Pine Fir
Sq.	Square
S.Y.P.	Southern Yellow Pine
Temp.	Tempered
Thik'n.	Thicken
T.O.B.	Top of Block
T.O.M.	Top of Masonry
T.O.P.	Top of Plate
Trans.	Transom Window
Typ.	Typical
UCL	Under Cabinet Lighting
U.N.O.	Unless Noted Otherwise
VB	Vanity Base
Vert.	Vertical
V.L.	Versalam
VTR	Vent through Roof
W	Washer
W/	With
W/C	Water Closet
W.A.	Wedge Anchor
Wd	Wood
WP	Water Proof

REFERENCE SYMBOLS

2x FRAME WALL

2x FRAME WALL WITH INSULATION

2x BEARING WALL WITH STUDS AT 16" O.C.

CONCRETE BLOCK WALL

2880
(2) 2660 ARCH F.G.
DOOR DESIGNATION

WINDOW DESIGNATION

DETAIL LETTER
SHEET NUMBER
B
A-9
BUILDING SECTION TAG

DETAIL KEY

BUILDING ELEVATION TAG

INDEX OF DRAWINGS

	ARCHITECTURAL
A1	COVER / INDEX SHEET
A2	SITE PLAN
A3	FIRST FLOOR PLAN
A4	SECOND FLOOR PLAN
A5	ELEVATIONS
A6	ELEVATIONS
A7	ROOF PLAN
A8	BUILDING SECTIONS
A9	STAIR SECTIONS
A10	WALL SECTIONS
A11	WALL SECTIONS

	ELECTRICAL
E1	FIRST FLOOR ELECTRICAL
E2	SECOND FLOOR ELECTRICAL

	STRUCTURAL
S1	FOUNDATION PLAN
S2	LINTEL PLAN
S3	LOWER TRUSS LAYOUT
S4	UPPER TRUSS LAYOUT
S5	STRUCTURAL DETAILS
S6	STRUCTURAL DETAILS

REVISION DATE:

ORIGINS RESIDENTIAL DESIGN
68 GRAHAM AVENUE
OVIEDO, FL 32765
407-365-1125
ORIGINS RESIDENTIAL DESIGN

THIS ITEM HAS BEEN ELECTRONICALLY SEALED BY CHARLES R. ADAMS & ASSOCIATES USING A DIGITAL SIGNATURE. PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

NATIVE HOMES INC.
2408 FORFARSHIRE DR.
WINTER PARK, FL. 32792
(407)740-5688

DUNN RESIDENCE
LOT 8
LAKE KILLARNEY SHORES

DRAWN BY:
AP

DATE:
12-14-2020

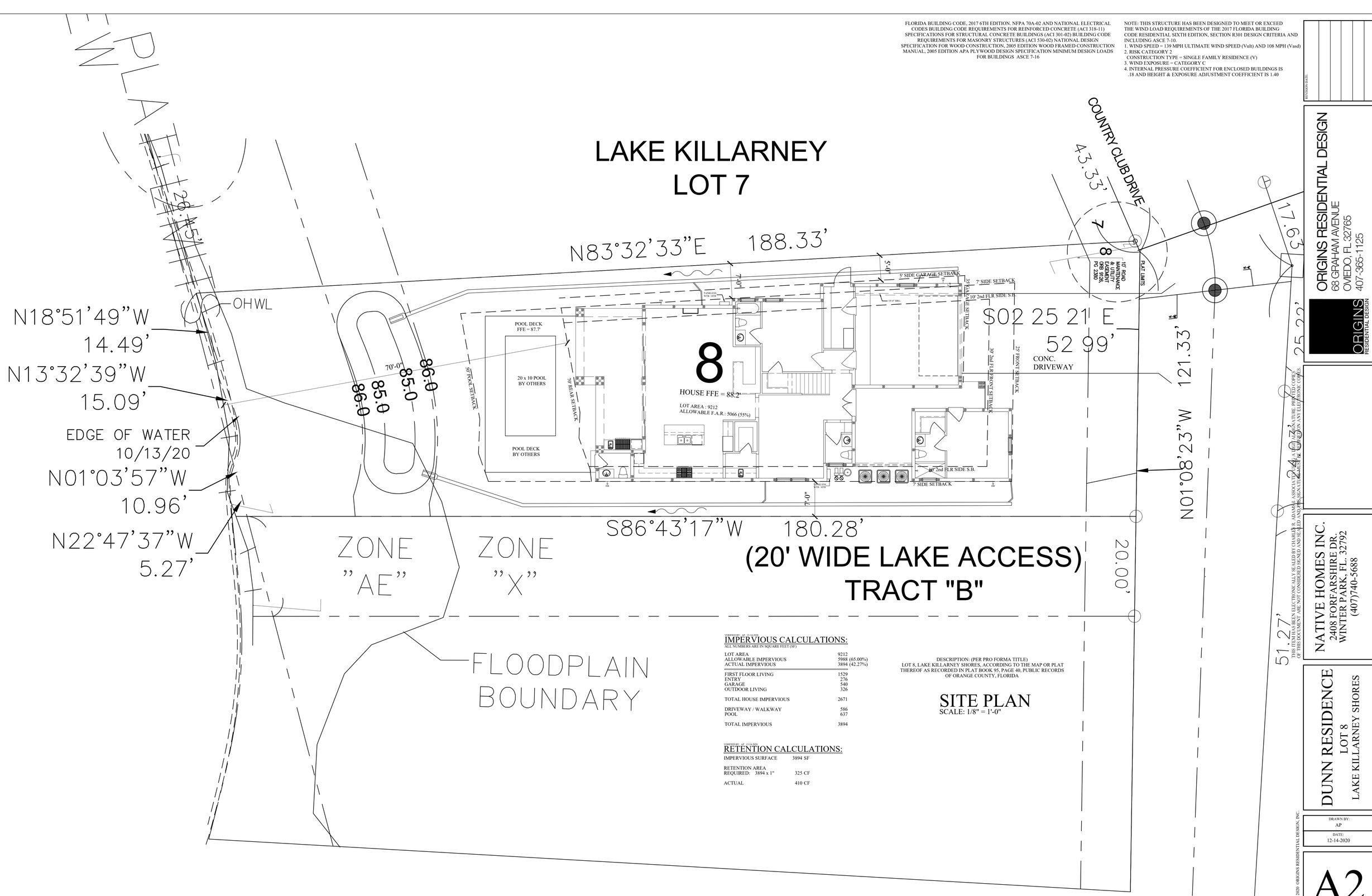
© COPYRIGHT 2020 ORIGINS RESIDENTIAL DESIGN, INC.

A1

FLORIDA BUILDING CODE, 2017 6TH EDITION. NFPA 70A-02 AND NATIONAL ELECTRICAL CODES BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318-11) SPECIFICATIONS FOR STRUCTURAL CONCRETE BUILDINGS (ACI 301-02) BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES (ACI 530-02) NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION, 2005 EDITION WOOD FRAMED CONSTRUCTION MANUAL, 2005 EDITION APA PLYWOOD DESIGN SPECIFICATION MINIMUM DESIGN LOADS FOR BUILDINGS ASCE 7-16

NOTE: THIS STRUCTURE HAS BEEN DESIGNED TO MEET OR EXCEED THE WIND LOAD REQUIREMENTS OF THE 2017 FLORIDA BUILDING CODE RESIDENTIAL SIXTH EDITION, SECTION R301 DESIGN CRITERIA AND INCLUDING ASCE 7-10.
1. WIND SPEED = 139 MPH ULTIMATE WIND SPEED (Vult) AND 108 MPH (Vasd)
2. RISK CATEGORY 2
CONSTRUCTION TYPE = SINGLE FAMILY RESIDENCE (V)
3. WIND EXPOSURE = CATEGORY C
4. INTERNAL PRESSURE COEFFICIENT FOR ENCLOSED BUILDINGS IS .18 AND HEIGHT & EXPOSURE ADJUSTMENT COEFFICIENT IS 1.40

LAKE KILLARNEY
LOT 7



VERIFIED BY: AP 12-14-2020

IMPERVIOUS CALCULATIONS:

ALL NUMBERS ARE IN SQUARE FEET (SF)

LOT AREA	9212
ALLOWABLE IMPERVIOUS	5988 (65.00%)
ACTUAL IMPERVIOUS	3894 (42.27%)
FIRST FLOOR LIVING	1529
ENTRY	276
GARAGE	540
OUTDOOR LIVING	326
TOTAL HOUSE IMPERVIOUS	2671
DRIVEWAY / WALKWAY	586
POOL	637
TOTAL IMPERVIOUS	3894

VERIFIED BY: AP 12-14-2020

RETENTION CALCULATIONS:

IMPERVIOUS SURFACE	3894 SF
RETENTION AREA REQUIRED: 3894 x 1"	325 CF
ACTUAL	410 CF

DESCRIPTION: (PER PRO FORMA TITLE)
LOT 8, LAKE KILLARNEY SHORES, ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 95, PAGE 40, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA

SITE PLAN
SCALE: 1/8" = 1'-0"

REVISION DATE:

ORIGINS RESIDENTIAL DESIGN
68 GRAHAM AVENUE
OVIEDO, FL 32765
407-365-1125

NATIVE HOMES INC.
2408 FORFARSHIRE DR.
WINTER PARK, FL. 32792
(407)740-5688

DUNN RESIDENCE
LOT 8
LAKE KILLARNEY SHORES

DRAWN BY: AP
DATE: 12-14-2020

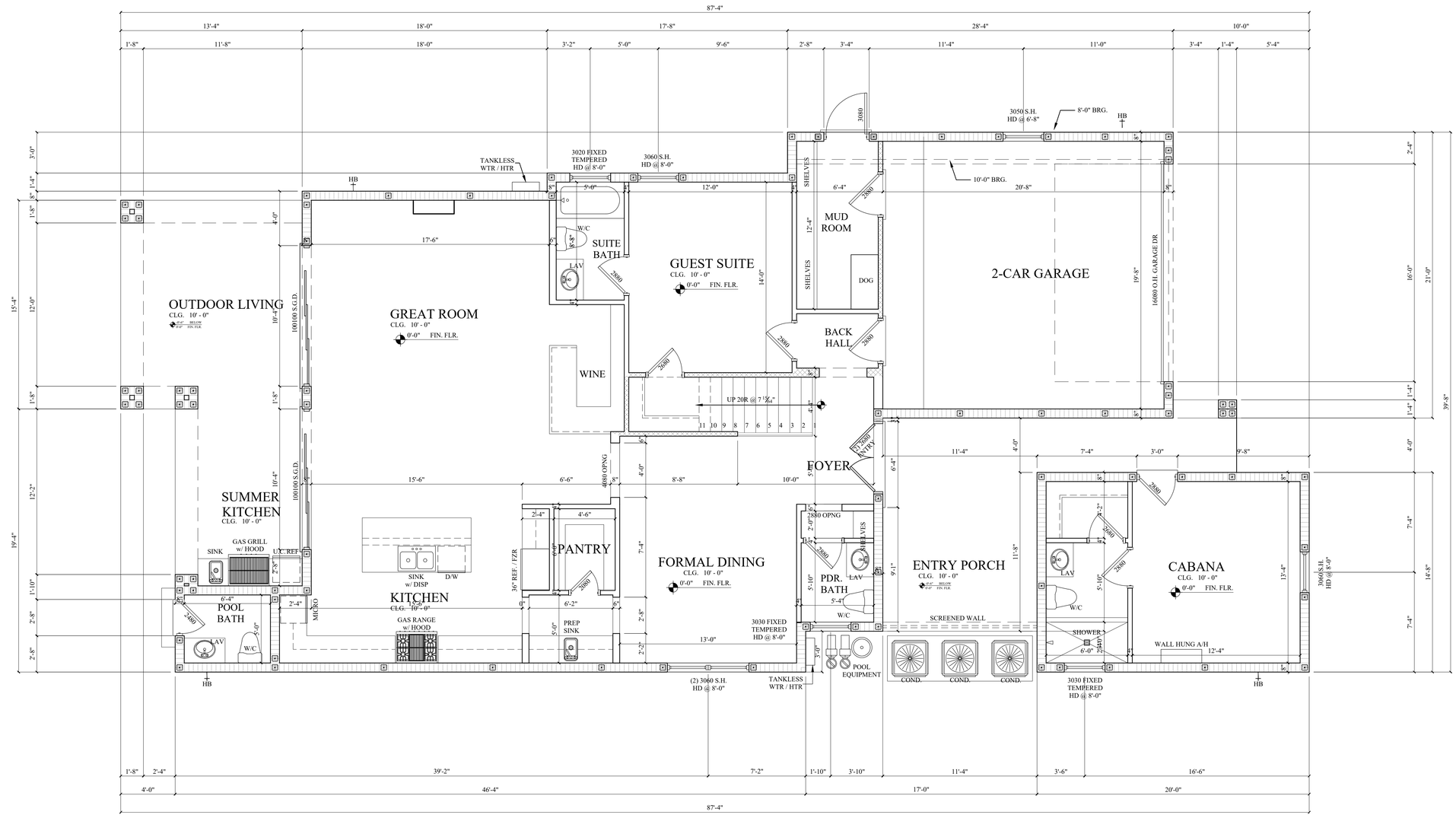
A2

THIS ITEM HAS BEEN ELECTRONICALLY SEALED BY CHARLES R. ADAMS & ASSOCIATES, LLC USING A DIGITAL SIGNATURE. PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

© COPYRIGHT 2020 ORIGINS RESIDENTIAL DESIGN, INC.

FLORIDA BUILDING CODE, 2017 6TH EDITION. NFPA 70A-02 AND NATIONAL ELECTRICAL CODES BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318-11) SPECIFICATIONS FOR STRUCTURAL CONCRETE BUILDINGS (ACI 301-02) BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES (ACI 530-02) NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION, 2005 EDITION WOOD FRAMED CONSTRUCTION MANUAL, 2005 EDITION APA PLYWOOD DESIGN SPECIFICATION MINIMUM DESIGN LOADS FOR BUILDINGS ASCE 7-16

NOTE: THIS STRUCTURE HAS BEEN DESIGNED TO MEET OR EXCEED THE WIND LOAD REQUIREMENTS OF THE 2017 FLORIDA BUILDING CODE RESIDENTIAL SIXTH EDITION, SECTION R301 DESIGN CRITERIA AND INCLUDING ASCE 7-10.
1. WIND SPEED = 139 MPH ULTIMATE WIND SPEED (Vult) AND 108 MPH (Vasd)
2. RISK CATEGORY 2
CONSTRUCTION TYPE = SINGLE FAMILY RESIDENCE (V)
3. WIND EXPOSURE = CATEGORY C
4. INTERNAL PRESSURE COEFFICIENT FOR ENCLOSED BUILDINGS IS .18 AND HEIGHT & EXPOSURE ADJUSTMENT COEFFICIENT IS 1.40



VERIFIED BY: AP 10/06/2020

F.A.R. TABULATIONS:

ALL NUMBERS ARE IN SQUARE FEET (SF)

LOT AREA	9212
ALLOWABLE F.A.R.	5066 (55.0%)
ACTUAL F.A.R.	4560 (49.5%)
FIRST FLOOR LIVING	1529
SECOND FLOOR LIVING	2197
TOTAL LIVING AREA	3726
GARAGE	540
CABANA	294
OUTDOOR LIVING	EXEMPT
ENTRY / BALCONY	EXEMPT
TOTAL	4560

DENOTES ENTRY/BALCONY AREA EXEMPT FROM F.A.R. (400 S.F. MAX : 391 S.F. ACTUAL)

DENOTES OUTDOOR LIVING AREA EXEMPT FROM F.A.R. (500 S.F. MAX : 326 S.F. ACTUAL)

VERIFIED BY: AP 10/06/2020

AREA TABULATIONS:

ALL NUMBERS ARE IN SQUARE FEET (SF)

FIRST FLOOR LIVING	1529
SECOND FLOOR LIVING	2197
CABANA	294
TOTAL LIVING AREA	4020
2 CAR GARAGE	540
ENTRY	276
OUTDOOR LIVING	326
SECOND FLOOR BALCONY	115
TOTAL UNDER ROOF	5277

FIRST FLOOR PLAN
SCALE: 1/4" = 1'-0"

REVISION DATE

ORIGINS RESIDENTIAL DESIGN
68 GRAHAM AVENUE
OVIEDO, FL 32765
407-365-1125
ORIGINS RESIDENTIAL DESIGN

THIS ITEM HAS BEEN ELECTRONICALLY SEALED BY CHARLES R. ADAMS & ASSOCIATES USING A DIGITAL SIGNATURE. PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

NATIVE HOMES INC.
2408 FORFARSHIRE DR.
WINTER PARK, FL. 32792
(407)740-5688

DUNN RESIDENCE
LOT 8
LAKE KILLARNEY SHORES

DRAWN BY: AP
DATE: 12-14-2020

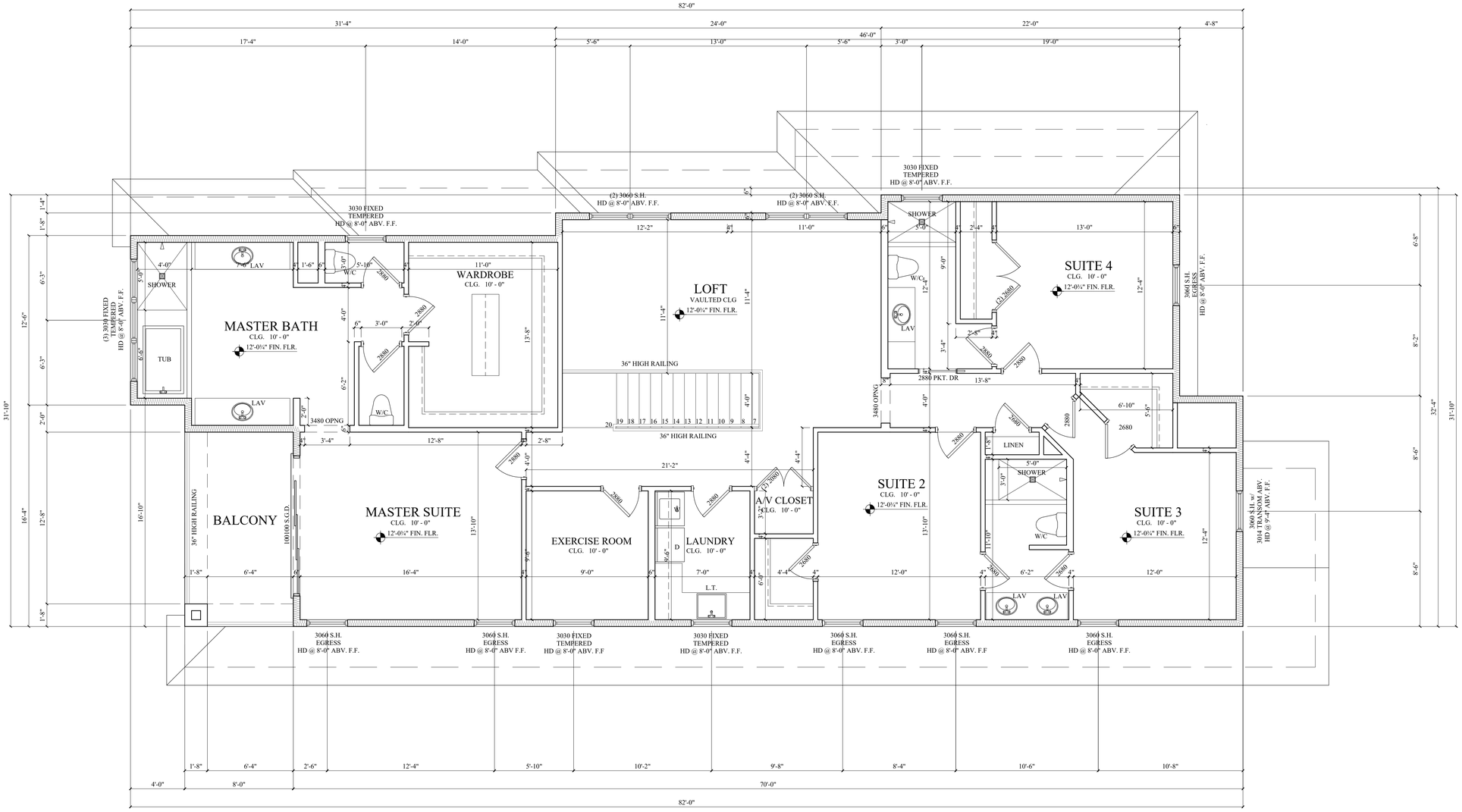
A3

© COPYRIGHT 2020 ORIGINS RESIDENTIAL DESIGN, INC.

FT100535

FLORIDA BUILDING CODE, 2017 6TH EDITION. NFPA 70A-02 AND NATIONAL ELECTRICAL CODES BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318-11) SPECIFICATIONS FOR STRUCTURAL CONCRETE BUILDINGS (ACI 301-02) BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES (ACI 530-02) NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION, 2005 EDITION WOOD FRAMED CONSTRUCTION MANUAL, 2005 EDITION APA PLYWOOD DESIGN SPECIFICATION MINIMUM DESIGN LOADS FOR BUILDINGS ASCE 7-16

NOTE: THIS STRUCTURE HAS BEEN DESIGNED TO MEET OR EXCEED THE WIND LOAD REQUIREMENTS OF THE 2017 FLORIDA BUILDING CODE RESIDENTIAL SIXTH EDITION, SECTION R301 DESIGN CRITERIA AND INCLUDING ASCE 7-10.
1. WIND SPEED = 139 MPH ULTIMATE WIND SPEED (Vult) AND 108 MPH (Vasd)
2. RISK CATEGORY 2
CONSTRUCTION TYPE = SINGLE FAMILY RESIDENCE (V)
3. WIND EXPOSURE = CATEGORY C
4. INTERNAL PRESSURE COEFFICIENT FOR ENCLOSED BUILDINGS IS .18 AND HEIGHT & EXPOSURE ADJUSTMENT COEFFICIENT IS 1.40



SECOND FLOOR PLAN
SCALE: 1/4" = 1'-0"

REVISION DATE

ORIGINS RESIDENTIAL DESIGN
68 GRAHAM AVENUE
OVIEDO, FL 32765
407-365-1125
ORIGINS RESIDENTIAL DESIGN

NATIVE HOMES INC.
2408 FORFARSHIRE DR.
WINTER PARK, FL. 32792
(407)740-5688

DUNN RESIDENCE
LOT 8
LAKE KILLARNEY SHORES

DRAWN BY: AP
DATE: 12-14-2020

A4

THIS ITEM HAS BEEN ELECTRONICALLY SEALED BY CHARLES R. ADAMS & ASSOCIATES USING A DIGITAL SIGNATURE PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

© COPYRIGHT 2020 ORIGINS RESIDENTIAL DESIGN, INC.

P19R030

FLORIDA BUILDING CODE, 2017 6TH EDITION. NFPA 70A-02 AND NATIONAL ELECTRICAL CODES BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318-11) SPECIFICATIONS FOR STRUCTURAL CONCRETE BUILDINGS (ACI 301-02) BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES (ACI 530-02) NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION, 2005 EDITION WOOD FRAMED CONSTRUCTION MANUAL, 2005 EDITION APA PLYWOOD DESIGN SPECIFICATION MINIMUM DESIGN LOADS FOR BUILDINGS ASCE 7-16

NOTE: THIS STRUCTURE HAS BEEN DESIGNED TO MEET OR EXCEED THE WIND LOAD REQUIREMENTS OF THE 2017 FLORIDA BUILDING CODE RESIDENTIAL SIXTH EDITION, SECTION R301 DESIGN CRITERIA AND INCLUDING ASCE 7-10.
1. WIND SPEED = 139 MPH ULTIMATE WIND SPEED (Vult) AND 108 MPH (Vasd)
2. RISK CATEGORY 2
CONSTRUCTION TYPE = SINGLE FAMILY RESIDENCE (V)
3. WIND EXPOSURE = CATEGORY C
4. INTERNAL PRESSURE COEFFICIENT FOR ENCLOSED BUILDINGS IS .18 AND HEIGHT & EXPOSURE ADJUSTMENT COEFFICIENT IS 1.40

REVISION DATE

ORIGINS RESIDENTIAL DESIGN
68 GRAHAM AVENUE
OVIEDO, FL 32765
407-365-1125
ORIGINS RESIDENTIAL DESIGN

THIS ITEM HAS BEEN ELECTRONICALLY SEALED BY CHARLES R. ADAMS & ASSOCIATES USING A DIGITAL SIGNATURE PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

NATIVE HOMES INC.
2408 FORFARSHIRE DR.
WINTER PARK, FL. 32792
(407)740-5688

DUNN RESIDENCE
LOT 8
LAKE KILLARNEY SHORES

DRAWN BY:
AP
DATE:
12-14-2020

A5

© COPYRIGHT 2020 ORIGINS RESIDENTIAL DESIGN, INC.

FY18/01

FLORIDA BUILDING CODE, 2017 6TH EDITION. NFPA 70A-02 AND NATIONAL ELECTRICAL CODES BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318-11) SPECIFICATIONS FOR STRUCTURAL CONCRETE BUILDINGS (ACI 301-02) BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES (ACI 530-02) NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION, 2005 EDITION WOOD FRAMED CONSTRUCTION MANUAL, 2005 EDITION APA PLYWOOD DESIGN SPECIFICATION MINIMUM DESIGN LOADS FOR BUILDINGS ASCE 7-16

NOTE: THIS STRUCTURE HAS BEEN DESIGNED TO MEET OR EXCEED THE WIND LOAD REQUIREMENTS OF THE 2017 FLORIDA BUILDING CODE RESIDENTIAL SIXTH EDITION, SECTION R301 DESIGN CRITERIA AND INCLUDING ASCE 7-10.
1. WIND SPEED = 139 MPH ULTIMATE WIND SPEED (Vult) AND 108 MPH (Vasd)
2. RISK CATEGORY 2
CONSTRUCTION TYPE = SINGLE FAMILY RESIDENCE (V)
3. WIND EXPOSURE = CATEGORY C
4. INTERNAL PRESSURE COEFFICIENT FOR ENCLOSED BUILDINGS IS .18 AND HEIGHT & EXPOSURE ADJUSTMENT COEFFICIENT IS 1.40

REVISION DATE

ORIGINS RESIDENTIAL DESIGN
68 GRAHAM AVENUE
OVIEDO, FL 32765
407-365-1125

ORIGINS RESIDENTIAL DESIGN

THIS ITEM HAS BEEN ELECTRONICALLY SEALED BY CHARLES R. ADAMS & ASSOCIATES USING A DIGITAL SIGNATURE. PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

NATIVE HOMES INC.
2408 FORFARSHIRE DR.
WINTER PARK, FL. 32792
(407)740-5688

DUNN RESIDENCE
LOT 8
LAKE KILLARNEY SHORES

DRAWN BY:
AP

DATE:
12-14-2020

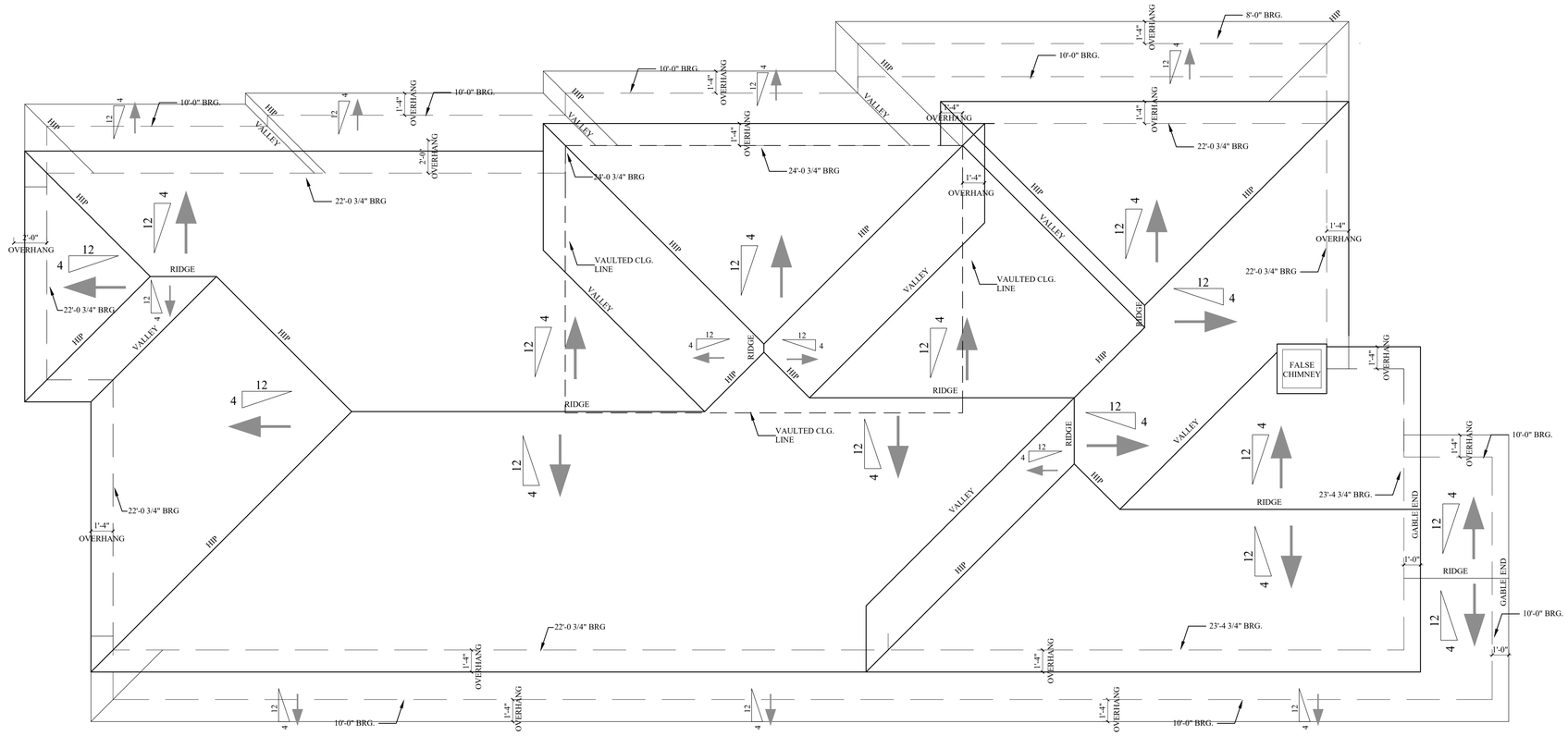
© COPYRIGHT 2020 ORIGINS RESIDENTIAL DESIGN, INC.

A6

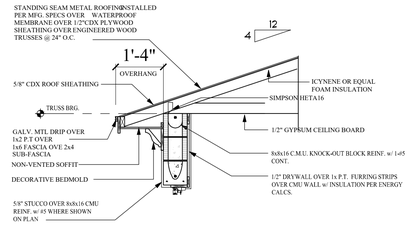
PP#000000

FLORIDA BUILDING CODE, 2017 6TH EDITION. NFPA 70A-02 AND NATIONAL ELECTRICAL CODES BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318-11) SPECIFICATIONS FOR STRUCTURAL CONCRETE BUILDINGS (ACI 301-02) BUILDING CODE REQUIREMENTS FOR MASONRY STRUCTURES (ACI 530-02) NATIONAL DESIGN SPECIFICATION FOR WOOD CONSTRUCTION, 2005 EDITION WOOD FRAMED CONSTRUCTION MANUAL, 2005 EDITION APA PLYWOOD DESIGN SPECIFICATION MINIMUM DESIGN LOADS FOR BUILDINGS ASCE 7-16

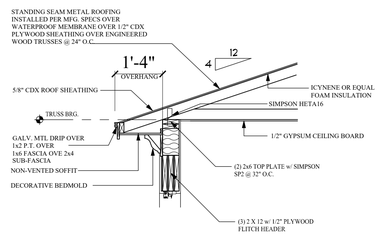
NOTE: THIS STRUCTURE HAS BEEN DESIGNED TO MEET OR EXCEED THE WIND LOAD REQUIREMENTS OF THE 2017 FLORIDA BUILDING CODE RESIDENTIAL SIXTH EDITION, SECTION R301 DESIGN CRITERIA AND INCLUDING ASCE 7-10.
1. WIND SPEED = 139 MPH ULTIMATE WIND SPEED (Vult) AND 108 MPH (Vasd)
2. RISK CATEGORY 2
CONSTRUCTION TYPE = SINGLE FAMILY RESIDENCE (V)
3. WIND EXPOSURE = CATEGORY C
4. INTERNAL PRESSURE COEFFICIENT FOR ENCLOSED BUILDINGS IS .18 AND HEIGHT & EXPOSURE ADJUSTMENT COEFFICIENT IS 1.40



ROOF SLOPE PLAN
SCALE: 1/4" = 1'-0"



TYP. CORNICE DETAIL
SCALE: 1/2" = 1'-0"



REVISION DATE

ORIGINS RESIDENTIAL DESIGN
68 GRAHAM AVENUE
OVIEDO, FL 32765
407-365-1125
ORIGINS RESIDENTIAL DESIGN

NATIVE HOMES INC.
2408 FORFARSHIRE DR.
WINTER PARK, FL. 32792
(407)740-5688

DUNN RESIDENCE
LOT 8
LAKE KILLARNEY SHORES

DRAWN BY: AP
DATE: 12-14-2020

A7

THIS ITEM HAS BEEN ELECTRONICALLY SEALED BY CHARLES R. ADAMS & ASSOCIATES USING A DIGITAL SIGNATURE. PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES.

© COPYRIGHT 2020 ORIGINS RESIDENTIAL DESIGN, INC.

FY18R08

Planning & TRANSPORTATION

401 South Park Avenue • Winter Park, Florida 32789

407-599-3324 • planning@cityofwinterpark.org
cityofwinterpark.org

Planning & Zoning Board
Staff Report for January 5, 2021 Meeting

SPR #20-14. Request of Lazarus Development Group, LLC for: Approval to construct a new, two-story, 4,204 square foot, single-family home located at 520 Country Club Drive on Lake Killarney, zoned R-2.

Background

As the Board may recall, this request was heard at the November 2, 2020 meeting, but was denied to the concerns raised by the adjacent neighbor at 518 Country Club Drive (south of this lot). Since that meeting, the applicant and prospective property owners met with the adjacent neighbor at 518 Country Club Drive and have submitted new plans based on their meeting. Staff is also recommending conditions of approval based on their meeting with the neighbor.

Summary of Request

Lazarus Development Group (representing the property owners) is requesting site plan approval to construct a new, two-story, 4,204 square-foot, single-family home at 520 Country Club Drive, which is located on Lake Killarney, and is zoned R-2. This property measures 8,995 square feet, and is also known as Lot 9 of the Lake Killarney Shores replat that was approved by the Commission on April 9, 2018. Below is a table summarizing this request in comparison to the R-2 zoning/lakefront lot requirements.

Table 1:

	R-2/Lakefront Lot Requirements	Proposed
Floor Area Ratio	Max 55%	3,722 square feet/41%
Impervious Lot Coverage	Max 65%	4,947 square feet/55%
Lakefront Setback	64.3 feet	85 feet

*Total size is 4,204 square feet but the FAR excludes entry/balcony and outdoor living areas.

Lakefront Lot Review Criteria:

Tree Preservation

The purpose and intent of the lakefront lots section of the code states that existing trees shall be preserved to the degree reasonably possible, and the appearance of the property and the shore when viewed from the water will be kept as natural as reasonably possible. As there are currently no trees located on this lot, the applicant is not proposing to remove any trees as part of this request. This criterion was not a concern at the November 2, 2020 meeting.

View from the Lake

The code limits walls and swimming pool decks facing the lake in excess of three feet in height. This lakefront review criterion was a concern of the neighbor at the November 2, 2020 meeting. Since then, the applicant has lowered the pool deck an additional 11 inches so that the deck is now approximately 0.7 inches above the existing grade, well below the three-foot height maximum.

Due to lowering their pool deck an additional 11 inches, this put their outdoor patio and pool deck on two different levels and made their outdoor patio narrower. In this current application, the applicants are requesting a five (5) foot front setback variance to add width onto their outdoor patio. This five (5) foot front setback variance is agreed to by the affected neighbor (518 Country Club Drive) in the attached letter. Staff does not see a concern with this front setback variance as requested, as it ultimately moves the home further away from the lakefront on the side of the lot closest to 518 Country Club Drive due to the curvature of the lake, and the size of the home (FAR) remains the same.

View of Neighbors

The purpose and intent of the lakefront lots section of the code is to ensure that the views of the lake from adjoining properties will not be unduly impaired by new houses, additions, second story additions, etc. In order to achieve this, the code states that the lakefront setback shall be the average established by the adjacent lakefront properties within 200 feet of the subject property, or a 50-foot setback, whichever is greater. However, the P&Z Board does have the authority to approve setbacks less than the average determined, or impose more restrictive setbacks.

During the replat of this Lake Killarney Shores subdivision, there was significant discussion about the lakefront views of the adjoining neighbors to this subdivision at both the P&Z Board and City Commission meetings. At the Commission meeting, it was decided that the lakefront average setback will be established by the setback of the existing homes along the lake within 200 feet of each request (even though they have subsequently been demolished).

Staff has determined that the average lakefront setback for this lot (known as Lot 9 of the attached plat) is approximately 64.3 feet from the lake (see attached analysis). The applicant/builder is proposing an 85-foot setback at the closest point from the lake to the home, which is significantly more than the average setback determined by staff. Additionally, a 50-foot setback to the pool deck is requested.

The applicant has provided a letter signed by the developer/owner of the Lake Killarney replat properties nearest to this lot and also signed by the nearest affected (non-developer owned property) neighbor to this lot (owner of 518 Country Club Drive), that states an agreed-upon building setback of 85 feet from the lake, and a 50-foot pool/deck setback.

Another concern at the November 2, 2020 meeting was that the owner of these replatted properties had previously brought in fill dirt to this lot, and caused it to be noticeably higher than the neighboring property at 518 Country Club Drive. Attached is a signed affidavit stating that no fill dirt has been brought to this property.

Stormwater Retention

The code requires retention of stormwater so that stormwater flowing over a waterfront lawn area does not carry any fertilizers, herbicides, or other chemicals that into the canal or chain of lakes that may affect the water quality of our community's most precious assets. The amount

of impervious surface on the lot determines the depth/size of the retention needed. The applicant is proposing a storm water swale near the lakefront that is sized to meet code requirements.

Due to the concern of the neighboring property at 518 Country Club Drive regarding runoff from this property affecting their lot, the applicant has agreed to install roof drains and underground pipes to carry the stormwater runoff from their roof down to the stormwater swale near the lakefront.

Summary

The applicant is proposing a home that is compatible with the natural grade of the property, is ensuring that water pollution from stormwater runoff and other sources will be minimized, and is preserving existing trees to the degree reasonably possible so that the appearance of the property and the shore when viewed from the water, will be kept as natural as reasonably possible. Overall, the plans meet the intent of the lakefront review criteria defined in the code.

Staff recommendation is for approval with the following conditions:

1. *That the applicant limits the height of their fence between the 85-foot setback and the lakefront to four (4) feet in height on the south side of their lot.*

2. *That the applicant installs roof drains and underground pipes to convey the stormwater from their roof down to the stormwater swale at the lakefront.*

LOCATION MAP

520 Country Club Drive

City of Winter Park
Florida

LOCATION MAP

520 Country Club Drive

City of Winter Park
Florida

N
W E
S

RE: 520 Country Club Drive, Winter Park, FL 32789 November 17th, 2020

To the City of Winter Park Planning & Zoning Commission,

My name is Jennifer, I would like to introduce you to myself and my husband Alex. We have each lived in Central Florida for the past 15 years and love calling the community our home. Alex works in finance for Charles Schwab and I am in management for a technology company, Finastra, which is headquartered in Lake Mary. We were married in 2018.

We have always loved spending time in Winter Park and dreamt of the day that we would be able to call this beautiful city our home. We are both lovers of the arts, nature, and good food – which led us to fall further in love with and wanting to become part of your community. We are passionate about volunteering and have even started our own family foundation so we can one day give to enhance the community for future generations to come.

Alex and I are both planners. We have rigorously saved our entire lives to one day be able to afford building our dream home. We were over the moon when we discovered Lake Killarney Shores. We were even more excited to discover there was one last lakefront lot – a beautiful piece of land overlooking the cypress trees and serenity of Lake Killarney. We immediately listed our home in College Park and sold it within a few days, preparing for the big adventure.

Alex and I believe in conservation, respecting rules, and respecting community. We understand and appreciate that Winter Park takes extra consideration in ensuring new development does not encroach upon existing neighbors or nature (such as specified setbacks, building guidelines etc.). We also understand that our specific lot went under even further consideration to extend setbacks as a courtesy to the neighboring homeowner – approved at their request.

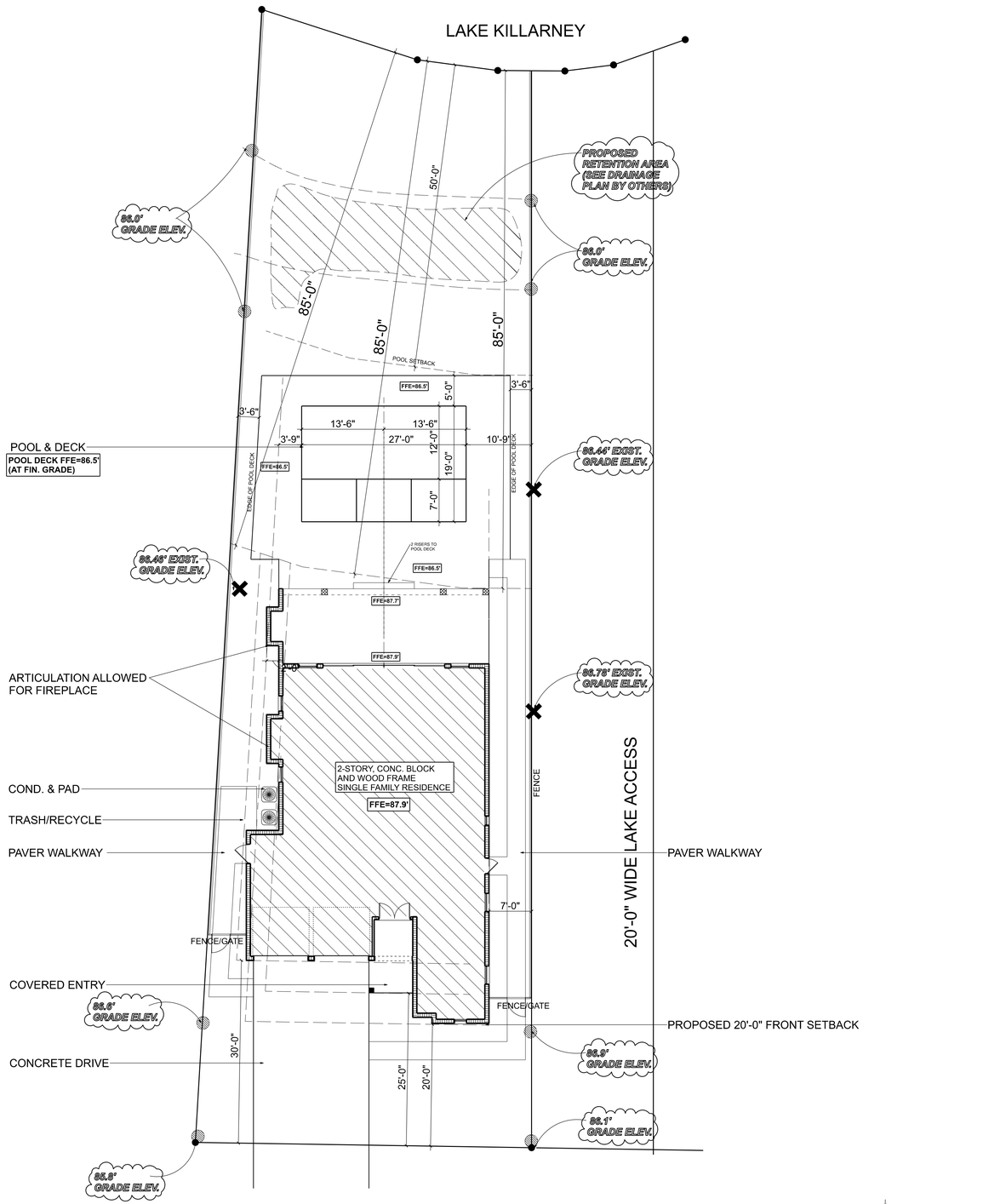
After months of planning, following the pre-established rules, and pouring our soul into the project we were saddened to hear our home was not approved at the Planning & Zoning meeting on November 2nd. We learned that the neighboring homeowner who owns the rental property next to our lot came forward with new considerations over and above what they originally asked for, understood, and agreed to. We want to have good relationships with all of our neighbors and have tried extremely hard to properly follow all rules so the neighborhood is enjoyed by all.

Our ask is that this committee and board recognize we have tried our best to follow the rules and be respectful Winter Park residents. We also ask you to recognize how challenging this is for us as we are chasing a *now* moving target that had already been agreed upon. We do not know how long the delays can go on for or where to go from here. We fear we, nor anyone else, may ever be able to make our neighbor 100% happy. Since the unexpected delay in approvals, we will begin facing a $600 weekly charge to delay our closing with our lender which is scheduled for December 22nd.

After following the rules and even agreeing to additional requests after the November 2nd meeting to keep our neighbor happy, we would like permission to move forward and build our dream home that fits within Winter Park guidelines.

Thank you for your consideration,

Jennifer & Alex Benedetti
Hopeful future 520 Country Club Drive, Winter Park, FL 32789 *residents*



LOT 9 AREA TABULATIONS	
LOT AREA	8995 SF
FAR ALLOWED (55%)	4947 SF
FIRST FLOOR A/C	1424 SF
SECOND FLOOR A/C	1826 SF
TOTAL A/C	3250 SF
GARAGE/STORAGE	472 SF
TOTAL FAR	3722 SF
ENTRY	72 SF
COVERED LANAI	320 SF
COVERED BALCONY	90 SF
TOTAL UNDER ROOF	4204 SF

LOT AREA	8995 SF
SETBACKS	
FRONT-1ST FLOOR	25'
FRONT-2ND FLOOR	30'
FRONT-GARAGE	35'
REAR	70'
SIDE HOUSE	7'
SIDE-2ND FLOOR	10'
SIDE-GARAGE	5'
ALLOWED IMPERVIOUS (65%)	5846 SF
PROPOSED IMPERVIOUS (BELOW)	
HOUSE COVERAGE	2288 SF
DRIVE/WALK	1042 SF
POOL/DECK	1360 SF
A/C PADS	18 SF
TOTAL LOT COVERAGE	4808 SF

PRELIMINARY SITE PLAN-VARIANCE APP

SCALE: 1" = 10'-0"

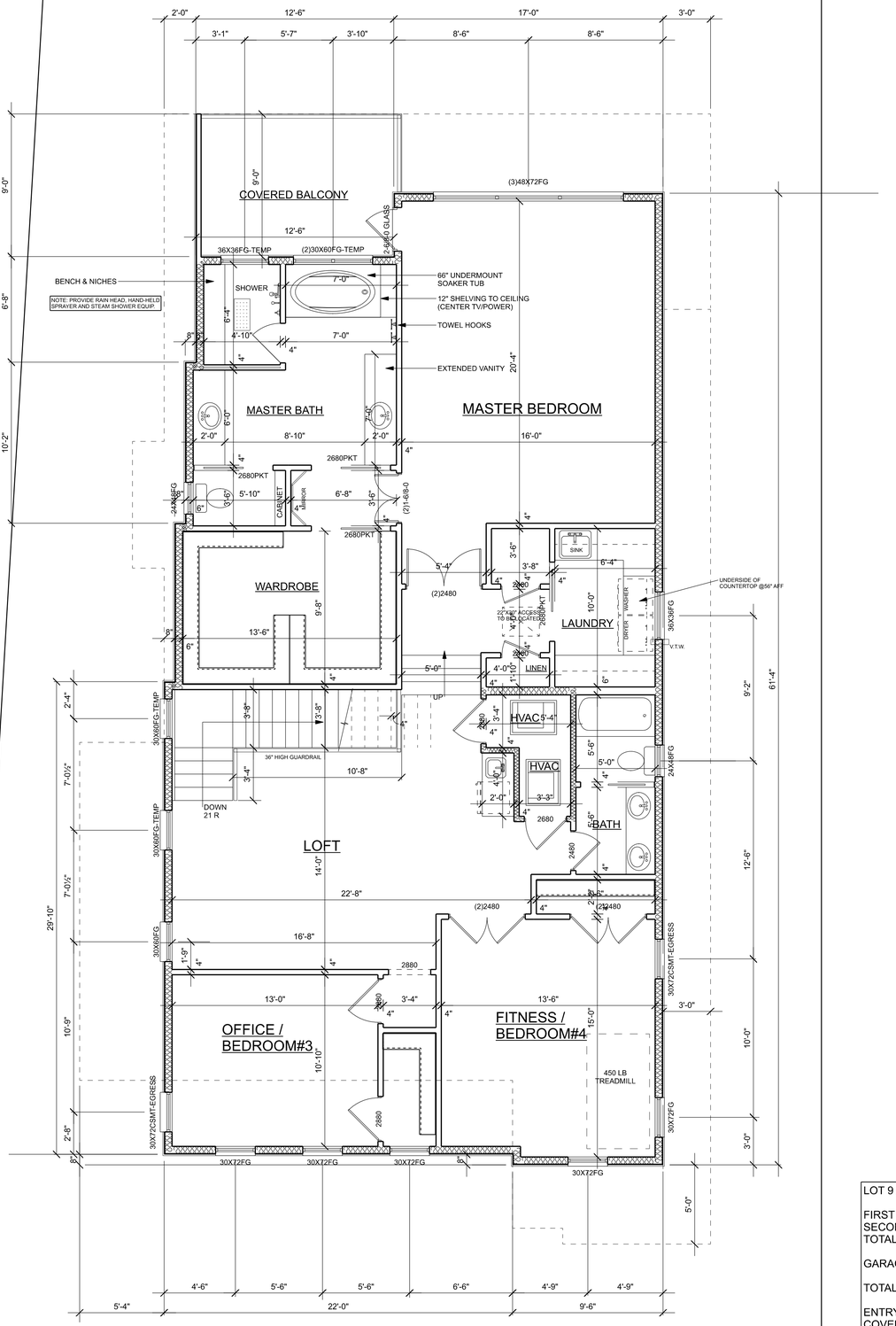
CHRIS GOODEN
DESIGNER
1009 SUMMER LAKES DRIVE; ORLANDO, FL 32835
PH: 407-701-6029 GOODENDESIGN@HOTMAIL.COM

WINTER PARK DEVELOPMENT

SINGLE FAMILY RESIDENCE
LOT 9 LAKE KILLARNEY
WINTER PARK, FLORIDA

DRAWN: CMG
DATE: 12/07/20
REV: 12/10/20

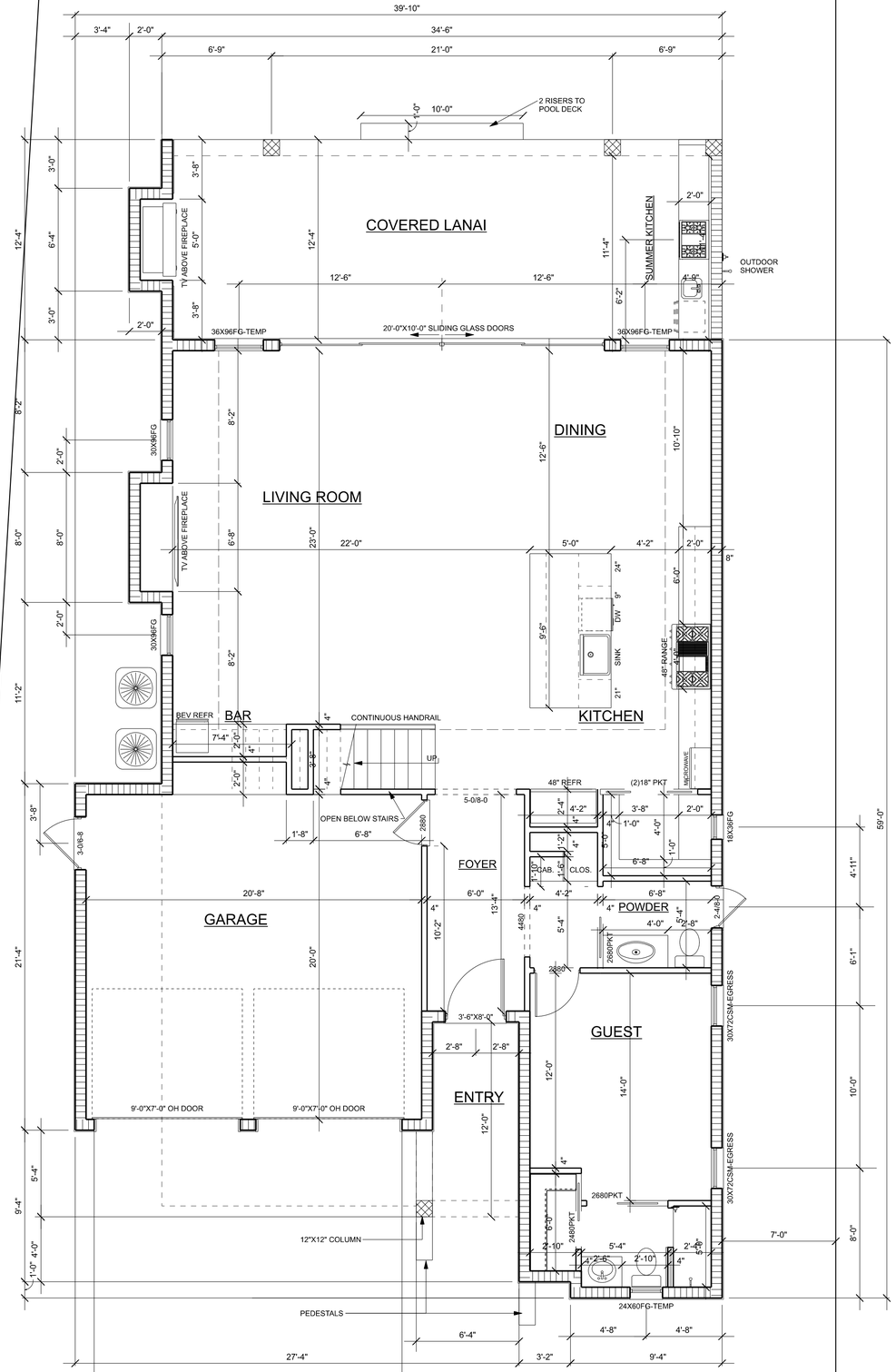
A0



SECOND FLOOR PLAN-VARIANCE APP

SCALE: 1/4" = 1'-0"

LOT 9 AREA TABULATIONS	
FIRST FLOOR A/C	1459 SF
SECOND FLOOR A/C	1841 SF
TOTAL A/C	3300 SF
GARAGE/STORAGE	422 SF
TOTAL FAR	3722 SF
ENTRY	72 SF
COVERED LANAI	320 SF
COVERED BALCONY	90 SF
TOTAL UNDER ROOF	4204 SF



FIRST FLOOR PLAN-VARIANCE APP

SCALE: 1/4" = 1'-0"

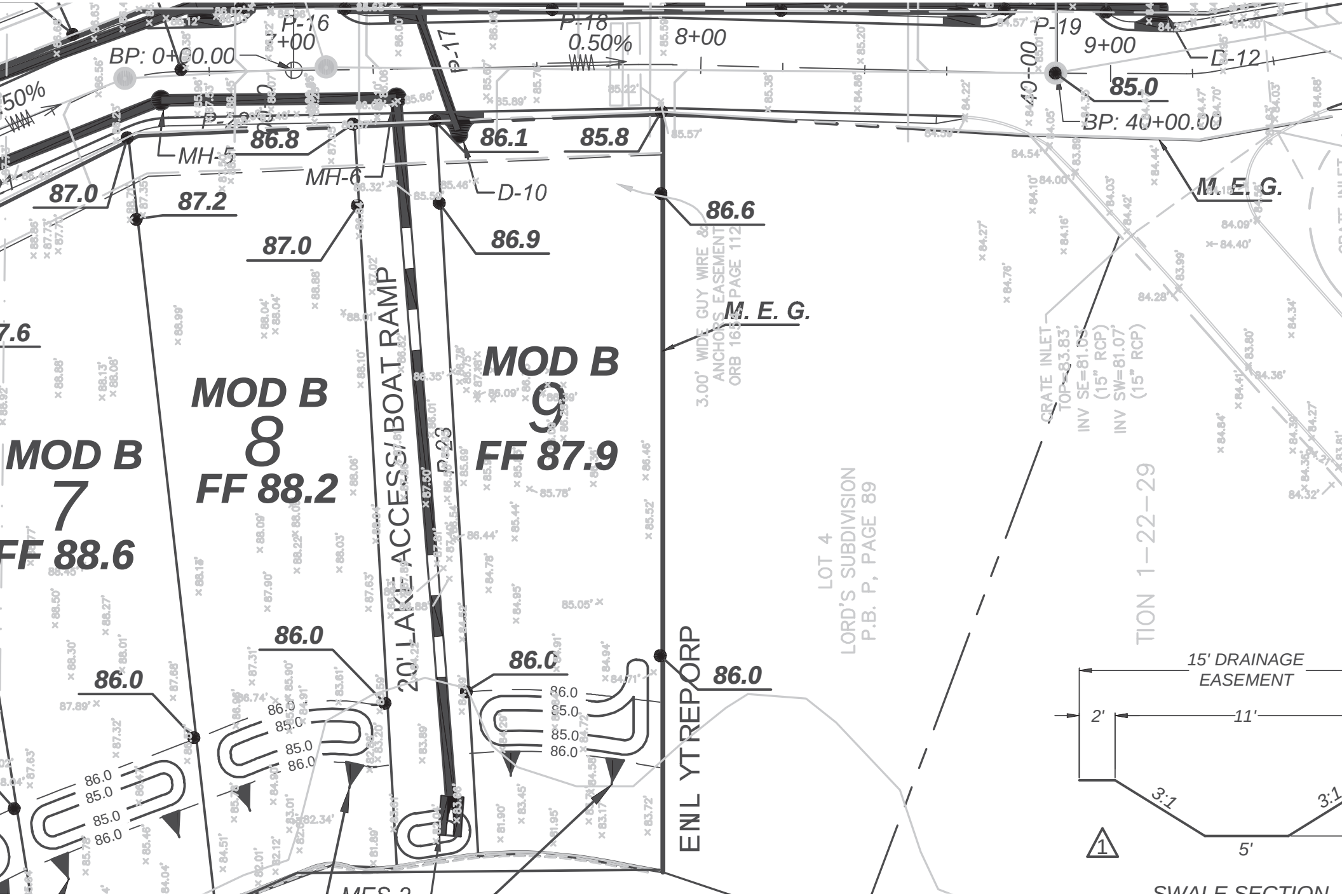
CHRIS GOODEN
DESIGNER
1009 SUMMER LAKES DRIVE, ORLANDO, FL. 32835
PH: 407-701-6629 GOODENDESIGN@HOTMAIL.COM

WINTER PARK DEVELOPMENT

SINGLE FAMILY RESIDENCE
LOT 9 LAKE KILLARNEY
WINTER PARK, FLORIDA

DRAWN: CMG
DATE: 10/12/20
REV: 12/3/20

A1


P-16
7+00
BP: 0+00.00
P-17
P-18
0.50%
8+00
P-19
9+00
D-12
85.0
BP: 40+00.00
40+00
M. E. G.
MH-5
MH-6
86.8
86.1
85.8
87.0
87.2
87.0
D-10
86.6
86.9
M. E. G.
MOD B
7
FF 88.6
MOD B
8
FF 88.2
MOD B
9
FF 87.9
20' LAKE ACCESS/BOAT RAMP
P-23
3.00' WIDE GUY WIRE & ANCHORS EASEMENT ORB 165 PAGE 112
CRATE INLET TOP=83.83'
INV SE=81.03' (15" RCP)
INV SW=81.07' (15" RCP)
LOT 4
LORD'S SUBDIVISION
P.B. P, PAGE 89
TION 1-22-29
ENL YT REP ORP
86.0
86.0
86.0
86.0
85.0
85.0
85.0
86.0
86.0
85.0
85.0
86.0
15' DRAINAGE EASEMENT
2'
11'
3:1
3:1
5'
1
SWALE SECTION

LEFT SIDE ELEVATION

SCALE: 1/4" = 1'-0"

FRONT ELEVATION

SCALE: 1/4" = 1'-0"

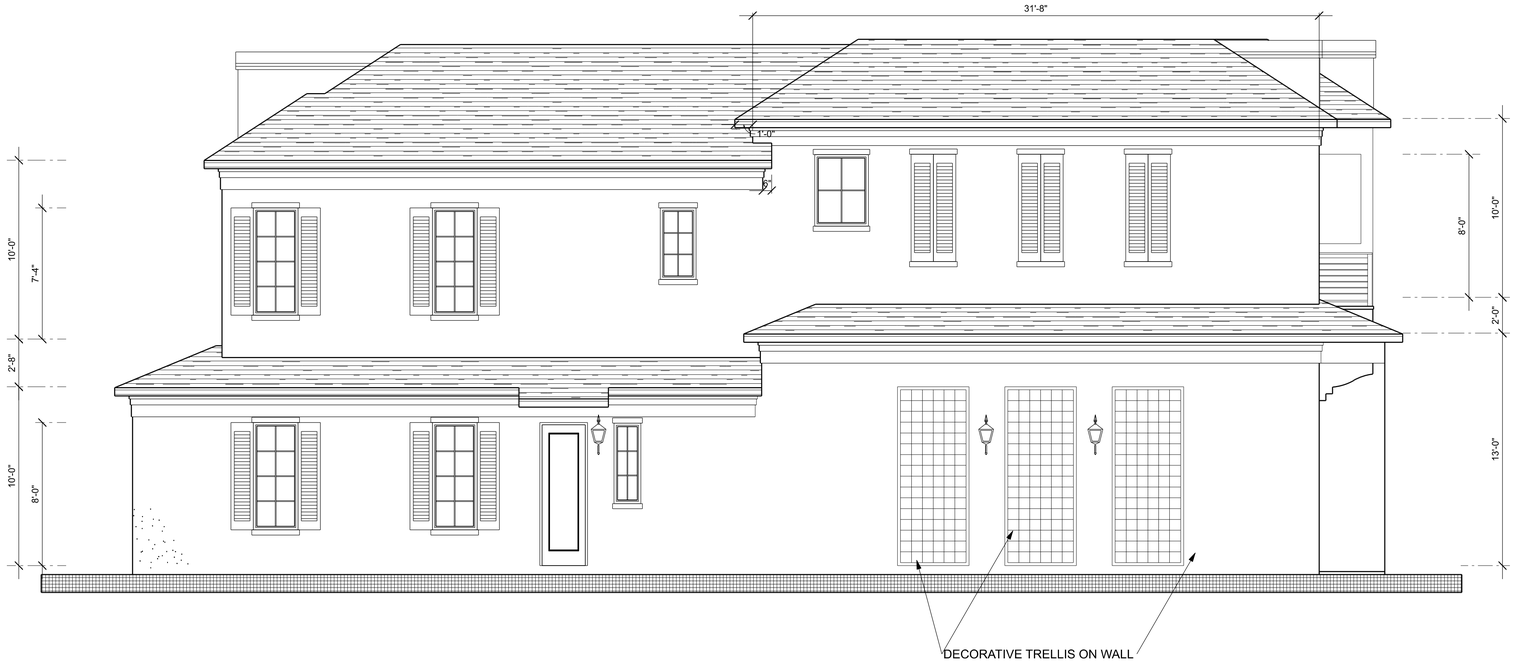
CHRIS GOODEN
DESIGNER
1009 SUMMER LAKES DRIVE, ORLANDO, FL. 32835
PH: 407-701-6029 GOODENDESIGN@HOTMAIL.COM

WINTER PARK DEVELOPMENT

SINGLE FAMILY RESIDENCE
LOT 9 LAKE KILLARNEY
WINTER PARK, FLORIDA

DRAWN: CMG
DATE: 9/16/20

A2



RIGHT SIDE ELEVATION

SCALE: 1/4" = 1'-0"

REAR ELEVATION

SCALE: 1/4" = 1'-0"

CHRIS GOODEN
DESIGNER
1009 SUMMER LAKES DRIVE, ORLANDO, FL 32835
PH: 407-701-8029 GOODENDESIGN@HOTMAIL.COM

WINTER PARK DEVELOPMENT

SINGLE FAMILY RESIDENCE
LOT 9 LAKE KILLARNEY
WINTER PARK, FLORIDA

DRAWN: CMG
DATE: 9/16/20

A3

AFFIDAVIT

STATE OF FLORIDA
COUNTY OF ORANGE

BEFORE ME, the undersigned authority, duly authorized to administer oaths and take acknowledgments, personally appeared **ANIL DESHPANDE,** who first being duly sworn, deposes and says:

1. That Affiant is over the age of eighteen (18) years and has personal knowledge of the facts herein contained.

2. Affiant is the developer of the property located at 520 County Club Drive, Winter Park, FL 32789, which is identified as Lot 9 within the Lake Killarney Shores Plat, Plat Book 95, Page 38, Orange County, FL (the "Property").

3. Affiant states that it is his knowledge and understanding that there was no dirt or fill brought onto the Property prior to or during the recent construction of the residential home on the Property.

4. That Affiant is familiar with the nature of an oath and with the penalties provided by the laws of the State of Florida for falsely swearing to a document.

FURTHER AFFIANT SAYETH NOT.

ANIL DESHPANDE
5401 S Kirkman Road, #640
Orlando, FL 32819

The foregoing instrument was sworn to and subscribed before me, by means of physical presence, this 9th day of DECEMBER, 2020, by Anil Deshpande, who ✓ is personally known to me ~~or who __ has produced _____ as identification~~.

NOTARY PUBLIC
MY COMMISSION EXPIRES:
NOTARY SEAL

NOTARY PUBLIC STATE OF FLORIDA

BRENDA FURBUSH
Commission # GG 986751
Expires September 3, 2024
Bonded Thru Budget Notary Services

LAKE KILLARNEY SHORES

BEING A REPLAT OF PORTIONS OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST CITY OF WINTER PARK, ORANGE COUNTY, FLORIDA

LEGAL DESCRIPTION:

PARCELS OF LAND BEING A PORTION OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, LYING WITHIN SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST, MORE PARTICULARLY DESCRIBED AS FOLLOWS:

(1ST DESCRIBED)

BEGIN AT A IRON PIPE (NO ID) MARKING THE NORTHEAST CORNER OF THAT PARTICULAR PROPERTY DESCRIBED IN OFFICIAL RECORDS BOOK 6449, PAGE 2972, (FOR A POINT OF REFERENCE, SAID POINT IS THE FOLLOWING THREE (3) COURSES FROM THE NORTHEAST CORNER OF SAID LOT 1, LORD'S SUBDIVISION: (1) SOUTH 88°48'25"WEST, A DISTANCE OF 192.85 FEET (2) SOUTH 00°08'22"EAST, A DISTANCE OF 270.14 FEET, AND (3) NORTH 88°48'25"EAST, A DISTANCE OF 165.00 FEET). THENCE, FROM SAID **POINT OF BEGINNING**, RUN SOUTH 00°51'00"EAST, ALONG THE WESTERLY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOK 4184, PAGE 1708 AND BOOK 10684, PAGE 7897, FOR A DISTANCE OF 115.36 FEET; THENCE RUN SOUTH 00°00'00"EAST, ALONG THE WESTERLY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOKS 10684, PAGE 7897; BOOK 7711, PAGE 4825; AND BOOK 9605, PAGE 2646, FOR A DISTANCE OF 199.59 FEET; THENCE CONTINUE SOUTH 20°28'36"EAST, ALONG SAID WESTERLY LINE, FOR A DISTANCE OF 12.83 FEET; THENCE CONTINUE SOUTH 69°08'54"WEST, FOR A DISTANCE OF 4.80 FEET; THENCE RUN SOUTH 00°00'00"EAST ALONG THE WESTERLY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOKS 9605, PAGE 2646; BOOK 10658, PAGE 3768; BOOK 10450, PAGE 32; BOOK 9838, PAGE 8700; AND BOOK 9247, PAGE 4865, FOR A DISTANCE OF 506.96 FEET TO A POINT ON THE NORTH LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 4425, PAGE 366; THENCE RUN NORTH 89°32'17"WEST, ALONG SAID NORTH PROPERTY LINE, FOR A DISTANCE OF 27.80 FEET TO THE NORTHWEST CORNER OF SAID PROPERTY; THENCE SOUTH 00°01'28"WEST, ALONG THE WESTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 119.05 FEET TO A POINT ON THE NORTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378 (KNOWN AS COUNTRY CLUB DRIVE); THENCE RUN ALONG THE NORTHERLY AND EASTERLY LINES OF SAID PROPERTY (COUNTRY CLUB DRIVE) THE FOLLOWING EIGHT (8) COURSES: (1) NORTH 82°56'13"WEST, FOR A DISTANCE OF 43.61 FEET; (2) NORTH 67°02'46"WEST, FOR A DISTANCE OF 32.85 FEET; (3) NORTH 44°29'39"WEST, FOR A DISTANCE OF 29.31 FEET; (4) NORTH 19°26'25"WEST, FOR A DISTANCE OF 36.63 FEET; (5) NORTH 10°13'52"WEST, FOR A DISTANCE OF 160.05 FEET; (6) NORTH 00°07'50" EAST, FOR A DISTANCE OF 260.46 FEET; (7) NORTH 21°40'44"WEST, FOR A DISTANCE OF 80.00 FEET; (8) NORTH 19°29'40"WEST, FOR A DISTANCE OF 100.16 FEET TO THE MOST SOUTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9750, PAGE 4747; THENCE DEPARTING SAID EASTERLY PROPERTY LINE (COUNTRY CLUB DRIVE), RUN NORTH 63°30'30"EAST, ALONG THE SOUTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9750, PAGE 4747, FOR A DISTANCE OF 150.00 FEET TO THE SOUTHEAST CORNER OF SAID PROPERTY; THENCE NORTH 24°01'54"WEST, ALONG THE EASTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 138.85 FEET TO THE MOST NORTHERLY CORNER OF SAID PROPERTY; THENCE NORTH 55°10'58"WEST, FOR A DISTANCE OF 23.45 FEET TO A POINT ON THE EAST LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 2925, PAGE 1746; THENCE RUN NORTH 00°08'22"WEST, ALONG THE EAST PROPERTY LINE OF THOSE PARTICULAR PROPERTIES AS DESCRIBED IN OFFICIAL RECORDS BOOK 2925, PAGE 1746 AND OFFICIAL RECORDS BOOK 10658, PAGE 4875, FOR A DISTANCE OF 83.56 FEET TO THE SOUTHWEST CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9126, PAGE 2378 (KNOWN AS ELLEN DRIVE), SAID POINT DESIGNATED HEREIN AS REFERENCE POINT 'A'; THENCE RUN NORTH 88°48'25"EAST, ALONG THE SOUTH LINE OF SAID PROPERTY (ELLEN DRIVE) AND THE SOUTH LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORD BOOK 10056, PAGE 9292, FOR A DISTANCE OF 165.00 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 145,116 SQUARE FEET OR 3.331 ACRES, MORE OR LESS.

ALONG WITH (2ND DESCRIBED):

COMMENCING AT THE AFOREMENTIONED REFERENCE POINT 'A', BEING THE SOUTHWEST CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9126, PAGE 2378 (KNOWN AS ELLEN DRIVE); THENCE RUN SOUTH 63°27'11"WEST, FOR A DISTANCE OF 75.47 FEET TO THE MOST NORTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 10948, PAGE 2596, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, FOR A **POINT OF BEGINNING**, THENCE RUN SOUTH 48°02'57"WEST, ALONG THE NORTHWESTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 135.24 FEET TO THE NORTHEASTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9126, PAGE 2378 (KNOWN AS COUNTRY CLUB DRIVE), SAID POINT DESIGNATED HEREIN AS REFERENCE POINT 'B'; THENCE NORTH 49°27'42"WEST, ALONG SAID NORTHEASTERLY PROPERTY LINE (COUNTRY CLUB DRIVE), FOR A DISTANCE OF 129.41 FEET, TO THE MOST SOUTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 9744, PAGE 4872; THENCE NORTH 47°05'24"EAST, ALONG THE SOUTHEASTERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 158.91 FEET TO THE MOST EASTERLY CORNER OF SAID PROPERTY, SAID POINT ALSO LYING ON THE WESTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 10658, PAGE 4875; THENCE SOUTH 37°51'42"EAST, ALONG SAID WESTERLY PROPERTY LINE, FOR A DISTANCE OF 112.20 FEET; THENCE SOUTH 47°41'20"EAST, CONTINUING ALONG SAID WESTERLY PROPERTY LINE, FOR A DISTANCE OF 19.19 FEET, TO THE **POINT OF BEGINNING**.

CONTAINING 18,880 SQUARE FEET, OR 0.433 ACRES, MORE OR LESS.

ALONG WITH (3RD DESCRIBED):

COMMENCING AT A 3/4-INCH IRON PIPE (NO ID) BEING THE AFOREMENTIONED REFERENCE POINT 'B', THENCE RUN SOUTH 53°05'10" WEST, CROSSING SAID COUNTRY CLUB DRIVE (AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378), FOR A DISTANCE OF 37.19 FEET, TO A POINT ON THE SOUTHWESTERLY LINE OF SAID PARCEL AND THE **POINT OF BEGINNING**, SAID POINT ALSO BEING THE MOST NORTHERLY CORNER OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 6642, PAGE 2836; THENCE RUN SOUTH 53°00'00"WEST, ALONG THE NORTHERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 164.27 FEET, MORE OR LESS, TO THE ORDINARY HIGH WATER LINE OF LAKE KILLARNEY; THENCE RUN NORTHWESTERLY ALONG SAID ORDINARY HIGH WATER LINE, THE FOLLOWING TWELVE (12) COURSES: (1) NORTH 27°05'50"WEST, FOR A DISTANCE OF 13.65 FEET; (2) NORTH 34°46'51"WEST, FOR A DISTANCE OF 18.32 FEET; (3) NORTH 32°27'29"WEST, FOR A DISTANCE OF 15.62 FEET; (4) NORTH 47°13'16"WEST, FOR A DISTANCE OF 33.98 FEET; (5) NORTH 43°42'48"WEST, FOR A DISTANCE OF 8.91 FEET; (6) NORTH 20°25'43"WEST, FOR A DISTANCE OF 4.18 FEET; (7) NORTH 05°55'13"WEST, FOR A DISTANCE OF 5.45 FEET; (8) NORTH 16°07'19"WEST, FOR A DISTANCE OF 4.13 FEET; (9) NORTH 66°35'18"WEST, FOR A DISTANCE OF 3.37 FEET; (10) NORTH 51°55'47"WEST, FOR A DISTANCE OF 21.23 FEET; (11) NORTH 53°20'21"WEST, FOR A DISTANCE OF 15.41 FEET; (12) NORTH 58°23'34"WEST, FOR A DISTANCE OF 9.98 FEET TO A POINT ON THE SOUTHEASTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 7445, PAGE 911; THENCE NORTH 38°11'00"EAST, ALONG SAID SOUTHEASTERLY PROPERTY LINE FOR A DISTANCE OF 142.84 FEET, MORE OR LESS, TO A POINT ON THE SOUTHWESTERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378 (KNOWN AS COUNTRY CLUB DRIVE);THENCE SOUTH 48°42'32"EAST, ALONG SAID SOUTHWESTERLY LINE, FOR A DISTANCE OF 190.16 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 25,008 SQUARE FEET, OR 0.574 ACRES, MORE OR LESS.

ALONG WITH (4TH DESCRIBED):

COMMENCING AT A 3/4-INCH IRON PIPE (NO ID) BEING THE AFOREMENTIONED REFERENCE POINT 'B', THENCE RUN SOUTH 53°05'10" WEST, CROSSING SAID COUNTRY CLUB DRIVE (AS DESCRIBED IN OFFICIAL RECORD BOOK 9126, PAGE 2378), FOR A DISTANCE OF 37.19 FEET, TO A POINT ON THE SOUTHWESTERLY LINE OF SAID PARCEL; THENCE SOUTH 43°49'00"EAST, ALONG SAID SOUTHWESTERLY PROPERTY LINE, FOR A DISTANCE OF 102.04 FEET; THENCE SOUTH 22°14'49"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 33.39 FEET TO THE **POINT OF BEGINNING**; THENCE RUN SOUTH 22°14'49"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 180.00 FEET TO AN ANGLE BREAK; THENCE SOUTH 26°43'08"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 45.70 FEET TO AN ANGLE BREAK; THENCE SOUTH 02°25'21"EAST, CONTINUING ALONG SAID PROPERTY LINE, FOR A DISTANCE OF 124.20 FEET TO A POINT ON THE NORTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 6538, PAGE 4203; THENCE DEPARTING SAID RIGHT OF WAY LINE, RUN NORTH 90°00'00"WEST, ALONG THE NORTHERLY LINE OF SAID PROPERTY, FOR A DISTANCE OF 186.31 FEET, MORE OR LESS, TO THE ORDINARY HIGH WATER LINE OF LAKE KILLARNEY; THENCE RUN NORTHWESTERLY ALONG SAID ORDINARY HIGH WATER LINE THE FOLLOWING SIXTEEN (16) COURSES: (1) NORTH 14°46'36"EAST, FOR A DISTANCE OF 26.86 FEET; (2) NORTH 04°48'08"EAST, FOR A DISTANCE OF 13.31 FEET; (3) NORTH 02°52'30"WEST, FOR A DISTANCE OF 11.03 FEET; (4) NORTH 10°29'47"WEST, FOR A DISTANCE OF 8.06 FEET; (5) NORTH 22°47'37"WEST, FOR A DISTANCE OF 12.48 FEET; (6) NORTH 01°03'57"WEST, FOR A DISTANCE OF 10.96 FEET; (7) NORTH 13°32'39" WEST, FOR A DISTANCE OF 15.09 FEET; (8) NORTH 18°51'49"WEST, FOR A DISTANCE OF 40.94 FEET; (9) NORTH 25°09'04"WEST, FOR A DISTANCE OF 14.95 FEET; (10) NORTH 30°03'30"WEST, FOR A DISTANCE OF 23.57 FEET; (11) NORTH 32°19'44"WEST, FOR A DISTANCE OF 24.01 FEET; (12) NORTH 20°34'10"WEST, FOR A DISTANCE OF 19.67 FEET; (13) NORTH 24°51'44"WEST, FOR A DISTANCE OF 29.29 FEET; (14) NORTH 23°52'10"WEST, FOR A DISTANCE OF 17.25 FEET; (15) NORTH 31°28'23"WEST, FOR A DISTANCE OF 24.16 FEET; (16) NORTH 27°05'50"WEST, FOR A DISTANCE OF 6.15 FEET TO A POINT ON THE SOUTHERLY LINE OF THAT PARTICULAR PROPERTY AS DESCRIBED IN OFFICIAL RECORDS BOOK 6642, PAGE 2836; THENCE RUN NORTH 71°31'12"EAST, ALONG SAID SOUTHERLY PROPERTY LINE, FOR A DISTANCE OF 190.51 FEET TO THE **POINT OF BEGINNING**.

CONTAINING 58,782 SQUARE FEET, OR 1.349 ACRES, MORE OR LESS.

FOR AN AGGREGATE TOTAL OF 247,786 SQUARE FEET, OR 5.688 ACRES, MORE OR LESS.

TOGETHER WITH THAT CERTAIN EASEMENT RECORDED IN O.R. BOOK 3282, PAGE 2096, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA.

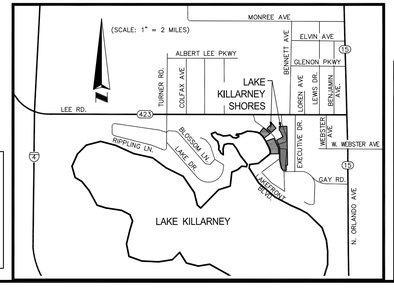
AND

TOGETHER WITH THAT DECLARATION OF ACCESS AND UTILITY EASEMENT RECORDED IN O.R. BOOK 7464, PAGE 1949, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA.

SURVEYOR'S NOTES:

1. THE BEARINGS SHOWN HEREON ARE BASED ON A PORTION OF THE MONUMENTED WESTERLY LINE OF COUNTRY CLUB DRIVE (SEE MAP SHEET 2 OF 3), PER OFFICIAL RECORDS BOOK 9126, PAGE 2378, AS HAVING A BEARING OF S 48°42'32" E, AND AS PER SURVEY MAP PROVIDED BY THE CITY OF WINTER PARK, PREPARED BY: HENRICH, TROTTER, CARTER & AYERS, INC., DATED 12/17/99, CERTIFIED BY: G.L.CARTER, REGISTERED LAND SURVEYOR NO. 2435, DATE: 5/15/01.
2. ALL PLATTED UTILITY EASEMENTS SHALL ALSO BE EASEMENTS FOR THE CONSTRUCTION, INSTALLATION, MAINTENANCE, AND OPERATION OF CABLE TELEVISION SERVICES PROVIDED, HOWEVER, NO SUCH CONSTRUCTION, INSTALLATION, MAINTENANCE, AND OPERATION OF CABLE TELEVISION SERVICES SHALL INTERFERE WITH THE FACILITIES AND SERVICES OF AN ELECTRIC, TELEPHONE, GAS OR OTHER PUBLIC UTILITY. IN THE EVENT A CABLE TELEVISION COMPANY DAMAGES THE FACILITIES OF A PUBLIC UTILITY, IT SHALL BE SOLELY RESPONSIBLE FOR THE DAMAGES. THIS SECTION SHALL NOT APPLY TO THOSE PRIVATE EASEMENTS GRANTED TO OR OBTAINED BY A PARTICULAR ELECTRIC, TELEPHONE, GAS OR OTHER PUBLIC UTILITY. SUCH CONSTRUCTION, INSTALLATION, MAINTENANCE, AND OPERATION SHALL COMPLY WITH THE NATIONAL ELECTRICAL SAFETY CODE AS ADOPTED BY THE FLORIDA PUBLIC SERVICE COMMISSION. (SECTION 177.091 (28), FLORIDA STATUTES).
3. DRAINAGE EASEMENTS SHOWN HEREON ARE HEREBY DEDICATED TO THE LAKE KILLARNEY SHORES HOMEOWNERS' ASSOCIATION, INC. FOR THE OPERATION, MAINTENANCE AND REPAIR OF THE SUBDIVISION'S STORMWATER MANAGEMENT AND DRAINAGE SYSTEM.
4. UTILITY EASEMENTS SHOWN HEREON ARE HEREBY DEDICATED TO THE CITY OF WINTER PARK AND TO OTHER PUBLIC UTILITY SERVICE PROVIDERS FOR THE OPERATION, INSTALLATION, MAINTENANCE AND REPAIR OF UTILITIES OF EVERY TYPE. THE CITY OF WINTER PARK SHALL ONLY BE RESPONSIBLE FOR THE MAINTENANCE OF UTILITIES IT INSTALLS OR ACCEPTS WITHIN ANY OF THE UTILITY EASEMENTS DEPICTED ON THIS PLAT, AND THE CITY SHALL NOT HAVE ANY RESPONSIBILITY FOR MAINTENANCE OF THE EASEMENTS FOR ANY OTHER PURPOSE, INCLUDING IN REGARD TO THE SURFACE IMPROVEMENTS, SOD AND LANDSCAPING AND UTILITIES OWNED BY OTHERS. THE CITY OF WINTER PARK'S UTILITY EASEMENT RIGHTS SHALL BE SUPERIOR TO THAT OF OTHER UTILITY OWNERS IN THE EVENT OF A CONFLICT.
5. THE CITY OF WINTER PARK AND THE ST. JOHNS RIVER WATER MANAGEMENT DISTRICT SHALL HAVE THE RIGHT, BUT NOT THE OBLIGATION, TO ACCESS, MAINTAIN, REPAIR, REPLACE OR OTHERWISE CARE FOR OR CAUSE TO BE CARED FOR, TRACT "A" AND DRAINAGE EASEMENTS INCLUDING, WITHOUT LIMITATION THE DRAINAGE SYSTEMS CONSTRUCTED THEREON. A BLANKET INGRESS/EGRESS EASEMENT IS GRANTED IN FAVOR OF THE CITY OF WINTER PARK AND/OR THE ST. JOHNS RIVER WATER MANAGEMENT DISTRICT FOR SAID PURPOSE OVER SAID DRAINAGE EASEMENTS, AND OVER TRACT "A".
6. SIDEWALK EASEMENTS SHOWN HEREON ARE HEREBY DEDICATED TO THE CITY OF WINTER PARK FOR THE OPERATION, REPAIR AND MAINTENANCE OF PUBLIC SIDEWALKS AND FOR THE GENERAL PUBLIC'S PEDESTRIAN ACCESS AND USE OF SIDEWALK IMPROVEMENTS THERIN. THE CITY OF WINTER PARK SHALL ONLY BE RESPONSIBLE FOR THE MAINTENANCE OF THE SIDEWALK IMPROVEMENTS IT INSTALLS OR ACCEPTS WITHIN ANY OF THE SIDEWALK EASEMENTS DEPICTED ON THIS PLAT, AND THE CITY SHALL NOT HAVE ANY RESPONSIBILITY FOR MAINTENANCE OF THE EASEMENTS FOR ANY OTHER PURPOSE.
7. LOT OWNERS SHALL BE RESPONSIBLE TO MAINTAIN THEIR LOTS AS SET FORTH IN THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS FOR LAKE KILLARNEY.
8. TRACT "E" (PUBLIC RIGHT-OF-WAY), AN EXTENSION OF THE ELLEN DRIVE RIGHT-OF-WAY, IS HEREBY DEDICATED TO THE CITY OF WINTER PARK FOR THE OPERATION, REPAIR AND MAINTENANCE OF THE RIGHT-OF-WAY FOR THE GENERAL PUBLIC'S VEHICULAR AND PEDESTRIAN ACCESS AND USE OF THE ROADWAY IMPROVEMENTS THEREON.
9. THIS SUBDIVISION IS SUBJECT TO AND GOVERNED BY THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS FOR LAKE KILLARNEY SHORES.
10. TRACT "A" (STORMWATER MANAGEMENT) AND TRACT "B" (LAKE ACCESS) SHALL BE OWNED AND MAINTAINED BY THE LAKE KILLARNEY SHORES HOMEOWNERS' ASSOCIATION, INC. FOR THE BENEFIT OF THE OWNERS OF LOTS 1-30 AND FOR THE OTHER PURPOSES AS SET FORTH IN THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS TO BE RECORDED IN THE PUBLIC RECORDS. A NON-EXCLUSIVE DRAINAGE EASEMENT IS HEREBY GRANTED TO THE CITY OF WINTER PARK OVER, UNDER, AND THROUGH TRACT "A" AND THE DRAINAGE EASEMENTS ADJACENT TO THE PUBLIC RIGHTS-OF-WAY. THE CITY OF WINTER PARK SHALL NOT HAVE ANY OPERATION, MAINTENANCE AND REPAIR OBLIGATIONS CONCERNING TRACT "A", TRACT "B" OR ANY DRAINAGE EASEMENTS DEDICATED HEREON.
11. EACH DRAINAGE EASEMENT AREA ON LOTS 1-11 AND 26-30 MUST BE MAINTAINED BY THE INDIVIDUAL LOT OWNERS.
12. THE CITY OF WINTER PARK AND ITS OFFICERS, EMPLOYEES AND AGENTS ARE HEREBY DEDICATED A NON-EXCLUSIVE, PERPETUAL EASEMENT OVER AND THROUGH TRACT "B" (20' WIDE LAKE ACCESS) AND IMPROVEMENTS THEREON FOR PEDESTRIAN, VEHICULAR AND BOAT ACCESS TO AND FROM LAKE KILLARNEY AND PUBLIC RIGHTS-OF-WAY FOR MAINTENANCE, SEARCH AND RESCUE, PATROLLING AND RELATED MATTERS. THIS DOES NOT CREATE A PUBLIC RIGHT OF USE AND ACCESS IN TRACT "B". THE CITY SHALL HAVE NO MAINTENANCE AND REPAIR OBLIGATIONS CONCERNING TRACT "B".
13. DOCKS/BOATHOUSES SHALL BE LOCATED ONLY WHERE INDICATED ON THIS PLAT (BY ASTERISK SYMBOL, SEE LEGEND). PURSUANT TO CITY COMMISSION APPROVAL, SUCH DOCKS/BOATHOUSES MAY BE LOCATED WITH ZERO SIDE SETBACKS AND NO MORE THAN THREE FEET. SIZE, HEIGHT AND DIMENSIONS SHALL BE PERMITTED BY THE CITY OF WINTER PARK LAKES AND WATERWAYS BOARD.
14. ACCESS EASEMENT TRACT "C" AND "D" SHALL BE CONVEYED TO THE LAKE KILLARNEY SHORES HOMEOWNER'S ASSOCIATION, INC. AND OPERATED, REPAIRED AND MAINTAINED FOR INGRESS AND EGRESS TO AND FROM CERTAIN LOTS AND THE PUBLIC RIGHT-OF-WAY AS FURTHER SET FORTH IN THE DECLARATION OF COVENANTS, CONDITIONS, EASEMENTS AND RESTRICTIONS TO BE RECORDED IN THE PUBLIC RECORDS.
15. THE DEDICATOR/PROPERTY OWNER FOR ITSELF AND ITS SUCCESSORS AND ASSIGNS HEREBY DISCLAIMS, RELEASES AND QUIT CLAIMS TO THE CITY OF WINTER PARK AND ANY AND ALL RIGHTS, TITLE AND INTERESTS DEDICATOR/PROPERTY OWNER MAY HAVE IN COUNTRY CLUB DRIVE AND TO ANY GAPS AND GORES THAT MAY EXIST BETWEEN THE PROPERTY BEING SUBDIVIDED BY THIS PLAT AND COUNTRY CLUB DRIVE, WHETHER BEING INSIDE OR OUTSIDE THE BOUNDARIES OF THIS PLAT.
16. REGARDLESS OF THE TRACT AND LOT BOUNDARIES ADJACENT TO LAKE KILLARNEY SHOWN HEREON, THE STATE OF FLORIDA INTERNAL IMPROVEMENT TRUST FUND OWNS TITLE, AS SOVEREIGN LANDS, TO THOSE LANDS OF LAKE KILLARNEY LYING WATERWARD OF THE ORDINARY HIGH WATER LINE (OHWL). ORDINARY HIGH WATER ELEVATION IS BASED ON A PUBLICATION BY CITY OF WINTER PARK, DEPARTMENT OF PUBLIC WORKS (LAKES DIVISION), WHICH LISTS THE ORDINARY HIGH WATER ELEVATION AS: 82.0 NAVD-1988 (82.8 NGVD-1929).
17. ALL SITE CONSTRUCTION AND TREE REMOVAL ACTIVITIES MUST BE IN ACCORDANCE WITH APPLICABLE CITY OF WINTER PARK CODES OF ORDINANCES AND THE CONDITIONS OF APPROVAL FOR THIS PLAT AND ALL OTHER DEVELOPMENT ORDERS AND BUILDING PERMITS ISSUED CONCERNING THIS SUBDIVISION. AMONG OTHER THINGS, THERE ARE SPECIFIC CONDITIONS OF APPROVAL FOR THIS SUBDIVISION CONCERNING BUILDING SETBACKS, TREE PRESERVATION AND RESTRICTING THE REMOVAL OF TREES FROM THE LOTS WHICH SHALL BE BINDING UPON THE LOTS AND MUST BE COMPLIED WITH BY ALL LOT OWNERS, AS SET FORTH IN THE CONDITIONS OF PLAT APPROVAL, WHEN DEEMED NECESSARY BY CITY'S URBAN FORESTRY PERSONNEL IN ORDER TO PRESERVE TREE(S) AND TREE CANOPY, THE BUILDING SETBACK TO AN INTERIOR (SAME SUBDIVISION) PROPERTY LINE CONCERNING LOTS #10, 11, 13, 14, 15, 19, 20 22, 23, 26, AND 28 MAY BE REDUCED TO A 5 FOOT ONE STORY AND 8 FOOT TWO STORY SIDE SETBACK IN LIEU OF THE REQUIRED 7 FOOT ONE STORY AND 10 FOOT TWO STORY SIDE SETBACKS AND A 15 FOOT SECOND STORY REAR IN LIEU OF THE REQUIRED 25 FOOT REAR SETBACK. ALL OTHER LOTS SHALL MEET THE NORMAL R-2 ZONING DISTRICT SETBACK REQUIREMENTS. NO PERMIT FOR REMOVAL OF TREES ON THE "TREES TO BE SAVED" PLAN ON FILE WITH THE CITY WILL BE GRANTED BY THE CITY FOR A PERIOD OF AT LEAST 25 YEARS FROM THE DATE OF RECORDING OF THIS PLAT UNLESS THE CITY ARBORIST DETERMINES THAT THEIR CONDITION, HEALTH OR OTHER NON-ECONOMIC FACTORS WARRANT THEIR REMOVAL.

VICINITY MAP



ASM
AMERICAN SURVEYING & MAPPING INC.
CERTIFICATION OF AUTHORIZATION NUMBER LB#6393
3191 MAGUIRE BOULEVARD, SUITE 200
ORLANDO, FLORIDA 32803
(407) 426-7979

NOTICE

THIS PLAT, AS RECORDED IN ITS GRAPHIC FORM, IS THE OFFICIAL DEPICTION OF THE SUBDIVIDED LANDS DESCRIBED HEREIN AND WILL IN NO CIRCUMSTANCES BE SUPPLANTED IN AUTHORITY BY ANY OTHER GRAPHIC OR DIGITAL FORM OF THE PLAT. THERE MAY BE ADDITIONAL RESTRICTIONS THAT ARE NOT RECORDED ON THIS PLAT THAT MAY BE FOUND IN THE PUBLIC RECORDS OF THIS COUNTY, AS PER 177.091 (27) F.A.C.

SURVEYOR'S CERTIFICATE

I HEREBY CERTIFY THAT THIS PLAT IS A TRUE AND CORRECT REPRESENTATION OF THE HEREON DESCRIBED LAND WHICH WAS RECENTLY SURVEYED AND PLATTED UNDER MY DIRECTION AND SUPERVISION, AND THAT PERMANENT REFERENCE MONUMENTS AND PERMANENT CONTROL POINTS HAVE BEEN SET IN ACCORDANCE WITH CHAPTER 177, FLORIDA STATUTES, AND THAT SURVEY DATA COMPLIES WITH ALL REQUIREMENTS OF CHAPTER 177, FLORIDA STATUTES.

SURVEYOR'S NAME: JAMES D. FLICK, PSM DATE
Registration Number: LS 6088
Florida Registration Number of Legal Entity: LB 6393
3191 Maguire Boulevard, Suite 200, Orlando FL 32803

PLAT BOOK PAGE

LAKE KILLARNEY SHORES DEDICATION

KNOW ALL MEN BY THESE PRESENTS, That the limited liability company named below, being the owner in fee simple of the lands shown hereon, does hereby dedicate said lands and plat for the uses and purposes therein expressed, including as set forth in the Surveyor's Notes.

IN WITNESS WHEREOF, has caused these presents to be signed and attested to by the officers named below on the_____day of_____, 2018.
Owner:
Turner Real Property Investments, LLC,
a Florida limited liability company

BY: _____ _____
TITLE

SIGNED AND SEALED IN THE PRESENCE OF:

_____ _____
SIGNATURE PRINTED NAME

_____ _____
SIGNATURE PRINTED NAME

ACKNOWLEDGEMENT

STATE OF FLORIDA
ORANGE COUNTY

THIS IS TO CERTIFY, That on _____, 2018, before me, an officer duly authorized to take acknowledgments in the State and County aforesaid, personally appeared

_____, _____
(title)

of Turner Real Property Investments, LLC, a limited liability company under the laws of the State of Florida, to me know to be the individual and officer described in and who executed the foregoing dedication and severally acknowledged the execution thereof to be his free act and deed as such officer there unto duly authorized; and that the said dedication for the uses and purposes therein expressed is the act and deed of said limited liability company.

IN WITNESS WHEREOF, I have hereto set my hand and seal on the above date

_____
NOTARY PUBLIC
My Commission Expires _____

CERTIFICATE OF APPROVAL, BY THE CITY OF WINTER PARK

THIS IS TO CERTIFY, That on _____, the City Commission of the City of Winter Park, Florida approved the foregoing plat.

_____
Steve Leary Mayor

Attest:

_____
Cindy Bonham City Clerk

CERTIFICATE OF APPROVAL BY CITY ENGINEER

Examined & Approved: _____ Date _____

City Engineer _____

Florida Registration No: _____

CERTIFICATE OF REVIEW BY CITY SURVEYOR

I have reviewed this plat and find it be in conformity with Chapter 177, Florida Statutes.

_____ _____
Signed Date
Florida Registration Number

CERTIFICATE OF COUNTY COMPTROLLER

I HEREBY CERTIFY that the foregoing plat was recorded in the Orange County Official Records on _____ as File No. _____.

County Comptroller in and for Orange County, Florida

_____

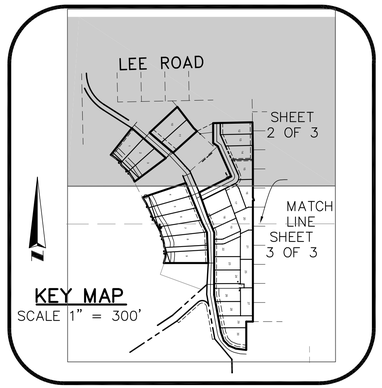
LAKE KILLARNEY SHORES

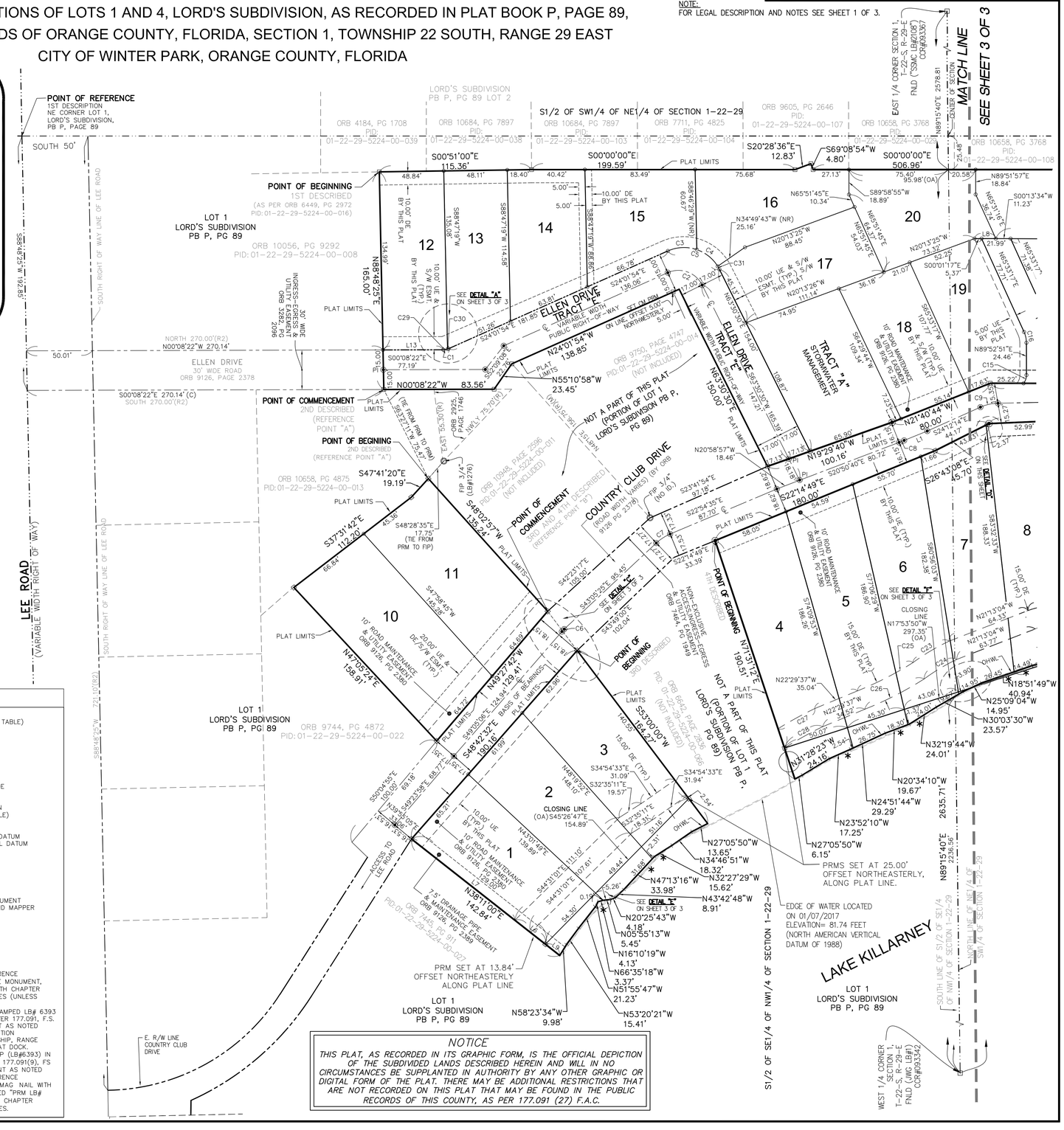
SHEET 2 OF 3

PLAT BOOK ____ PAGE ____

BEING A REPLAT OF PORTIONS OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST CITY OF WINTER PARK, ORANGE COUNTY, FLORIDA

NOTE:
FOR LEGAL DESCRIPTION AND NOTES SEE SHEET 1 OF 3.





Curve Table					
Curve #	Length	Radius	Delta	Chord Bearing	Chord
C1	4.17'	10.00'	023°53'31"	S12°05'08"E	4.14'
C2	30.56'	20.00'	087°32'23"	S19°44'18"W	27.67'
C3	22.46'	30.00'	042°53'42"	S02°35'03"E	21.94'
C4	20.65'	30.00'	039°25'45"	S38°34'40"W	20.24'
C5	45.84'	30.00'	087°32'23"	S19°44'18"W	41.51'
C6	2.61'	25.00'	005°59'07"	N46°06'31"W	2.61'
C7	12.60'	36.00'	020°02'52"	N33°03'59"W	12.53'
C8	4.11'	200.00'	001°10'42"	S21°26'01"E	4.11'
C23	31.78'	160.18'	011°21'59"	N30°43'49"W	31.72'
C24	28.83'	145.18'	011°22'36"	N30°55'42"W	28.78'
C25	47.26'	254.94'	010°37'21"	N27°00'29"W	47.20'
C26	50.50'	269.94'	010°43'06"	N27°04'43"W	50.42'
C27	31.73'	288.20'	006°18'28"	N28°47'38"W	31.71'
C28	33.27'	273.20'	006°58'38"	N29°12'50"W	33.25'
C29	3.24'	10.00'	018°35'05"	S09°25'55"E	3.23'
C30	0.93'	10.00'	005°18'27"	S21°22'40"E	0.93'
C31	2.73'	30.00'	005°12'57"	N60°54'01"E	2.73'

1" = 40'
GRAPHIC SCALE
0 20' 40' 80'

Line Table		
Line #	Direction	Length
L1	S22°01'22"E	18.87'
L5	N86°49'37"W	26.07'
L6	N38°11'00"E	29.39'
L7	N38°11'00"E	99.61'
L8	S00°01'17"E	16.12'
L9	N38°11'00"E	13.84'
L13	S00°08'22"E	45.70'

LEGEND

- (C) CALCULATED DATA
- C# CURVE NUMBER (SEE CURVE TABLE)
- CB CHORD BEARING
- CCR CERTIFIED CORNER RECORD
- CHD CHORD LENGTH
- ℄ CENTER LINE
- CM CONCRETE MONUMENT
- DE DRAINAGE EASEMENT
- ID. IDENTIFICATION
- EW EDGE OF WATER
- F.A.C FLORIDA ADMINISTRATIVE CODE
- FIP FOUND IRON PIPE
- FNLD FOUND NAIL & DISK
- FPC FLORIDA POWER CORPORATON
- L# LINE NUMBER (SEE LINE TABLE)
- LB # LICENSE BUSINESS NUMBER
- LS LICENSED SURVEYOR
- NAVD NORTH AMERICAN VERTICAL DATUM
- NGVD NATIONAL GEODETIC VERTICAL DATUM
- (NR) NON RADIAL
- OHWL ORDINARY HIGH WATER LINE
- ORB OFFICIAL RECORDS BOOK
- PB PLAT BOOK
- PC POINT OF CURVATURE
- PG PAGE
- PI POINT OF INTERSECTION
- PRM PERMANENT REFERENCE MONUMENT
- PSM PROFESSIONAL SURVEYOR AND MAPPER
- PT POINT OF TANGENCY
- (R2) PER ORB 9126, PAGE 2378
- R/W RIGHT OF WAY
- SEC SECTION
- S/W SIDEWALK
- ESMT. EASEMENT
- TYP. TYPICAL
- UE UTILITY EASEMENT
- SET PRM (PERMANENT REFERENCE MONUMENT) 4"X4" CONCRETE MONUMENT, LB#6393 IN ACCORDANCE WITH CHAPTER 177.091(7), FLORIDA STATUTES (UNLESS OTHERWISE NOTED)
- SET PCP NAIL AND DISK, STAMPED LB# 6393 IN ACCORDANCE WITH CHAPTER 177.091, F.S.
- FOUND CONCRETE MONUMENT AS NOTED
- INDICATES CHANGE OF DIRECTION
- 29-27-27 SECTION, TOWNSHIP, RANGE
- PROPOSED LOCATION OF BOAT DOCK.
- SET 1/2" IRON ROD AND CAP (LB#6393) IN ACCORDANCE WITH CHAPTER 177.091(9), FS
- FOUND IRON PIPE MONUMENT AS NOTED
- SET PRM (PERMANENT REFERENCE MONUMENT) 1/8" DIAMETER MAG NAIL WITH 1 1/4" BRASS DISK INSCRIBED "PRM LB# 6393" IN ACCORDANCE WITH CHAPTER 177.091(8), FLORIDA STATUTES.

ASM
AMERICAN SURVEYING & MAPPING INC.
CERTIFICATION OF AUTHORIZATION NUMBER LB#6393
3191 MAGUIRE BOULEVARD, SUITE 200
ORLANDO, FLORIDA 32803
(407) 426-7979

NOTICE

THIS PLAT, AS RECORDED IN ITS GRAPHIC FORM, IS THE OFFICIAL DEPICTION OF THE SUBDIVIDED LANDS DESCRIBED HEREIN AND WILL IN NO CIRCUMSTANCES BE SUPPLANTED IN AUTHORITY BY ANY OTHER GRAPHIC OR DIGITAL FORM OF THE PLAT. THERE MAY BE ADDITIONAL RESTRICTIONS THAT ARE NOT RECORDED ON THIS PLAT THAT MAY BE FOUND IN THE PUBLIC RECORDS OF THIS COUNTY, AS PER 177.091 (27) F.A.C.

LAKE KILLARNEY SHORES

SHEET 3 OF 3

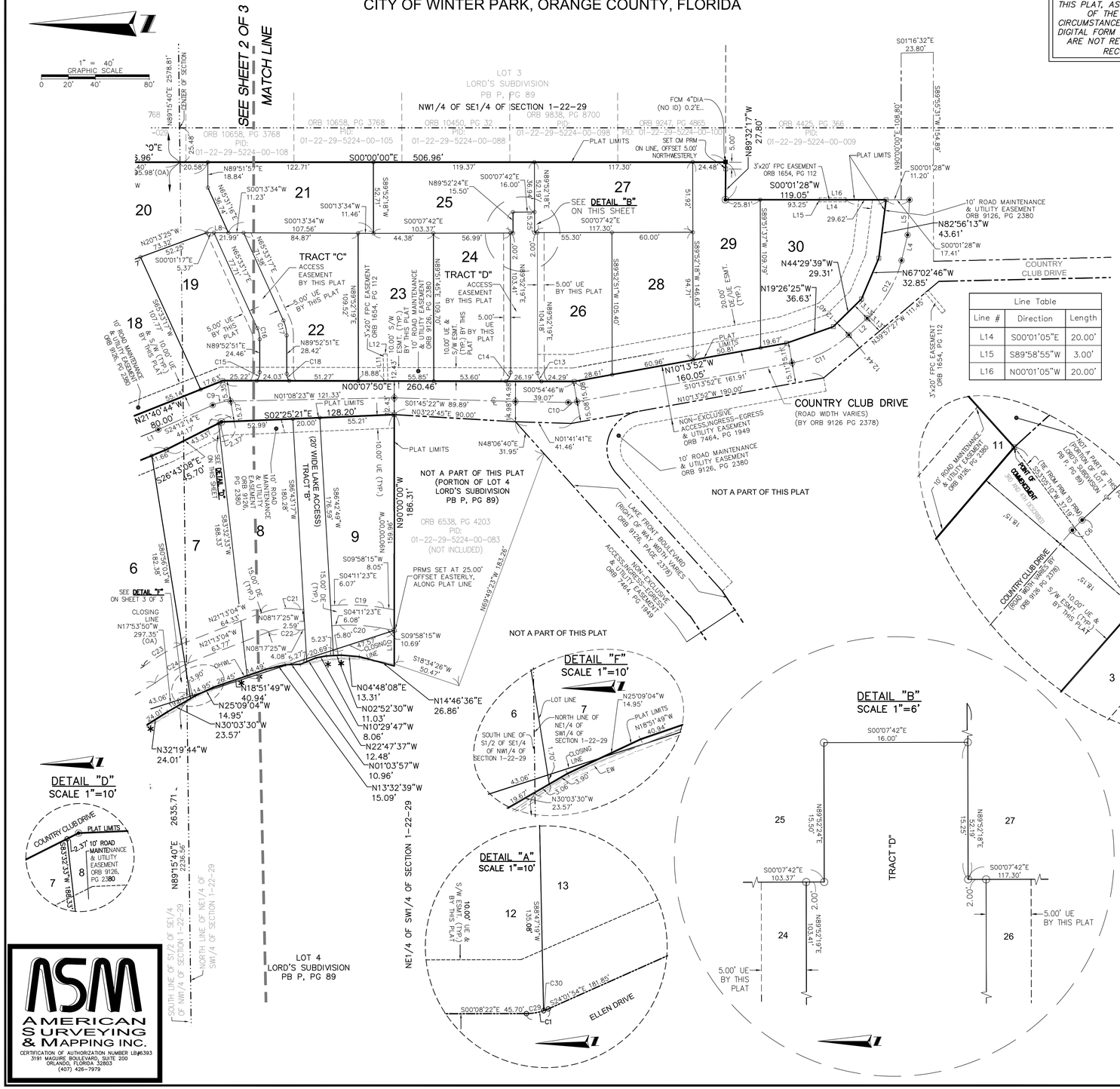
PLAT BOOK ____ PAGE ____

BEING A REPLAT OF PORTIONS OF LOTS 1 AND 4, LORD'S SUBDIVISION, AS RECORDED IN PLAT BOOK P, PAGE 89, OF THE PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, SECTION 1, TOWNSHIP 22 SOUTH, RANGE 29 EAST CITY OF WINTER PARK, ORANGE COUNTY, FLORIDA

NOTE:
FOR LEGAL DESCRIPTION AND NOTES SEE SHEET 1 OF 3.
FOR LEGEND SEE SHEET 2 OF 3.

NOTICE

THIS PLAT, AS RECORDED IN ITS GRAPHIC FORM, IS THE OFFICIAL DEPICTION OF THE SUBDIVIDED LANDS DESCRIBED HEREIN AND WILL IN NO CIRCUMSTANCES BE SUPPLANTED IN AUTHORITY BY ANY OTHER GRAPHIC OR DIGITAL FORM OF THE PLAT. THERE MAY BE ADDITIONAL RESTRICTIONS THAT ARE NOT RECORDED ON THIS PLAT THAT MAY BE FOUND IN THE PUBLIC RECORDS OF THIS COUNTY, AS PER 177.091 (27) F.A.C.



Curve Table					
Curve #	Length	Radius	Delta	Chord Bearing	Chord
C1	4.17'	10.00'	023°53'31"	S12°05'08"E	4.14'
C6	2.61'	25.00'	005°59'07"	N46°06'31"W	2.61'
C9	13.69'	34.00'	023°03'50"	N12°40'18"W	13.59'
C10	6.61'	34.00'	011°08'38"	S04°39'33"E	6.60'
C11	47.47'	85.00'	031°59'54"	S26°13'49"E	46.86'
C12	51.01'	85.00'	034°22'53"	S59°25'12"E	50.24'
C13	6.97'	7.94'	050°20'50"	S65°09'53"W	6.75'
C14	7.63'	7.99'	054°44'04"	S62°49'27"E	7.35'
C15	6.79'	9.99'	038°54'32"	S70°39'53"E	6.66'
C16	8.49'	20.03'	024°17'24"	N77°43'04"E	8.43'
C17	11.46'	27.00'	024°19'34"	N77°43'04"E	11.38'
C18	5.64'	8.00'	040°25'01"	S70°43'46"W	5.53'
C19	33.33'	152.51'	012°31'13"	N03°44'48"E	33.26'
C20	29.83'	137.51'	012°25'52"	N03°47'28"E	29.78'
C21	29.27'	52.26'	032°05'10"	N04°48'58"W	28.89'
C22	23.19'	37.26'	035°39'47"	N02°57'42"W	22.82'
C29	3.24'	10.00'	018°35'05"	S09°25'55"E	3.23'
C30	0.93'	10.00'	005°18'27"	S21°22'40"E	0.93'

Line Table		
Line #	Direction	Length
L14	S00°01'05"E	20.00'
L15	S89°58'55"W	3.00'
L16	N00°01'05"W	20.00'

Line Table		
Line #	Direction	Length
L2	N42°13'46"W	18.37'
L4	N76°36'39"W	19.06'
L5	N86°49'37"W	26.07'
L6	N38°11'00"E	29.39'
L8	S00°01'17"E	16.12'
L10	N90°00'00"W	26.35'
L11	N88°18'01"E	20.00'
L12	S01°41'59"E	3.00'

AMERICAN SURVEYING & MAPPING INC.
CERTIFICATION OF AUTHORIZATION NUMBER LB#6393
3191 MAGUIRE BOULEVARD, SUITE 200
ORLANDO, FLORIDA 32803
(407) 426-7979

Existing Buildings Inside and Within 200' of Killarney Shores

Property Address	Existing Setback (ft)
638 Country Club Drive	52
632 Country Club Drive	50
624/626 Country Club Drive	51.8
618/620 Country Club Drive	75.6
612 Country Club Drive	79.7
604/606 Country Club Drive	71.4
600/602 Country Club Drive	55
532/534 Country Club Drive	85.8
524 Country Club Drive	99.5
518 Country Club Drive	85.5
605 Lakefront Boulevard	50
595 Lakefront Boulevard	45

Existing Lakefront Setback Averages within 200' Per Lot

Lot 1

Property Address	Existing Setback (ft)
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
624/626 Country Club Drive	51.8
632 Country Club Drive	50
638 Country Club Drive	52
Average	**61.82**

Lot 2

Property Address	Existing Setback (ft)
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
624/626 Country Club Drive	51.8
632 Country Club Drive	50
638 Country Club Drive	52
Average	**63.4**

Lot 3

Property Address	Existing Setback (ft)
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
624/626 Country Club Drive	51.8
632 Country Club Drive	50
Average	**65.7**

Lot 4

Property Address	Existing Setback (ft)
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
Average	**70.4**

Lot 5

Property Address	Existing Setback (ft)
524 Country Club Drive	99.5
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
618/620 Country Club Drive	75.6
Average	**76.2**

Lots 6 & 7

Property Address	Existing Setback (ft)
518 Country Club Drive	85.5
524 Country Club Drive	99.5
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
612 Country Club Drive	79.7
Average	**78.2**

Lot 8

Property Address	Existing Setback (ft)
605 Lakefront Boulevard	50
518 Country Club Drive	85.5
524 Country Club Drive	99.5
600/602 Country Club Drive	55
604/606 Country Club Drive	71.4
Average	**72.3**

Lot 9

Property Address	Existing Setback (ft)
605 Lakefront Boulevard	50
518 Country Club Drive	85.5
532/534 Country Club Drive	85.8
600/602 Country Club Drive	55
595 Lakefront Boulevard	45
Average	**64.3**

Letter of Agreement

Lake Killarney, LLC (the Developer of Lake Killarney Shores) and David Robold (owner of 612 and 518 Country Club Drive, Winter Park, 32789) agree to the following regarding the setbacks and characteristics of the homes to be built on Lake Killarney as part of the Lake Killarney Shores development:

- Lot 1: Rear Setback 60'
- Lot 2: Rear Setback 61'
- Lot 3: Rear setback of 70' on the side adjacent to 612 Country Club Drive. Front setback reduced to 20'. Pool will be located furthest from 612 Country Club Drive and shall be at an elevation even to or recessed below ground level to avoid obstruction of views from 612 Country Club Drive.
- Lot 4: Rear setback 70'. Front setback reduced to 20'. Pool will be placed furthest from 612 Country Club Drive to avoid obstruction of views from 612 Country Club Drive.
- Lots 5-8: Rear setback of 70'.
- Lot 9: Rear setback of 85'. Front setback reduced to 20'. Pool will be located furthest from 518 Country Club Drive and shall be even to or recessed below ground level to avoid obstruction of views from 518 Country Club Drive.
- Pools can be placed between the agreed rear setback and the 50' lakefront setback.
- Pools on lots 3, 4, 8, and 9 will be at an elevation even to or recessed below ground level as necessary to insure those backyards are not at higher elevation compared to 612 and 518 Country Club Drive.
- Solid fence or privacy fence shall not extend beyond the rear corners of the house indoor living area at rear setback line. Beyond that point, the fence will be see-through picket fence.
- There will be no pool enclosures, cabanas, summer-kitchens, or similar type construction in the pool area or beyond the rear corners of the house indoor living area.
- Underground utilities will be attached to 518 and 612 Country Club Drive at no additional cost.
- All curbing will be replaced along right-of-way.
- Speed humps will be located in right-of-way north of the intersection of Country Club Drive and Lake Front Drive. However, they will not obstruct ingress/egress to the driveways located at 518 and 612 Country Club Drive.
- This agreement is subject to final approval by the Winter Park City Commission.

Date: 9/6/2018

Anil Deshpande
Lake Killarney, LLC
5401 S. Kirkman Road, Suite 640
Orlando, Florida 32819

Date: 9-11-18

David Robold
612 Country Club Drive
Winter Park, Florida 32789

Planning & TRANSPORTATION

401 South Park Avenue • Winter Park, Florida 32789

407-599-3324 • planning@cityofwinterpark.org
cityofwinterpark.org

Planning & Zoning Board
Staff Report for January 5, 2021 Meeting

Application for Henderson Hotel - Conditional Use 20-07, Comprehensive Plan Amendment 20-06, Rezoning 20-06

The approvals sought for the Henderson Hotel project include:

1. Vacating portions of Killarney Drive and Fairview Avenue, and in return, creating open space/park areas.
2. Amending the Comprehensive Plan Future Land Use policy text to add a new policy for this site in order to:
    a. Allow the Single Family and Office land use/zoning to change to Commercial and Open Space Recreational.
    b. Allow a maximum Floor Area Ratio (FAR - building size) of 99% in lieu of the maximum 60% FAR in Commercial and 20% FAR in Open Space Recreational.
    c. Allow a 5-story building (55 feet including four corner architectural features up to 73 feet in height) in lieu of the current maximum 4-stories (55 feet).
3. Amending the Zoning Code Commercial (C-3) and Open Space Recreational (PR) text to provide for the allowances above for only this specific property.
4. Amending the Future Land Use/Zoning Map from Single Family (R-1A) and Office on 2.09 acres of this site to Commercial (C-3) and 0.88 acres to Open Space Recreational (PR).
5. Approval of a Conditional Use for the specific hotel project of a 5-story, 132-room hotel with associated restaurant, ballroom/meeting space, and 235-space parking garage with one floor of underground parking.

Application Overview:

This agenda item has multiple requests (see above) for Comprehensive Plan amendments, Zoning Code amendments, and Conditional Use approval for a proposed 132 room, 5-story, hotel project. This Hotel project (known as the Henderson Hotel) is proposed on approximately 2.97 acres, adjacent to Lake Killarney between Beachview Avenue and Fairview Avenue. The Hotel project and associated outdoor areas are proposed on multiple properties that would be aggregated into one redevelopment site. The properties associated with this project currently hold two commercial/office buildings and four single family lakefront homes. The applicant is also proposing a park/open space area to the south of the hotel that is currently occupied by three single family homes, totaling 0.88 acres. Additionally, the right-of-way proposed to be vacated would be combined with all other properties to create the 2.97 acre development site. Additionally, a property that is under the ownership of one of the development group partners is a part of the application materials submitted, but is not proposed for any new development and the acreage or available FAR of that site is not being used in any calculations towards the hotel project. Per the attached Community Enhancement Agreement, this property was included in the application to show the developers intent to remove the blighted motel that is on the site. No entitlements outside of what would be allowed per the existing C-3 Zoning would be

considered for any future development/re-development of the site. This staff report will explain and assess the requests in the order outlined above, which is also the sequence of votes required.

History & Background:

The project proposal is comprised of lots that are a part of the Killarney Estates Plat (attached). Of note is that Block 8 of the subdivision has had the Killarney Drive portion vacated and the lots have been combined into one developable parcel. That property currently has the Hillstone Restaurant, though it has been a number of uses over the years.

Proposed Vacating of Killarney Drive/Fairview Avenue:

The initial request is for the City to vacate the public right-of-way of the portion of Killarney Drive that exists between Beachview Avenue and Fairview Avenue, and a portion of Fairview Avenue (exhibits attached). When the City vacates a roadway, the public right-of-way becomes private land split between the property owners on each side of the right-of-way. The total area that is requested to be vacated measures approximately 24,659 square feet (0.57 acres).

All of the right-of-way proposed to be vacated would be used as open space, which will remain available to the public via easements, which will prevent any future development on the open space area.

Comprehensive Plan:

One of the concerns voiced is the proximity to single-family homes. There are numerous policies within the Comprehensive Plan that discuss the protection of residential areas and neighborhoods, such as the following:

> **Policy 1-J-9: Protect Single-Family Residential Use in the Killarney Neighborhood from Non-Residential Land Use Encroachment:** The City shall preserve and protect the single-family residential land use within the Killarney neighborhood from commercial and office encroachment, excluding parcels that have or obtain Parking Lot (PL) zoning designation along the edges where commercial, office and residential meet. All development should include appropriate landscape buffers, including walls if necessary, so as not to have a negative impact on the residential neighborhood.
>
> **Policy 1-5.3.2: Protect Single Family & Low-Density Residential Property from Parking Garages:** The City shall prohibit above grade parking garages within 100 feet of a single family or low-density residential property.

There are also existing Policies of the Comprehensive Plan that could be interpreted as support for this proposal of this type:

> **Policy 1-5.1.6: Development of Large Lakefront Tracts to Provide Adequate Public Access for Public Purposes.** The development of large un-platted lakefront tracts shall include provisions for public access of a passive nature and public visual access along with agreements for the proper maintenance of these areas.

**it should be noted that these lots are platted, but would be re-platted (if approved) as one single large lakefront tract*

Most importantly, the following policy states the procedure for the conversion of residential lots, giving 4 criteria that should be met in order to support to change.

Policy 1-5.3.1: Criteria for Managing Encroachment of Nonresidential Uses into Established Residential Neighborhoods. The City shall require that any change in land use designation from residential to nonresidential comply with all of the following:

1. That this change shall not be a precedent toward other similar applications for change requesting similar land use as a matter of equity or fairness;
2. That the change can be demonstrated to be in the best interests of the City at large;
3. That the change can be demonstrated to be in the best interests of the adjacent residential area;
4. That residential use of the property is no longer a viable use.

The Winter Park Comp Plan gets amended quite often, but it is typically in relation to smaller-scale projects. This particular request is quite significant, but special Comp Plan Policies have been considered before, resulting in projects such as the Winter Park Wellness Center. These projects are typically unique in nature and require thorough analysis and consideration of the location.

Comprehensive Plan Policy Amendment Proposed by the Applicant:

The applicant is requesting that the City consider adopting the Comprehensive Plan amendment written by the applicant as outlined below:

Proposed New Policy 1-J-15: *In order to encourage appropriate redevelopment and in exchange for additional open space and traffic calming improvements, the following standards shall apply to the redevelopment of this property, as defined in Map FLUM-1-21a, a hotel and park with ancillary uses such as parking, restaurant and meeting rooms.*

- *44% Open Space which shall include walking/biking path;*
- *ISR 59%;*
- *FAR up to 0.99;*
- *Maximum height fifty-five feet (55') (with mansard/parapet roof up to 65')*
- *Up to four (4) architectural features seventy-three feet (73') in height;*
- *The building shall be seventy-five feet (75') setback from Lake Killarney at a height of only thirty-five feet (35'). Additional height must also meet the lakeside setback requirements in the City's Land Development Code;*
- *Public Use of Grove Avenue Park and associated parking spaces;*
- *The parking garage shall be concealed from view of single-family homes;*

- *Final traffic calming to be approved by City Commission at Conditional Use Permit; and*

- *Final architecture and massing to be approved by City Commission at Conditional Use Permit.*

This Subarea Policy shall prevail over any inconsistent policies found in the City's Comprehensive Plan Policies.

Impact of the Comprehensive Plan Policy change:

This proposed Comprehensive Plan policy would allow for the rezoning from residential to commercial that would otherwise be prohibited. It also permits increased Floor Area Ratio (FAR) from a current 60% maximum on the office zoned properties and a current 42% FAR on the single-family zoned properties, to up to 99% and the increase in building height from a 4-story maximum to 5-stories, though the overall allowable height of 55 feet would be maintained.

Assessment of the Proposed Comprehensive Plan Policy:

The new proposed Comprehensive Plan policy text contains two qualifiers for the proposed new development standards. The policy states that 44% of the site will be dedicated to open space that includes a walking/biking path and is accessible to the public. Currently, there are no open space requirements in the Land Development Code. The second provision is that traffic calming will be provided that will aid in the significant cut-through traffic that is occurring the area.

The Killarney Estates area has become a very serious traffic concern due to cut-through traffic. On two separate occasions, staff has observed the cut-through traffic during rush hour periods. The cut-through traffic is quite significant and also creates a great deal of speeding, illegal turns, and driving the wrong-way down one-way streets. It has been determined that the increased traffic is primarily due to mapping applications that direct drivers through these cut-through routes. The applicant performed a traffic study for the Killarney Estates neighborhood, which suggested some traffic calming measures. Staff and the Killarney Estates neighborhood worked together to find a solution that has been test fit for many months and has solved the cut-through traffic issues. Final plans for the improvements are at 90% completion and will be ready to be installed soon. The Henderson Hotel applicant offered to purchase the planter bollards that will block off Killarney Drive and Hill-Gray, the developers of the former Ranch Mall site, offered significant financial contributions toward the cost of a traffic calming area that will also be used as a parklet. The purchase of the planter bollards by the applicant, which will replace the temporary barricades, was not related to any request from the City and was one of the solutions that was discussed by the Traffic Study performed for the Killarney Estates residents by the applicant. The City also had no part of that traffic study and did not request that the applicant perform this study for the neighborhood. If any concern exists over the applicant having purchased these planter bollards for the Killarney Traffic Calming Project, the City will happily reimburse the cost of the planters to the applicant.

Proposed new Policy 1-J-15 provides that the community is being granted open space. A portion of the open space will result from the vacation of a portion of Killarney Drive and

Fairview Avenue. No building is being proposed in the area requested to be vacated, just open space granted to the public for use and access.

At an earlier work session on this project, there were discussions by the City Commission about the concept of a "Hotel Zoning" district. A "Hotel Zoning" district would benefit a "class" of users versus just one property ownership group so this method of addressing hotel applications did not move forward.

Conditional Use Application:

This Henderson Hotel project is a five-story, 129,100 square foot building that also has one level of underground parking. Floor plans have been provided for each of the floor levels. The parking garage is planned to be "valet" parking only, with no self-parking, similar to the Alfond Inn. The first floor includes the typical elements of a hotel including the lobby, restaurant/bar, meeting rooms/event space and areas for various 'back-of-house' hotel functions. Generally, the second through fifth floor will hold the hotel rooms and amenity space.

These hotel plans have been reviewed by all city departments via our Technical Advisory Committee (TAC). The functional elements of Fire Department access, storm water retention, building code compliance, etc. have been satisfied. There is one exception or variance that related to the lack of setbacks required for the upper floors. The Zoning Code requires additional setback for the upper floors (above the 2nd floor) in order to mitigate (break-up) the mass of the building and to avoid a flat vertical building wall.

This project, when originally submitted, did not have the required terracing and setbacks facing Orlando Avenue. This was a significant issue to planning staff as this is the view from Orlando Avenue to the 35,000 cars/day. This project is now setback from Orlando Avenue and the elevation façade has been significantly changed to incorporate articulations needed to break up the mass of the five-story façade. The planning staff is very appreciative of the significant changes in the plans to enhance the view from all sides of the building and look just as attractive as the rear (Lake Killarney). However, there is still an exception or variance required because the upper floors are not setback from the lower floors as required by code. The applicant has used various architectural treatments to soften the facades in these areas.

Parking Compliance:

The City's Code requires that a Hotel provide parking for the cumulative uses in the building based on one space for each hotel room (132 spaces), one space for each 3 seats in the restaurant/bar (with 220 seats = 73 spaces), and one space for each 250 square feet of meeting/ballroom room area (with 7,500 square feet = 30). That total is 235 spaces. The plans submitted show 235 parking spaces provided, thus no variances to the city's parking requirements are requested. The applicant is also proposing another 31 parking spaces exclusively for the open/park space and are not counted towards meeting the hotel parking requirements.

Traffic Impact:

The Henderson Hotel applicant submitted a Traffic Impact Analysis (TIA) completed by Kimley-Horn. The Planning & Transportation Department reviewed the TIA and provided comments to

the applicant during the Technical Advisory Committee Meeting held on October 14, 2020. A revised TIA was submitted to the City that incorporated and addressed the comments of City Staff.

The TIA shows that the proposed redevelopment would generate 917 new trips (when compared to the trips generated by the existing development). Of these 917 new trips, 59 of these trips would be during the PM peak hour (31 in/28 out). The TIA also shows the trip distribution at the three intersections included in the impact analysis. The three intersections analyzed are as follows:

- 17-92 & Beachview Avenue (unsignalized)
- 17-92 & Fairview Avenue (unsignalized)
- 17-92 & W Fairbanks Avenue (signalized)

As shown in the TIA (attached), the intersection of 17-92 & W Fairbanks Avenue would continue to operate at a Level of Service (LOS) F and the delay per vehicle at the intersection will increase 2.1 seconds (from 90 seconds/vehicle during Background Conditions to 92.1 seconds/vehicle during Buildout Conditions). While the eastbound approaches at the intersection of 17-92 & Fairview Avenue and the intersection of 17-92 & Beachview Avenue would continue to operate at LOS F, there is still capacity on both minor approaches. Under Buildout Conditions, the eastbound delay at Beachview Avenue is 66.2 seconds per vehicle with a 95 percentile queue of 2.7 vehicles during the PM peak hour (less than 75 feet if we assume 25 feet per vehicle). During the PM peak hour at Fairview Avenue, the eastbound delay under Buildout Conditions is 65.8 seconds per vehicle with a 95th percentile queue of 3.8 vehicles (less than 100 feet). While these are both increases in delay from 2021 background growth conditions, both approaches would be operating with Volume to Capacity Ratios (v/c) ratios of less than 1.0.

Why isn't a traffic signal being proposed as part of the project?

FDOT has a minimum spacing for traffic signals; along this stretch of roadway the minimum would be 1,320 ft between signals (FDOT is typically willing to look at a variance of up to 10 percent). 17-92 is an FDOT roadway and, near the proposed project, the two closest signals are at 17-92 & Fairbanks Ave and 17-92 & Morse Boulevard. The distance between these two signals is approximately 1,665 ft. Hypothetically, if a signal were placed smack in the middle of these two intersections, we would be at a spacing of around 830 ft between each signal; and this would be less than the minimum FDOT spacing (even with the 10 percent variance). The intersection of Fairview Avenue & 17-92 is approximately 960 ft from Fairbanks and approximately 705 ft from Morse Boulevard. And while the intersection of Beachview Avenue & 17-92 is approximately 1,255 ft from Fairbanks, it is only approximately 410 ft from Morse Boulevard. So, based on the FDOT criteria outlined above, signalizing one of these intersections doesn't meet FDOT standards.

Submittal Requirements for Conditional Use applications:

There have been a number of very valid questions about submittals and what is required to be provided by the applicant as a part of an application such as this. Staff has determined that the requirements have been met. There are a number of questions that have been raised by staff and members of the community that are important and very valid, but are not a required part of this portion of the process. The applications being considered are to request that the applicant be allowed to construct the proposed hotel. Site plans, existing

conditions plans, landscape plans, preliminary drainage plans, renderings of the facades and other similar plans are required at this point. In-depth geotechnical reports, structural engineering plans and more in-depth engineering plans are not a part of the Conditional Use process. Those plans are provided to be reviewed by City engineers, building plan reviewers and by the appropriate State agencies. If any of the plans are not approved, the project would not be able to be constructed. The main concerns from the public and staff are in regard to the use of underground parking in a location that is close to Lake Killarney and in a region with a high seasonal water level. Though not yet required in the application process, staff has requested that the applicant have a member of their development team available to answer any questions from the Planning & Zoning Board or City Commission in regard to the geotechnical feasibility of the project.

Standards for Consideration of Conditional Use Requests:

The decision of the Planning & Zoning Board shall consider the following applicable standards as a basis for its recommendations to the city commission. Before any proposed conditional use may be approved or approved with conditions by the City Commission, they shall conclude that the following applicable standards are satisfied. All actions to approve conditional uses by the Planning & Zoning Board and the City Commission shall presume as a matter of fact that the following applicable standards have been met by the applicant regardless of whether they are specifically enumerated in writing or in discussion as part of the motion for approval.

(1) That the proposed plan is consistent with all applicable goals, objectives, policies and standards in the city comprehensive plan;

(2) That the proposed plan meets or exceeds all other applicable minimum standards and requirements as set forth in this section and this article;

(3) That the proposed site plan and proposed use, business type, operating hours, noise, parking and traffic impact will be compatible with existing and anticipated land use activities in the immediate neighborhood and that such application will be compatible with the character of the surrounding area;

(4) That adequate facilities and services necessary to service the development associated with the proposed site plan will be available and in place at the time of impact of the development or phase thereof;

(5) That the building size, floor area ratio, height and mass are compatible with the zoning code requirements and consistent with the scale and character of the immediate neighborhood;

(6) That the proposed site is properly landscaped and irrigated in and around buildings, along sidewalks, and buffering neighboring land. The topographical and natural features of the site shall be given priority consideration, thus assuring the retention of the trees. The developer, furthermore, shall make provisions for the continued maintenance of landscaped areas, open spaces, and recreational areas. Other screening and buffering may also be required when necessary to protect the integrity of the surrounding area;

(7) That traffic generated from the proposed uses shall not, on a daily or peak-hour basis, degrade the level of service on adjacent roads or intersections or raise any traffic safety hazards; that driveway and curb cut access directs traffic onto more heavily traveled roadways and away from residential neighborhoods;

(8) That the site plan provides onsite parking to meet the Code required and expected demands of the proposed use;

(9) That adequate provisions have been made for light, air, access, and privacy in the arrangement of buildings, one to another and to neighbors;

(10) That the architectural design and aesthetic features of the building plans are compatible with the surrounding area;

(11) That adequate light shall be provided in all parking areas and interior streets. This shall include the replacement of light poles with appropriate illumination appropriately spaced;

(12) That the proposed use does not create through noise, intensity of activity, traffic, overflow, parking, stormwater runoff, etc., any conditions that degrade the value of adjacent properties, the peaceful use of adjacent properties, degrade the economy of adjacent businesses or negatively impacts the existing character or future use of the surrounding neighborhood or adjacent properties.

Summary

This Henderson Hotel project is a very unique project that would create a transformative change along the shores of Lake Killarney and would likely stimulate additional redevelopment along the 17-92 corridor. The project could provide economic stimulation to an area that currently has some blighted properties. The proposed hotel is to be built in an architectural style reminiscent of the 1880's historic lakefront hotels of the early days of Winter Park. Hotels provide many benefits to the local economy and to their immediate surroundings because they attract a continuous stream of guests interested in shopping and dining.

Staff Recommendation:

This hotel proposal is a very unique project that pays homage to the history and architecture of Winter Park's lakefront hotels of the past, and the numerous motels that once existed along Orlando Avenue. The proposal combines architectural design not often seen in modern development and significant open space that gives the public access to Lake Killarney.

The proposal also incorporates requests that would be in conflict with elements of the Winter Park Comprehensive Plan. The applicant has proposed a new Comprehensive Plan policy that would address these conflicts, which does create concern about this method of modifying the Comp Plan for large-scale projects being used in the future on other projects.

Staff feels that this unique project should be considered in open, public-hearings where the applicant can present their proposal and the community can provide their input on the proposed project. Staff is therefore providing an overall recommendation for approval to

allow this project to be fairly and publicly considered by the Planning and Zoning Board and the City Commission, but recommends that the Planning & Zoning Board and City Commission take each of the elements of the application and consider them individually. For this project to be approved as proposed, it would be dependent on approval of the elements listed below in the order shown. The project proposal package has the following requests:

- Vacation of right-of-way for portions of Killarney Drive and Fairview Avenue. The benefit to the vacation, as stated by the applicant, is the creation of protected open space/park areas. **(This part of the project will not be voted on by the Planning & Zoning Board)**

- Amending the Comprehensive Plan to add the following Policy specific only to the subject property:

  Proposed New Policy 1-J-15:

  In order to encourage appropriate redevelopment and in exchange for additional open space and traffic calming improvements, the following standards shall apply to the redevelopment of this property as a hotel and park with ancillary uses such as parking, restaurant and meeting rooms.

  - *44% Open Space which shall include walking/biking path;*
  - *Impervious Surface Ratio (ISR) 59%;*
  - *FAR up to 0.99;*
  - *Maximum height fifty-five feet (55') (with mansard/parapet roof up to 65')*
  - *Up to four (4) architectural features seventy-three feet (73') in height;*
  - *The building shall be seventy-five feet (75') setback from Lake Killarney at a height of only thirty-five feet (35'). Additional height must also meet the lakeside setback requirements in the City's Land Development Code;*
  - *Public Use of Grove Avenue Park and associated parking spaces;*
  - *The parking garage shall be concealed from view of single-family homes;*
  - *Final traffic calming to be approved by City Commission at Conditional Use Permit; and*
  - *Final architecture and massing to be approved by City Commission at Conditional Use Permit.*

  This Subarea Policy shall prevail over any inconsistent policies found in the City's Comprehensive Plan Policies. This proposed policy is in further of the

City's Visions Statement to "Plan our growth through a collaborative process that protects our City's timeless scale and character." This proposed policy "recognize[s] unique areas of the city and provide[s] a collaborative planning process that ensures quality development while reflecting the context and heritage of the area." The project will "enhance walking, biking, and recreational activities through a connected and integrated network of open space." It will "foster sustainable public and private parks and open spaces using state of the art practices and techniques" and "increase the connection to nature by incentivizing public and private green space through the design and development process." The proposed project will also "invest in innovative infrastructure to ensure that our means to get around is safe and efficient, through prioritization of pedestrians, bicyclist, and transit users."

- Amending the Future Land Use/Zoning Map of the Comprehensive Plan from Single Family (R-1A) and Office on 2.09 acres of this site to Commercial (C-3) and 0.88 acres to Open Space Recreational (PR).

- Rezoning of properties from Single Family (R-1A) and Office on 2.09 acres of this site to Commercial (C-3) and 0.88 acres to Open Space Recreational (PR).

- Conditional Use to construct a 5-story, 132-room hotel with associated restaurant, ballroom/meeting space, and 235-space parking garage with one floor of underground parking. The Conditional Use request would require that the project be built as depicted in the submitted plans.

If the project were to be considered for approval, staff would recommend the following conditions be added to any approval:

1. Without cost to the City, the property owner shall grant to the City for the benefit of the public, perpetual public access and recreational use easements for the Grove Avenue Park area (portion designated Open Space and Recreation FLU) and the open space /green area between Lake Killarney and the rear of the main hotel building, which includes the full portion of Lake Killarney Drive to be vacated at the developer's request. The easements must be in a form and with terms acceptable to the City Manager with counsel from the City Attorney. The easement for the Grove Avenue Park area shall include public rights of use of the park's associated vehicular parking spaces. The easement for the open space/green area between Lake Killarney and the hotel building will be non-exclusive and may retain to the hotel property owner certain rights of use that do not interfere with non-vehicular access by the public. The open space and public access easements required of the project must be executed and recorded prior to or simultaneously with the recording of the final plat and before any building permits are issued for vertical construction. The open space/park area improvements shown on the project plans shall be in place and ready for public use before any certificate of occupancy is issued for the hotel building. The property owner shall, at its cost, bear the full responsibility for the operation, maintenance and repair of the public access and use easement areas and the sod, landscaping and improvements thereon. Because the project is in part made up of the proposed vacated public right-of-way, staff recommends that this above described area should remain open for public recreational enjoyment.

2. The dock be only for pedestrian access with no boat or other vessel slips or moorings allowed. There shall only be one dock not to exceed 600 square feet in size and such will not extend further than 25 feet into Lake Killarney. All existing docks shall be removed prior to issuance of a certificate of occupancy. Restaurant and/or bar service is prohibited on the dock.

3. No watersports or watersports rentals be allowed.

4. Recommend adding the following language to the Proposed New Policy 1-J-15: Except for the permitted uses of hotel and park/open space, the ancillary uses of a restaurant and meeting rooms for the hotel, and the customary principal structures and accessory structures (such as a parking garage, hardscaping, stormwater facilities and a pedestrian dock) for the permitted uses, all other uses and structures are prohibited on the property regardless of the permitted uses and conditional uses of the underlying future land use map and zoning designations for the property.

5. The hotel provides shuttle service available for guests to reduce parking demand.

6. Prior to issuance of building permits for any building(s) to be developed upon the property, a final plat (replat) of such property meeting the requirements of Chapter 177, Florida Statutes and Chapter 58 of the Winter Park Code of Ordinances shall be submitted by the property owner and approved by the City and recorded in the Orange County public records.

7. The project shall be developed consistent with engineered drawings, elevations and architectural renderings submitted with the conditional use application and considered by the City Commission.

8. Due to the age and historic nature of some of the structures proposed to be demolished, the applicant shall make available any home that would meet the City of Winter Park Historic Preservation criteria proposed to be demolished to any party, who at their own expense, may move the structure to a site where it can be restored. Any interested party shall make arrangements with the applicant for the removal of the structure within 60 days of approval of the project.

9. The parking garage shall include at least two (2) EV charging spaces. The parking garage shall have at least 20% of the parking spaces wired for EV charging spaces.

10. The developer/property owner will remove invasive species along the lake shoreline in front of its property and will replant with species appropriate for littoral zones. In addition, property owner/developer as part of an approved Landscape Plan will save the existing four Cypress trees along the lakefront and will add an additional four Bald Cypress.

11. The exterior lighting shall use zero lot line lighting technology and directional lighting. The project shall use directional sound technology for all exterior speakers.

12. In addition to meeting all federal, state, City and St. Johns Water Management District requirements for retention and treatment of stormwater, property owner/developer shall: (a) at property owner's/developer's expense, construct a baffle box in the stormwater utility line running under Beachview Avenue in order to

treat the existing stormwater run-off from the City's streets into Lake Killarney; (b) after the City's acceptance of the baffle box construction described above, property owner/developer, at its sole cost and expense, remove the chain link fence in Lake Killarney which is currently located at the outfall of the stormwater utility line; and (c) enter into a Maintenance Schedule Agreement with City prior to issuance of certificate of occupancy to ensure the proper maintenance for the exfiltration system.

13. The property owner/developer shall cause the cessation of the motel operation at 271 S. Orlando Avenue upon commencement of construction of the project. The property owner/developer will demolish or renovate the buildings at 271 S. Orlando Avenue within six (6) months of its receipt of Certificate of Occupancy for the Henderson Hotel main building.

14. In accordance with Comprehensive Plan Future Land Use Policy 1-J-15, except for the hotel, open space and ancillary uses to the hotel stated herein, all other uses are prohibited on the property. There shall be a maximum of 132 Hotel Rooms. Ancillary uses of Ballroom/Meeting Space up to 7,500 square feet and Restaurant up to 220 seats.

15. 235 vehicle parking spaces shall be provided between the sub-surface parking garage and the first floor drop off area. 30 vehicle parking spaces shall be installed in the Grove Avenue Park area.

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF WINTER PARK, FLORIDA, TO VACATE AND ABANDON PORTIONS OF THE KILLARNEY DRIVE AND FAIRVIEW AVENUE PUBLIC RIGHTS-OF-WAY DEDICATED BY THE RESURVEY OF KILLARNEY ESTATES, ACCORDING TO THE PLAT THEREOF RECORDED AT PLAT BOOK L, PAGE 9, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA; PROVIDING FOR THE RESERVATION BY THE CITY OF A UTILITY AND DRAINAGE EASEMENT OVER THE ENTIRE AREA BEING VACATED AND ABANDONED; PROVIDING FOR CONFLICTS, CORRECTION OF ERRORS, RECORDING AND AN EFFECTIVE DATE.

WHEREAS, the City Commission desires to foster the development of the proposed Henderson Hotel redevelopment project (the "Hotel Project") encompassing the properties at 1201, 1310 & 1321 Fairview Avenue, 230,240 & 250 Killarney Drive, and 337 & 339 Grove Avenue so that an alternate mobility system can be developed to better serve the economic growth of this area and provide open space for residents; and

WHEREAS, the fee simple owner(s) of the properties adjacent to the public rights-of-way sought to be vacated and abandoned by this Ordinance made application to the City of Winter Park requesting the vacation and abandonment of certain portions of the Killarney Drive and Fairview Avenue public rights-of-way dedicated by the Resurvey of Killarney Estates, according to the plat thereof recorded at Plat Book L, Page 9, Public Records of Orange County, Florida as more specifically described in Exhibit "A" attached hereto; and

WHEREAS, this Ordinance meets the criteria established by Chapter 166 and Chapter 177, Florida Statutes and pursuant to and in compliance with law, notice has been given to abutting property owners and to the public by publication in a newspaper of general circulation to notify the public of this proposed Ordinance and of public hearings to be held; and

WHEREAS, the City Public Works Department has provided for participation by the public in the process by providing information as requested and has also rendered its recommendations to the City Commission; and

WHEREAS, the Winter Park City Commission has reviewed the proposed Ordinance and held advertised public hearings at which the City Commission has provided for public participation in the process in accordance with the requirements of state law; and

WHEREAS, the Winter Park City Commission finds that this Ordinance is in the best interest of the public safety, health and welfare.

NOW, THEREFORE, BE IT ENACTED as follows:

Section 1. Right-of-Way Vacation and Abandonment. The City Commission of the City of Winter Park hereby finds and declares that those portions of the Killarney Drive public right-of-way between Fairview Avenue and Beachview Avenue, as more particularly described in Exhibit "A" as Tract "A" are no longer necessary for a public right-of-way and said lands are vacated and abandoned as a public right-of-way subject to the easement reservation set forth in Section 3.

Section 2. Right-of-Way Vacation and Abandonment. The City Commission of the City of Winter Park hereby finds and declares that those portions of the Fairview Avenue public right-of-way between Killarney Drive and South Orlando Avenue, as more particularly described in Exhibit "A" as Tract "B" are no longer necessary for a public right-of-way and said lands are vacated and abandoned as a public right-of-way subject to the easement reservation set forth in Section 3.

Section 3. **Easement Reservation**. That the City of Winter Park hereby reserves and retains over, under and through the lands vacated and abandoned in Section 1 and Section 2 of this Ordinance, a utility and drainage easement until such time as existing utilities and drainage improvements are relocated as directed by the City and the street improvements are removed and an acceptable replacement utility and drainage easements are granted to the City. The City of Winter Park City Manager shall have the authority to record a notice of termination of this easement reservation on behalf of the City upon the City becoming satisfied that the conditions for termination of this easement reservation have occurred.

Section 4. Conflicts. In the event of any conflict between this Ordinance and any other ordinance or portions of ordinances, this Ordinance controls to the extent of the conflict.

Section 5. Correction of Errors. If it is discovered that there is any error or omission in legal description or in depiction of the portion of the rights-of-way vacated and abandoned by this Ordinance, such error may be corrected by subsequent curative document to be executed and approved by the City of Winter Park City Manager.

Section 6. Recording. This Ordinance is to be recorded in the public records of Orange County, Florida, but only after: (i) the proposed City of Winter Park ordinances adopting a comprehensive plan amendment and rezonings relating to the Hotel Project ("Hotel Project Ordinances") have been adopted and are effective; (ii) the developer obtains conditional use approval for the Hotel Project; (iii) the developer submits for approval and obtains City

approval of a final plat (replat) incorporating all parcels of land being developed with the Hotel Project (including the required open space) with said final plat satisfying all requirements for its recording in the public records; and (iv) the developer provides, to the satisfaction of the City, the open space and public access dedications required by the Hotel Project conditions of approval.

If the Hotel Project Ordinances are not adopted and/or do not become effective within sixty (60) days of the City Commission's adoption of this Ordinance, the City Commission may at any time thereafter rescind this Ordinance by adopting a resolution at a public meeting. If one or more of the Hotel Project Ordinances or the conditional use approval for the Hotel Project are denied by vote of the City Commission, this Ordinance (even if adopted) shall not become effective and will not be recorded in the public records.

If the final plat approval and open space and public access dedications required by the Hotel Project conditions of approval do not occur within six (6) months from the adoption of this Ordinance, the City Commission may at any time thereafter, rescind this Ordinance by adopting a resolution at a public meeting. It is intended that this Ordinance be recorded close in time with the recording of the final plat required for the Hotel Project.

Section 7. Effective Date. Subject to the provisions of Section 6 of this Ordinance, this Ordinance shall become effective upon its recording in the Public Records of Orange County after its adoption by the City Commission.

ADOPTED at a regular meeting of the City Commission of the City of Winter Park, Florida, on the _____ day of _____, 2021.

City of Winter Park

_____

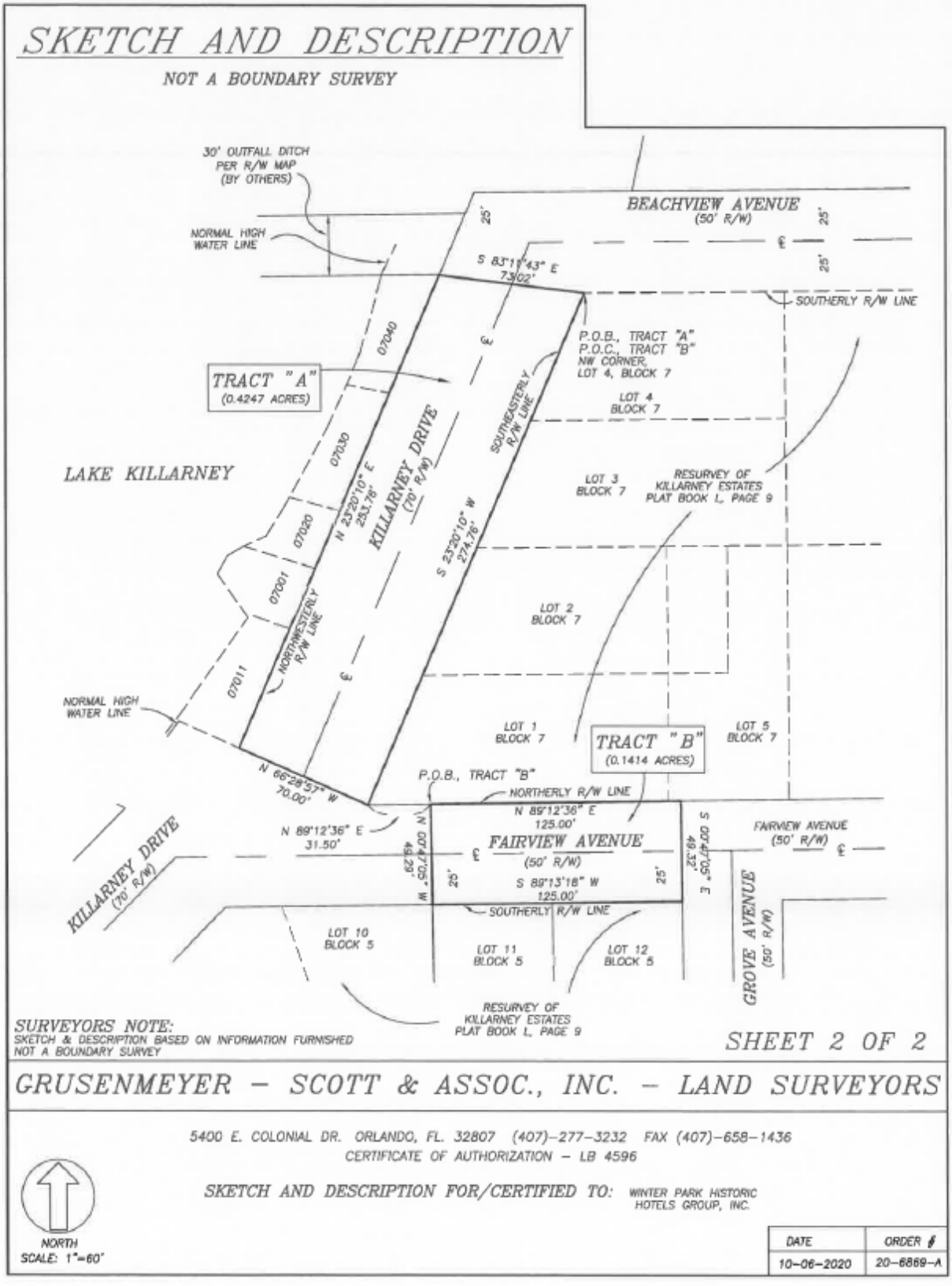
Mayor Steve Leary

ATTEST:

_____

Rene Cranis, City Clerk

Exhibit "A"


SKETCH AND DESCRIPTION
NOT A BOUNDARY SURVEY
30' OUTFALL DITCH PER R/W MAP (BY OTHERS)
NORMAL HIGH WATER LINE
BEACHVIEW AVENUE (50' R/W)
25'
S 83°11'43" E 73.02'
SOUTHERLY R/W LINE
P.O.B., TRACT "A"
P.O.C., TRACT "B"
NW CORNER, LOT 4, BLOCK 7
TRACT "A" (0.4247 ACRES)
07040
07030
07020
07001
07011
KILLARNEY DRIVE (70' R/W)
SOUTHEASTERLY R/W LINE
NORTHWESTERLY R/W LINE
N 23°20'10" E 253.76'
S 23°20'10" W 274.76'
LAKE KILLARNEY
LOT 4 BLOCK 7
LOT 3 BLOCK 7
RESURVEY OF KILLARNEY ESTATES PLAT BOOK L, PAGE 9
LOT 2 BLOCK 7
LOT 1 BLOCK 7
TRACT "B" (0.1414 ACRES)
LOT 5 BLOCK 7
NORMAL HIGH WATER LINE
N 66°28'57" W 70.00'
P.O.B., TRACT "B"
NORTHERLY R/W LINE
N 89°12'36" E 125.00'
N 89°12'36" E 31.50'
FAIRVIEW AVENUE (50' R/W)
S 89°13'18" W 125.00'
SOUTHERLY R/W LINE
S 00°47'05" E 49.23'
FAIRVIEW AVENUE (50' R/W)
GROVE AVENUE (50' R/W)
LOT 10 BLOCK 5
LOT 11 BLOCK 5
LOT 12 BLOCK 5
RESURVEY OF KILLARNEY ESTATES PLAT BOOK L, PAGE 9
SURVEYORS NOTE:
SKETCH & DESCRIPTION BASED ON INFORMATION FURNISHED
NOT A BOUNDARY SURVEY
SHEET 2 OF 2
GRUSENMEYER – SCOTT & ASSOC., INC. – LAND SURVEYORS
5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)-277-3232 FAX (407)-658-1436
CERTIFICATE OF AUTHORIZATION – LB 4596
SKETCH AND DESCRIPTION FOR/CERTIFIED TO: WINTER PARK HISTORIC HOTELS GROUP, INC.
NORTH
SCALE: 1"=60'
DATE 10-06-2020
ORDER # 20-6869-A

SKETCH AND DESCRIPTION

NOT A BOUNDARY SURVEY

LEGAL DESCRIPTION

Part of Killarney Drive and Fairview Avenue, RESURVEY OF KILLARNEY ESTATES, as recorded in Plat Book L, Page 9, of the Public Records of Orange County, Florida, lying in Section 12, Township 22 South, Range 29 East, being Tracts "A" & "B" described as follows:

TRACT "A"
Begin at the Northwest corner of Lot 4, Block 7, said Resurvey of Killarney Estates; thence S.23°20'10"W., along the Southeasterly right-of-way line of said Killarney Drive, a distance of 274.76 feet to the Northerly right-of-way line of said Fairview Avenue; thence N.66°28'57"W., to the Northwesterly right-of-way line of said Killarney Drive; thence N.23°20'10"E., along said Northwesterly right-of-way line, a distance of 253.76 feet; thence S.83°11'43"E., a distance of 73.02 feet to the POINT OF BEGINNING.
Containing 18,498 square feet or 0.42 acres, more or less.
AND
TRACT "B"
Commence at the Northwest corner of Lot 4, Block 7, said Resurvey of Killarney Estates; thence S.23°20'10"W., along the Southeasterly right-of-way line of said Killarney Drive, a distance of 274.76 feet to the Northerly right-of-way line of said Fairview Avenue; thence N.89°12'36"E., along said Northerly right-of-way line, 31.50 feet to the POINT OF BEGINNING; thence continue N.89°12'36"E., along said Northerly right-of-way line, a distance of 125.00 feet; thence S.00°47'05"E., a distance of 49.32 feet to the Southerly right-of-way line of said Fairview Avenue; thence S.89°13'18"W., a distance of 125.00 feet; thence N.00°47'05"W., a distance of 49.29 feet to the POINT OF BEGINNING. Containing 6,163 square feet or 0.14 acres, more or less.

SURVEYORS NOTE:
SKETCH & DESCRIPTION BASED ON INFORMATION FURNISHED
NOT A BOUNDARY SURVEY

GRUSENMEYER – SCOTT & ASSOC., INC. – LAND SURVEYORS

5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)-277-3232 FAX (407)-658-1436
CERTIFICATE OF AUTHORIZATION – LB 4596

LEGEND –

P	= PLAT
F	= FIELD
I.P.	= IRON PIPE
I.R.	= IRON ROD
C.M.	= CONCRETE MONUMENT
SET I.R.	= 1/2" I.R. w/#LB 4596
REC.	= RECOVERED
P.O.B.	= POINT OF BEGINNING
P.O.C.	= POINT OF COMMENCEMENT
℄	= CENTERLINE
N&D	= NAIL & DISK
R/W	= RIGHT-OF-WAY
ESMT.	= EASEMENT
DRAIN.	= DRAINAGE
UTIL.	= UTILITY
C.L.F.C.	= CHAIN LINK FENCE
W.D.F.C.	= WOOD FENCE
C/B	= CONCRETE BLOCK
P.C.	= POINT OF CURVATURE
P.T.	= POINT OF TANGENCY
DESC.	= DESCRIPTION

LEGEND –

R	= RADIUS
L	= ARC LENGTH
D	= DELTA
C	= CHORD
C.B.	= CHORD BEARING
P.O.L.	= POINT ON LINE
TYP.	= TYPICAL
P.R.C.	= POINT OF REVERSE CURVATURE
P.C.C.	= POINT OF COMPOUND CURVATURE
RAD.	= RADIAL
N.R.	= NON-RADIAL
W.P.	= WITNESS POINT
CALC.	= CALCULATED
P.R.M.	= PERMANENT REFERENCE MONUMENT
F.F.	= FINISHED FLOOR ELEVATION
B.S.L.	= BUILDING SETBACK LINE
B.M.	= BENCHMARK
B.B.	= BASE BEARING
CONC.	= CONCRETE
FIRM	= FLOOD INSURANCE RATE MAP
I.D.	= IDENTIFICATION

NOTES:
1. THE UNDERSIGNED DOES HEREBY CERTIFY THAT THIS SKETCH MEETS THE MINIMUM TECHNICAL STANDARDS SET FORTH BY THE FLORIDA BOARD OF PROFESSIONAL LAND SURVEYORS IN CHAPTER 61G17-6 FLORIDA ADMINISTRATIVE CODE PURSUANT TO SECTION 472.027 FLORIDA STATUTES.
2. UNLESS EMBOSSED WITH SURVEYOR'S SEAL, THIS SKETCH IS NOT VALID AND IS PRESENTED FOR INFORMATIONAL PURPOSES ONLY.
3. THIS SKETCH WAS PREPARED FROM TITLE INFORMATION FURNISHED TO THE SURVEYOR. THERE MAY BE OTHER RESTRICTIONS OR EASEMENTS THAT AFFECT THIS PROPERTY.
4. NO UNDERGROUND IMPROVEMENTS HAVE BEEN LOCATED UNLESS OTHERWISE SHOWN.
5. THIS SKETCH IS PREPARED FOR THE SOLE BENEFIT OF THOSE CERTIFIED TO AND SHOULD NOT BE RELIED UPON BY ANY OTHER ENTITY.
6. DIMENSIONS SHOWN FOR THE LOCATION OF IMPROVEMENTS HEREON SHOULD NOT BE USED TO RECONSTRUCT BOUNDARY LINES.
7. BEARINGS, IF SHOWN, ARE BASED ASSUMED DATUM AND ON THE LINE SHOWN AS BASE BEARING (B.B.)
8. ELEVATIONS, IF SHOWN, ARE BASED ON NGVD 1929, UNLESS OTHERWISE NOTED.

DATE	ORDER #
10-06-2020	20-6869-A

SCALE: 1" = 60'

NORTH

CERTIFIED BY:

R.L.S. #

JAMES W. SCOTT, R.L.S # 4801 TOM X. GRUSENMEYER, R.L.S. # 4714

SKETCH AND DESCRIPTION FOR/CERTIFIED TO: WINTER PARK HISTORIC HOTELS GROUP, INC.

ORDINANCE NO. _____

AN ORDINANCE TO AMEND CHAPTER 58, LAND DEVELOPMENT CODE, ARTICLE I, "COMPREHENSIVE PLAN" SO AS TO ADOPT IN THE FUTURE LAND USE ELEMENT, A NEW POLICY OUTLINING PERMITTED USES, INCREASED INTENSITIES, BUILDING HEIGHTS AND OTHER DEVELOPMENT STANDARDS APPLYING ONLY TO THE BLOCK BOUNDED BY BEACHVIEW AVENUE, ORLANDO AVENUE, GROVE AVENUE, FAIRVIEW AVENUE AND LAKE KILLARNEY DRIVE AND AMENDING THE COMPREHENSIVE PLAN FUTURE LAND USE MAP SO AS TO CHANGE THE FUTURE LAND USE DESIGNATIONS OF OFFICE AND SINGLE FAMILY RESIDENTIAL TO COMMERCIAL FUTURE LAND USE FOR THE PROPERTIES AT 1298 BEACHVIEW AVENUE, 1201/1321 FAIRVIEW AVENUE AND 230/240/250 KILLARNEY DRIVE FROM SINGLE FAMILY RESIDENTIAL TO OPEN SPACE AND RECREATION FUTURE LAND USE ON THE PROPERTIES AT 1310 FARIVIEW AVE AND 337/349 GROVE AVENUE, FROM NO DESIGNATION TO COMMERCIAL FUTURE LAND USE FOR A VACATED PORTION OF KILLARNEY DRIVE, AND FROM NO DESIGNATION TO OPEN SPACE AND RECREATION FUTURE LAND USE FOR A VACATED PORTION OF FAIRVIEW AVENUE; PROVIDING FOR CODIFICATION, SEVERABILITY, CONFLICTS AND AN EFFECTIVE DATE.

WHEREAS, Sections 163.3184 and 163.3187, Florida Statutes, establishes a process for adoption of comprehensive plans or plan amendments amending the future land use designation of property along with text amendments relating thereto; and

WHEREAS, this Comprehensive Plan amendment meets the requirements and criteria established by Chapter 163 and 166, Florida Statutes, including without limitation, the small-scale comprehensive plan amendment provisions of Section 163.3187, Florida Statutes; and

WHEREAS, pursuant to and in compliance with law, notice has been given to Orange County and to the public by publication in a newspaper of general circulation to notify the public of this proposed Ordinance and of public hearings to be held; and

WHEREAS, the Winter Park Planning and Zoning Board, acting as the designated Local Planning Agency, has reviewed and recommended adoption of the proposed Comprehensive Plan amendment, having held an advertised public hearing on January 5, 2021, provided for participation by the public in the process, and rendered its recommendations to the City Commission; and

WHEREAS, the Winter Park City Commission has reviewed the proposed Comprehensive Plan amendment and held advertised public hearings on January 13, 2021 and on January 27, 2021 and provided for public participation in the process in accordance with the requirements of state law and the procedures adopted for public participation in the planning process.

NOW THEREFORE BE IT ENACTED BY THE CITY COMMISSION OF THE CITY OF WINTER PARK, FLORIDA, AS FOLLOWS:

SECTION 1. FLU MAP AMENDMENT. That Chapter 58 "Land Development Code", Article I, "Comprehensive Plan" Future Land Use plan map is hereby amended so as to change the Future Land Use map designation of Office and Residential to Commercial on the properties at 1298 Beachview Avenue, 1201/1321 Fairview Avenue And 230/240/250 Killarney Drive, more particularly described as follows:

Orange County Property Tax ID #s: 12-22-29-4172-07-040, 12-22-29-4172-07-030, 12-22-29-4172-07-020, 12-22-29-4172-07-011, 12-22-29-4172-07-080, 12-22-29-4172-07-001 and 12-22-29-4172-07-060 as identified on the map attached hereto as Exhibit "1".

SECTION 2. FLU MAP AMENDMENT That Chapter 58 "Land Development Code", Article I, "Comprehensive Plan" Future Land Use plan map is hereby amended so as to change the Future Land Use map designation of Single Family Residential to Open Space and Recreation on the properties at 1310 Fairview Avenue and 337/349 Grove Avenue, more particularly described as follows:

Orange County Property Tax ID #s: 12-22-29-4172-05-110, 12-22-29-4172-05-170, and 12-22-29-4172-05-160 as identified on the map attached hereto as Exhibit "2".

SECTION 3. FLU MAP AMENDMENT. That Chapter 58 "Land Development Code", Article I, "Comprehensive Plan" Future Land Use plan map is hereby amended so as to change from no designation to Commercial Future Land Use for the vacated portion of Killarney Drive identified as Tract A and legally described on Exhibit "3" attached hereto.

SECTION 4. FLU MAP AMENDMENT. That Chapter 58 "Land Development Code", Article I, "Comprehensive Plan" Future Land Use plan map is hereby amended so as to change from no designation to Open Space and Recreation Future Land Use for the vacated portion of Fairview Avenue identified as Tract B and legally described on Exhibit "3" attached hereto.

SECTION 5. **TEXT AMENDMENT**. That Chapter 58 "Land Development Code", Article I, "Comprehensive Plan", is hereby amended by adding a new Future Land Use map , FLUM-1-21a as shown in Exhibit "A" relating to a new policy, and a new Future Land Use text Policy 1-J-15 within the U.S. Highway 17-92 Corridor Planning Area "J" of the Goals, Objectives and Policies to read as follows (words that are underlined are additions; words that are ~~stricken~~ are deletions; and all other provisions of such sections not included shall remain unchanged):

PLANNING AREA J: U.S. HIGHWAY 17-92 CORRIDOR

Policy 1-J-15: In order to encourage appropriate redevelopment and in exchange for additional open space and traffic calming improvements, the following standards shall apply to the redevelopment of the property defined in Map FLUM-1-21a, as a hotel and park with ancillary uses such as parking, restaurant and meeting rooms.

- 44% Open Space which shall include walking/biking path;

- ISR 59%;
- FAR up to 0.99;
- Maximum height fifty-five feet (55') (with mansard/parapet roof up to 65');
- Up to four (4) architectural features seventy-three feet (73') in height;
- The building shall be seventy-five feet (75') setback from Lake Killarney at a height of only thirty-five feet (35'). Additional height must also meet the lakeside setback requirements in the City's Land Development Code;
- Public Use of Grove Avenue Park (designated Open Space and Recreation FLU) and associated parking spaces;
- The parking garage shall be concealed from view of single-family homes;
- Final traffic calming to be approved by City Commission at Conditional Use Permit; and
- Final architecture and massing to be approved by City Commission at Conditional Use Permit.

This Subarea Policy shall prevail to govern the development of the property defined in Map FLUM-1-21a over any inconsistent goals, objectives and policies found in the City's Comprehensive Plan.

SECTION 6. **Codification**. Sections 1 through 5 of Ordinance shall be incorporated into the Winter Park Comprehensive Plan. Any section, paragraph number, letter and/or any heading may be changed or modified as necessary to effectuate the foregoing intent. Grammatical, typographical and similar or like errors may be corrected, and additions, alterations, and omissions not affecting the construction or meaning of this Ordinance and the Comprehensive Plan may be freely made.

SECTION 7. Severability. If any Section or portion of a Section of this Ordinance proves to be invalid, unlawful, or unconstitutional, it shall not be held to invalidate or impair the validity, force, or effect of any other Section or part of this Ordinance.

SECTION 8. Conflicts. In the event of a conflict between this Ordinance and any other ordinance of the City of Winter Park or part thereof, this Ordinance shall control to the extent of the conflict.

SECTION 9. Effective Date of Ordinance. The effective date of this Ordinance shall be 31 days after its adoption unless the comprehensive plan amendment(s) set forth herein is/are timely challenged in accordance with general law. If timely challenged, this ordinance shall become effective in accordance with general law.

ADOPTED at a regular meeting of the City Commission of the City of Winter Park, Florida, held in City Hall, Winter Park, on this _____ day of _____, 2021.

_____

Mayor Steve Leary

Attest:

_____

City Clerk

Exhibit
"A"

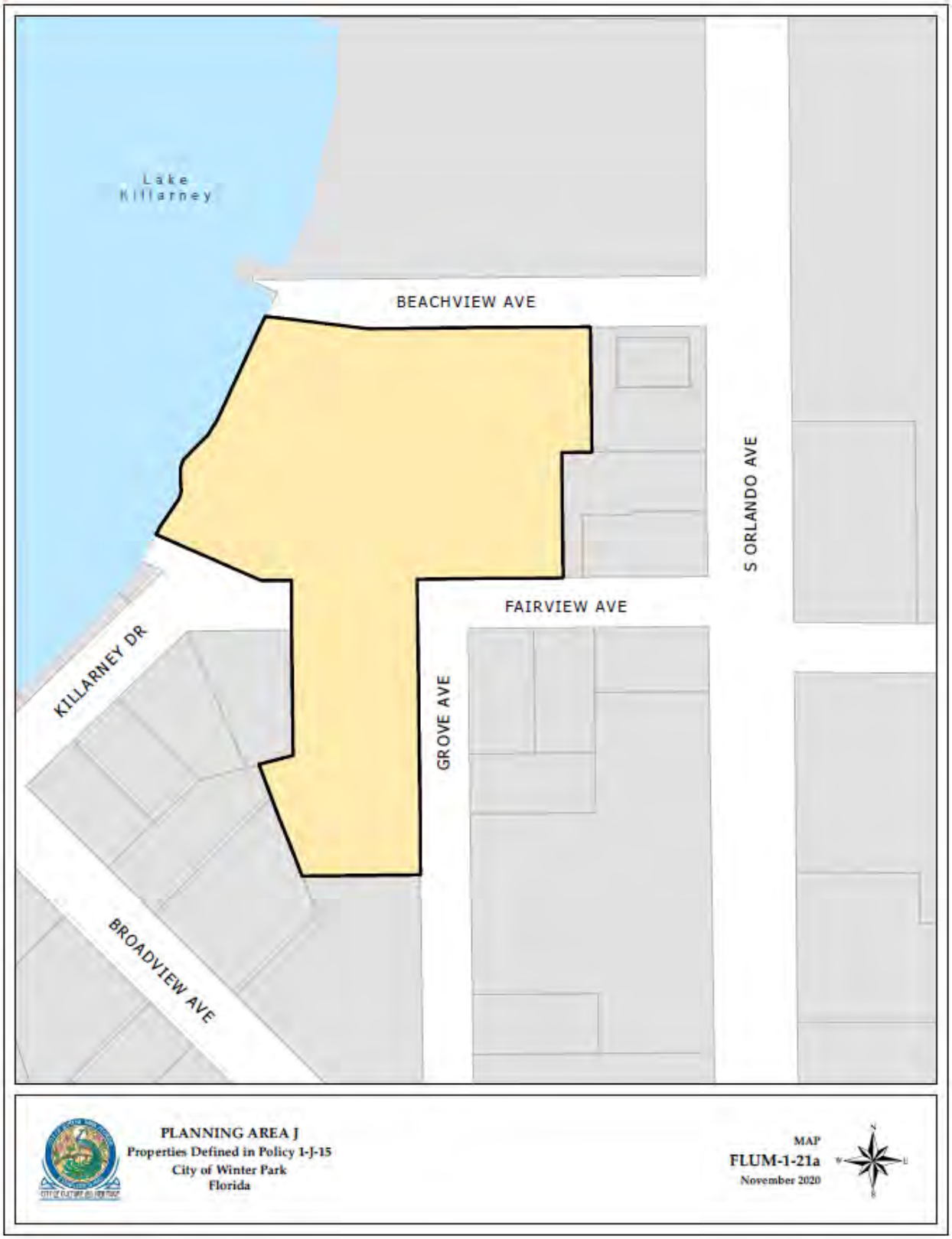

Lake
Killarney
BEACHVIEW AVE
S ORLANDO AVE
FAIRVIEW AVE
KILLARNEY DR
GROVE AVE
BROADVIEW AVE
PLANNING AREA J
Properties Defined in Policy 1-J-15
City of Winter Park
Florida
MAP
FLUM-1-21a
November 2020

Exhibit "1"

Lake
Killarney
BEACHVIEW AVE
S ORLANDO AVE
FAIRVIEW AVE
KILLARNEY DR
GROVE AVE
BROADVIEW AVE
EXHIBIT "1"
City of Winter Park
Florida

Exhibit “2”

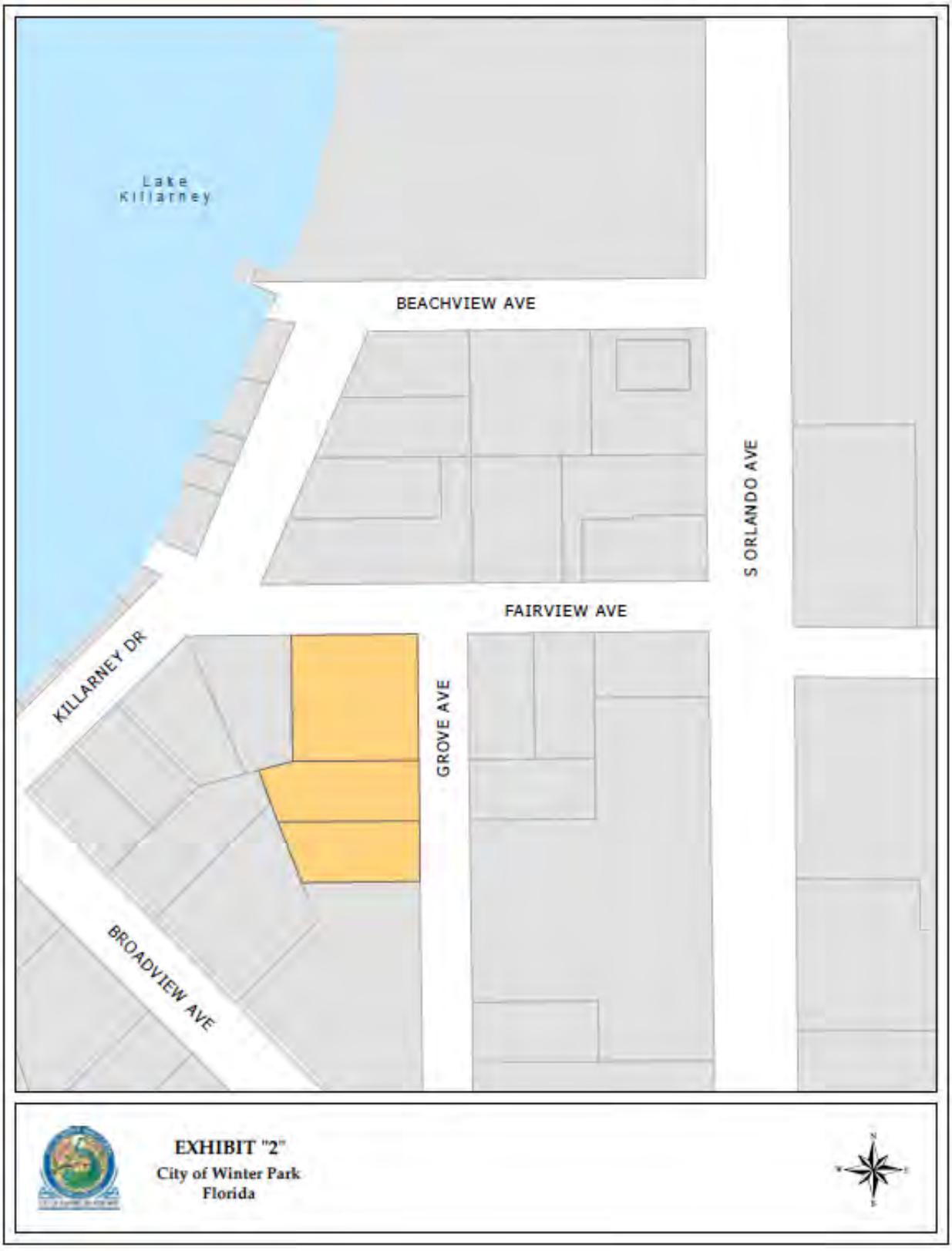
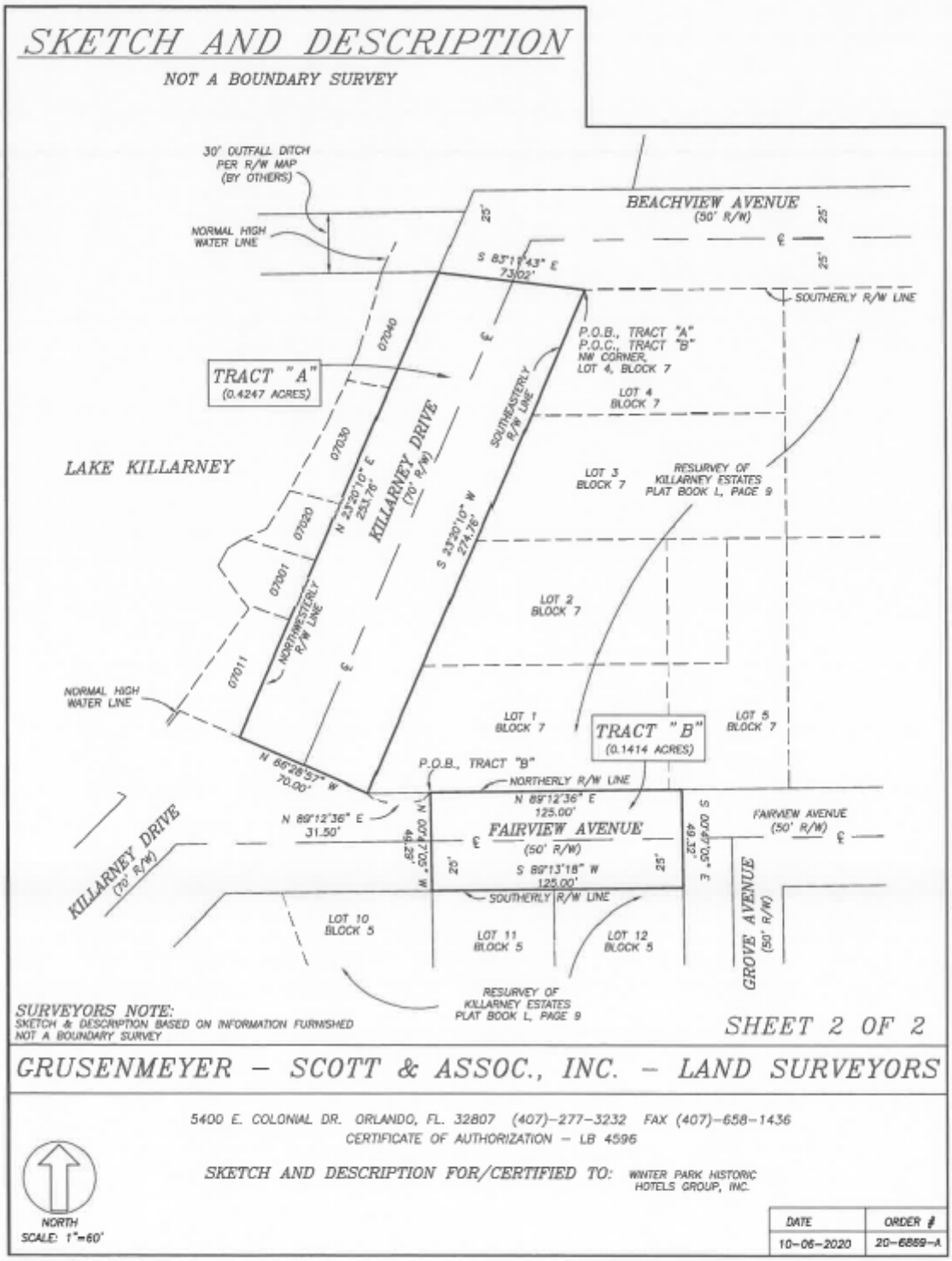

Lake
Killarney
BEACHVIEW AVE
S ORLANDO AVE
FAIRVIEW AVE
KILLARNEY DR
GROVE AVE
BROADVIEW AVE
EXHIBIT "2"
City of Winter Park
Florida

Exhibit "3"


SKETCH AND DESCRIPTION
NOT A BOUNDARY SURVEY
30' OUTFALL DITCH PER R/W MAP (BY OTHERS)
NORMAL HIGH WATER LINE
BEACHVIEW AVENUE (50' R/W)
S 83°11'43" E 73.02'
SOUTHERLY R/W LINE
P.O.B., TRACT "A"
P.O.C., TRACT "B"
NW CORNER, LOT 4, BLOCK 7
TRACT "A" (0.4247 ACRES)
LOT 4 BLOCK 7
LAKE KILLARNEY
KILLARNEY DRIVE (70' R/W)
SOUTHEASTERLY R/W LINE
N 23°30'10" E 253.76'
S 23°30'10" W 274.76'
LOT 3 BLOCK 7
RESURVEY OF KILLARNEY ESTATES PLAT BOOK L, PAGE 9
LOT 2 BLOCK 7
NORTHWESTERLY R/W LINE
NORMAL HIGH WATER LINE
LOT 1 BLOCK 7
TRACT "B" (0.1414 ACRES)
LOT 5 BLOCK 7
N 86°28'57" W 70.00'
P.O.B., TRACT "B"
NORTHERLY R/W LINE
N 89°12'36" E 125.00'
N 89°12'36" E 31.50'
FAIRVIEW AVENUE (50' R/W)
S 89°13'18" W 125.00'
SOUTHERLY R/W LINE
FAIRVIEW AVENUE (50' R/W)
KILLARNEY DRIVE (70' R/W)
LOT 10 BLOCK 5
LOT 11 BLOCK 5
LOT 12 BLOCK 5
GROVE AVENUE (50' R/W)
RESURVEY OF KILLARNEY ESTATES PLAT BOOK L, PAGE 9
SURVEYORS NOTE:
SKETCH & DESCRIPTION BASED ON INFORMATION FURNISHED
NOT A BOUNDARY SURVEY
SHEET 2 OF 2
GRUSENMEYER – SCOTT & ASSOC., INC. – LAND SURVEYORS
5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)-277-3232 FAX (407)-658-1436
CERTIFICATE OF AUTHORIZATION – LB 4596
SKETCH AND DESCRIPTION FOR/CERTIFIED TO: WINTER PARK HISTORIC HOTELS GROUP, INC.
NORTH
SCALE: 1"=60'
DATE 10-06-2020
ORDER # 20-6859-A

ORDINANCE NO. _____

AN ORDINANCE TO AMEND CHAPTER 58, LAND DEVELOPMENT CODE, ARTICLE III, "ZONING", SO AS TO AMEND THE TEXT OF SECTION 58-76 COMMERCIAL (C-3) DISTRICT AND SECTION 58-81 PARKS AND RECREATION (PR) DISTRICT, TO IMPLEMENT COMPREHENSIVE PLAN FUTURE LAND USE POLICY 1-J-15 AND PROVIDE PERMITTED USES, INCREASED INTENSITIES, BUILDING HEIGHTS AND OTHER DEVELOPMENT STANDARDS APPLYING ONLY TO THE BLOCK BOUNDED BY BEACHVIEW, ORLANDO, FAIRVIEW AVENUES AND LAKE KILLARNEY; AMEND THE OFFICIAL ZONING MAP TO CHANGE FROM OFFICE (O-1) ZONING AND SINGLE FAMILY RESIDENTIAL (R-1A) ZONING TO COMMERCIAL (C-3) DISTRICT ZONING ON THE PROPERTIES AT 1298 BEACHVIEW AVENUE, 1201/1321 FAIRVIEW AVENUE AND 230/240/250 KILLARNEY DRIVE, AND FROM SINGLE FAMILY RESIDENTIAL (R-1A) TO PARK AND RECREATION (PR) ON THE PROPERTIES AT 1310 FAIRVIEW AVENUE AND 337/349 GROVE AVENUE; AMEND THE OFFICIAL ZONING MAP TO CHANGE FROM NO DESIGNATION TO COMMERCIAL (C-3) DISTRICT ZONING FOR A VACATED PORTION OF KILLARNEY DRIVE; AMEND THE OFFICIAL ZONING MAP TO CHANGE FROM NO DESIGNATION TO PARK AND RECREATION (PR) ZONING DISTRICT FOR A VACATED PORTION OF FAIRVIEW AVENUE; PROVIDING FOR SEVERABILITY, CONFLICTS AND AN EFFECTIVE DATE.

WHEREAS, the owner of the subject property has requested a Zoning map amendment consistent with the Comprehensive Plan, and such municipal zoning meets the criteria established by Chapter 166, Florida Statutes and pursuant to and in compliance with law to notify the public of this proposed Ordinance and of public hearings to be held; and

WHEREAS, on January 5, 2021, the Planning and Zoning Board of the City of Winter Park held a public hearing and made a recommendation to the City Commission concerning the adoption of this Ordinance; and

WHEREAS, the City Commission of the City of Winter Park held a duly noticed public hearing on the proposed zoning change set forth hereunder and considered findings and advice of staff, citizens, and all interested parties submitting written and oral comments and supporting data and analysis, and after complete deliberation, hereby finds the requested change consistent with the City of Winter Park Comprehensive Plan and that sufficient, competent, and substantial evidence supports the zoning change set forth hereunder; and

WHEREAS, the City Commission hereby finds that this Ordinance serves a legitimate government purpose and is in the best interests of the public health, safety, and welfare of the citizens of Winter Park, Florida.

NOW THEREFORE BE IT ENACTED BY THE CITY COMMISSION OF THE CITY OF

WINTER PARK, FLORIDA, AS FOLLOWS:

SECTION 1. Official Zoning Map Amendment. That Chapter 58 "Land Development Code", Article III, "Zoning" and the Official Zoning Map is hereby amended so as to change the zoning designation of Office (O-1) and Single Family Residential (R-1A) to Commercial (C-3) on the properties at 1298 Beachview Avenue, 1201/1321 Fairview Avenue And 230/240/250 Killarney Drive, more particularly described as follows:

Orange County Property Tax ID #s: 12-22-29-4172-07-040, 12-22-29-4172-07-030, 12-22-29-4172-07-020, 12-22-29-4172-07-011, 12-22-29-4172-07-080, 12-22-29-4172-07-001 and 12-22-29-4172-07-060 as identified on the map attached hereto as Exhibit "1".

SECTION 2. Official Zoning Map Amendment. That Chapter 58 "Land Development Code", Article III, "Zoning" and the Official Zoning Map is hereby amended so as to change the zoning designation of Single Family Residential (R-1A) to Parks and Recreation (PR) on the properties at 1310 Fairview Avenue and 337/349 Grove Avenue, more particularly described as follows:

Orange County Property Tax ID #s: 12-22-29-4172-05-110, 12-22-29-4172-05-170, and 12-22-29-4172-05-160 as identified on the map attached hereto as Exhibit "2".

SECTION 3. Official Zoning Map Amendment. That Chapter 58 "Land Development Code", Article III, "Zoning" and the Official Zoning Map is hereby amended so as to change from no zoning designation to Commercial (C-3) zoning district for the vacated portion of Killarney Drive identified as Tract A and legally described on Exhibit "3" attached hereto.

SECTION 4. Official Zoning Map Amendment. That Chapter 58 "Land Development Code", Article III, "Zoning" and the Official Zoning Map is hereby amended so as to change from no zoning designation to Parks and Recreation (PR) zoning district for the vacated portion of Fairview Avenue identified as Tract B and legally described on Exhibit "3" attached hereto.

SECTION 5. Zoning Text Amendment. That Chapter 58, Land Development Code, Article III, Zoning Regulations, Section 58-76 "Commercial (C-3) District" and Section 58-81 "Parks and Recreation (PR) District" are hereby amended to read as follows (words that are <u>underlined</u> are additions; words that are ~~stricken~~ are deletions; and all other provisions of such sections not included shall remain unchanged):

Sec. 58-76. - Commercial (C-3) district.

<u>(j) *Specific property development standards.* In order to encourage appropriate redevelopment and in exchange for additional open space and traffic calming improvements, the standards, intensities and permitted uses as defined in the Comprehensive Plan Future Land Use Policy 1-J-15 shall apply to the property defined in the Comprehensive Plan Future Land Use Map FLUM-1-21a and control over conflicting provisions of this chapter subject to receipt of conditional use approval for a specific development project. Prior to issuance of building permits for any building(s) to be developed upon the property defined in the</u>

Comprehensive Plan Future Land Use Map FLUM-1-21a, a replat of such property meeting the requirements of Chapter 177, Florida Statutes and this chapter shall be submitted by the property owner and approved by the city and recorded in the Orange County public records.

Sec. 58-81. - Parks and recreation (PR) district.

(f) *Specific property development standards.* In order to encourage appropriate redevelopment and in exchange for additional open space and traffic calming improvements, the standards, intensities and permitted uses as defined in the Comprehensive Plan Future Land Use Policy 1-J-15 shall apply to the property defined in the Comprehensive Plan Future Land Use Map FLUM-1-21a and control over conflicting provisions of this chapter.

SECTION 6. Severability. If any Section or portion of a Section of this Ordinance proves to be invalid, unlawful, or unconstitutional, it shall not be held to invalidate or impair the validity, force, or effect of any other Section or part of this Ordinance.

SECTION 7. Conflicts. In the event of a conflict between this Ordinance and any other ordinance of the City of Winter Park or part thereof, this Ordinance shall control to the extent of the conflict.

SECTION 8. Effective Date. After its adoption, this Ordinance shall become effective simultaneously with the effective date of the ordinance adopting a comprehensive plan amendment to add Comprehensive Plan Future Land Use Policy 1-J-15 governing the properties subject to the rezonings set forth in Sections 1, 2, 3 and 4 of this Ordinance.

ADOPTED at a regular meeting of the City Commission of the City of Winter Park, Florida, held in City Hall, Winter Park, on this _____ day of _____, 2021.

_____
Mayor Steve Leary

Attest:

_____
City Clerk

Exhibit "1"

Lake
Killarney
BEACHVIEW AVE
S ORLANDO AVE
FAIRVIEW AVE
KILLARNEY DR
GROVE AVE
BROADVIEW AVE
EXHIBIT "1"
City of Winter Park
Florida

Exhibit “2”

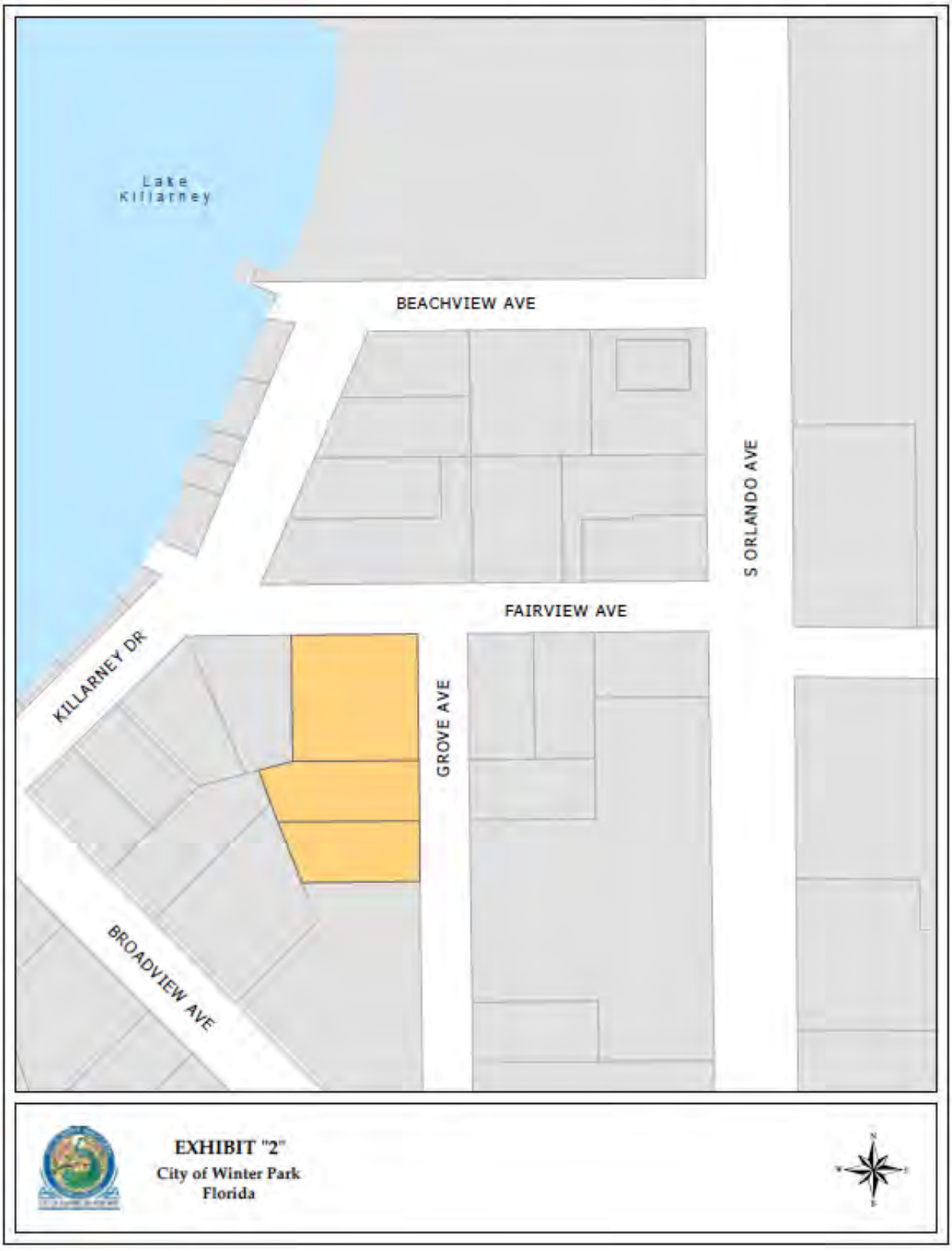
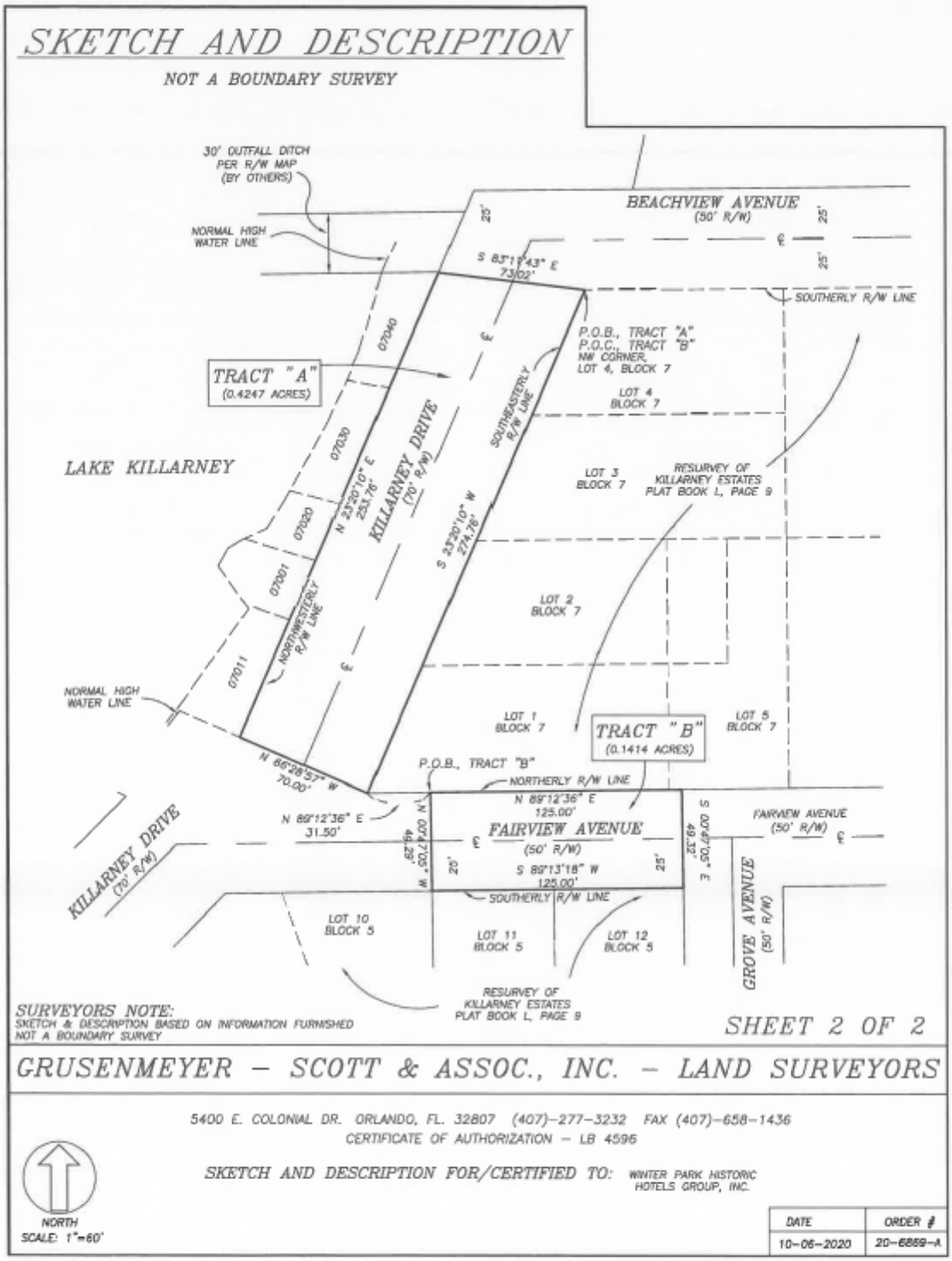

Lake
Killarney
BEACHVIEW AVE
S ORLANDO AVE
FAIRVIEW AVE
KILLARNEY DR
GROVE AVE
BROADVIEW AVE
EXHIBIT "2"
City of Winter Park
Florida

Exhibit "3"


SKETCH AND DESCRIPTION
NOT A BOUNDARY SURVEY
30' OUTFALL DITCH PER R/W MAP (BY OTHERS)
NORMAL HIGH WATER LINE
BEACHVIEW AVENUE (50' R/W)
S 83°11'43" E 73.02'
SOUTHERLY R/W LINE
P.O.B., TRACT "A"
P.O.C., TRACT "B"
NW CORNER, LOT 4, BLOCK 7
TRACT "A" (0.4247 ACRES)
LAKE KILLARNEY
KILLARNEY DRIVE (70' R/W)
SOUTHEASTERLY R/W LINE
LOT 4 BLOCK 7
LOT 3 BLOCK 7
RESURVEY OF KILLARNEY ESTATES PLAT BOOK L, PAGE 9
N 23°20'10" E 253.76'
S 23°20'10" W 274.76'
LOT 2 BLOCK 7
NORTHWESTERLY R/W LINE
NORMAL HIGH WATER LINE
LOT 1 BLOCK 7
TRACT "B" (0.1414 ACRES)
LOT 5 BLOCK 7
N 66°28'57" W 70.00'
P.O.B., TRACT "B"
NORTHERLY R/W LINE
N 89°12'36" E 125.00'
N 89°12'36" E 31.50'
FAIRVIEW AVENUE (50' R/W)
N 00°47'05" W 49.32'
S 89°13'18" W 125.00'
S 00°47'05" E 49.32'
FAIRVIEW AVENUE (50' R/W)
SOUTHERLY R/W LINE
KILLARNEY DRIVE (70' R/W)
LOT 10 BLOCK 5
LOT 11 BLOCK 5
LOT 12 BLOCK 5
GROVE AVENUE (50' R/W)
RESURVEY OF KILLARNEY ESTATES PLAT BOOK L, PAGE 9
SURVEYORS NOTE:
SKETCH & DESCRIPTION BASED ON INFORMATION FURNISHED
NOT A BOUNDARY SURVEY
SHEET 2 OF 2
GRUSENMEYER – SCOTT & ASSOC., INC. – LAND SURVEYORS
5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)-277-3232 FAX (407)-658-1436
CERTIFICATE OF AUTHORIZATION – LB 4596
SKETCH AND DESCRIPTION FOR/CERTIFIED TO: WINTER PARK HISTORIC HOTELS GROUP, INC.
NORTH
SCALE: 1"=60'
DATE 10-06-2020
ORDER # 20-6859-A

DRAFT

COMMUNITY BENEFIT AGREEMENT
(Winter Park Historic Hotels Group, LLC.)

This **COMMUNITY BENEFIT AGREEMENT** (the "Agreement") is made and entered into the _____ day of _____, 2020, between the **City of Winter Park, Florida**, a Florida municipality ("**City**"), whose address is 401 Park Avenue South, Winter Park, Florida 32789; **Winter Park Historic Hotels Group, LLC**, a Florida limited liability company ("**WPHH**"), whose address is 1069 W. Morse Blvd., Winter Park, Florida 32789.

WHEREAS, WPHH owns and/or is the contract purchaser of approximately 2.97 acres (inclusive of vacated Right-of-Way) generally bounded by Beachview Avenue to the north, Fairview Avenue to the south and Lake Killarney to the west, a legal description is set forth in **Exhibit "A"** attached hereto ("**Hotel and Park Property**");

WHEREAS, WPHH or its affiliate also owns approximately 0.18 acres at the intersection of S. Orlando Avenue and Fairview Avenue, a legal description is set forth in **Exhibit "B"** attached hereto ("**Phase II Retail**");

WHEREAS, WPHH desires to redevelop the Hotel and Park Property as a 132 room hotel with ancillary uses of restaurant, meeting space, ballroom, underground parking and a public park with parking spaces ("**Henderson Hotel**").

WHEREAS, the City desires to ensure the orderly redevelopment of the Hotel and Park Property in a manner compatible with the surrounding uses;

WHEREAS, WPHH has submitted a request to amend the City's Comprehensive Plan from Low Density Residential and Office to Commercial and Open Space/Recreational along with an associated text amendment; amend the Official Zoning Map from R-1A/O-1 to C-3 and receive a Final Conditional Use Permit for approvals to build the Henderson Hotel;

NOW, THEREFORE, in consideration of the premises thereof, the promises and provisions contained herein and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, the City and WPHH agrees as follows:

1. Incorporation of Recitals. The foregoing recitals shall be and are hereby incorporated into and made a part of this Agreement.

2. Approved Use. The following uses are approved pursuant to CU#20-0XX:

(a) 132 Hotel Rooms;

(b) 235 Parking Spaces, provided both in a sub-surface garage and in the first floor drop off area;

(c) Ancillary uses of Ballroom/Meeting Space up to 7,500 square feet and Restaurant up to 220 seats; and

(d) Approximately 0.88 acres of public use park as further described herein.

3. Development Standards:

(a) Maximum Floor Area Ratio: 0.99

(b) Maximum Impervious Surface Ratio: 59%

(c) Required Open Space: 44%

4. Stormwater. In addition to meeting all federal, state, City and St. Johns Water Management District requirements for retention and treatment of stormwater, WPHH agrees to the following:

(a) At the sole cost of WPHH, it will construct a baffle box in the stormwater utility line running under Beachview Avenue in order to treat the existing stormwater run-off from the City's streets into Lake Killarney.

(b) After the City's acceptance of the baffle box construction described above, WPHH will, at its sole cost and expense, remove the chain link fence in Lake Killarney which is currently located at the outfall of the stormwater utility line.

(c) WPHH will enter into a Maintenance Schedule Agreement with City prior to building permit to ensure the proper maintenance for the exfiltration system.

5. Restricted Use of Lake Killarney. The Henderson Hotel will be limited to one dock not to exceed 600 square feet in size and will not extend further than 25 feet on Lake Killarney. All existing docks shall be removed prior to Certificate of Occupancy. Restaurant and/or bar service prohibited on the dock. No motorized or non-motorized boat access from the hotel dock.

6. Lake Killarney Beautification. WPHH will remove invasive species along the shoreline in front of its property and will replant with species appropriate for littoral zones. In addition, WPHH as part of the approved Landscape Plan will save the existing four Cypress trees along the lakefront and will add an additional four Bald Cypress.

7. Public Use Park. WPHH will restrict approximately 0.88 acres on Grove Ave ("The Park") for a passive park with public access from sunrise to sunset. The Park shall include 30 parking spaces. WPHH agrees to offer a perpetual easement to the Winter Park Land Trust and/or City of Winter Park for use of the Park. WPHH will maintain The Park at its cost and expense. The Park will be treated as part of the Henderson Hotel for purposes of security.

8. Noise and Sound. Henderson Hotel exterior lighting will use zero lot line lighting technology and directional lighting. Henderson Hotel will use directional sound technology for all exterior speakers.

9. Electric Vehicle (EV) Parking. The parking garage shall include at least two (2) EV charging spaces. The parking garage shall have at least 20% of the parking spaces wired for EV charging spaces.

10. Phase 2 Retail. WPHH will cease the motel operation at 271 S. Orlando Avenue upon commencement of construction. WPHH will demolish or renovate the buildings at 271 S. Orlando Avenue upon receipt of Certificate of Occupancy of the Henderson Hotel.

11. Notices. Any notice delivered with respect to this Agreement shall be in writing and shall be deemed to be delivered (whether or not actually received) (i) when hand delivered to the person(s) hereinafter designated, or (ii) upon deposit of such notice in the United States Mail, postage prepaid, certified mail, return receipt requested, addressed to the person at the address set forth opposite the Party's name below, or at such other address or to such other person as the party shall have specified by written notice to the other Party delivered in accordance herewith

As to City:	City of Winter Park Attn: City Manager 401 Park Avenue South Winter Park, Florida 32789
And a Copy to:	Fishback Dominick Law Firm Attn: Kurt Ardaman 1947 Lee Road Winter Park, Florida 32789
As to Henderson Hotel:	Winter Park Historic Hotels Group, LLC. c/o Atrium Management Company Attn: Adam Wonus awonus@atriummanagement.com 1509 S Orange Avenue Orlando, FL 32806
And a Copy to:	Lowndes, Drosdick, Doster, Kantor & Reed, P.A. Attn: M. Rebecca Wilson 215 N. Eola Drive Orlando, Florida 32801

12. Amendment. Except as expressly provided herein, this Agreement contains the entire agreement between the parties and may not be changed, modified, amended, waived, or cancelled except by an amendment in writing and executed by each of the parties hereto.

13. Recording. This Agreement shall be recorded in the official records of Orange County, Florida, at the expense of WPHH.

14. Counterparts and Facsimile Signatures. This Agreement may be executed in one or more counterparts each of which shall be deemed an original but all of which together shall

constitute one and the same instrument. This Agreement may be executed by facsimile or .pdf electronic signature by any party and such signature will be deemed binding for all purposes hereof without delivery of an original signature being thereafter required.

15. Applicable Law and Jurisdiction. This Agreement shall be governed by and construed in accordance with the laws of the State of Florida. Each party agrees that any action or proceeding with respect to this Agreement may only be brought in a federal or state court situated in Orange County, Florida, and by execution and delivery of this Agreement, such party irrevocably consents to jurisdiction and venue in each such court.

16. Binding Nature. This Agreement inures to the benefit of and is binding upon the successors or assigns in interest or the legal representatives of the parties hereto.

17. Effective Date. This Agreement shall become effective upon the adoption of Ordinance _____ (Comp Plan Amendment); Ordinance _____ (Zoning Ordinance) and City Commission approval of the Preliminary Conditional Use Permit submitted on _____, and the expiration of all appeal periods.

(SIGNATURES ON FOLLOWING PAGES)

WITNESSES:

CITY OF WINTER PARK, FLORIDA, a Florida municipality

_____
Print Name: _____

By: _____
Name: _____
Title: _____

_____
Print Name: _____

STATE OF _____
COUNTY OF _____

The foregoing instrument was acknowledged before me by means of ☐ physical presence or ☐ online notarization, this _____ day of _____, 2020, by _____, as _____ of the City of Winter Park, a Florida municipality, on behalf of the municipality. He (She) ☐ is personally known to me or ☐ has produced _____ as identification.

(NOTARY SEAL)

_____
Notary Public Signature

_____
(Name typed, printed or stamped)

WITNESSES:

WINTER PARK HOTELS GROUP, LLC.
a Florida limited liability company

_____
Print Name: _____

By: _____
Name: _____
Title: _____

_____
Print Name: _____

STATE OF _____
COUNTY OF _____

The foregoing instrument was acknowledged before me by means of ☐ physical presence or ☐ online notarization, this _____ day of _____, 2020, by _____, as _____ of Winter Park Hotels Group, LLC, a limited liability company, on behalf of the company. He (She) ☐ is personally known to me or ☐ has produced _____ as identification.

(NOTARY SEAL)

_____
Notary Public Signature

_____
(Name typed, printed or stamped)

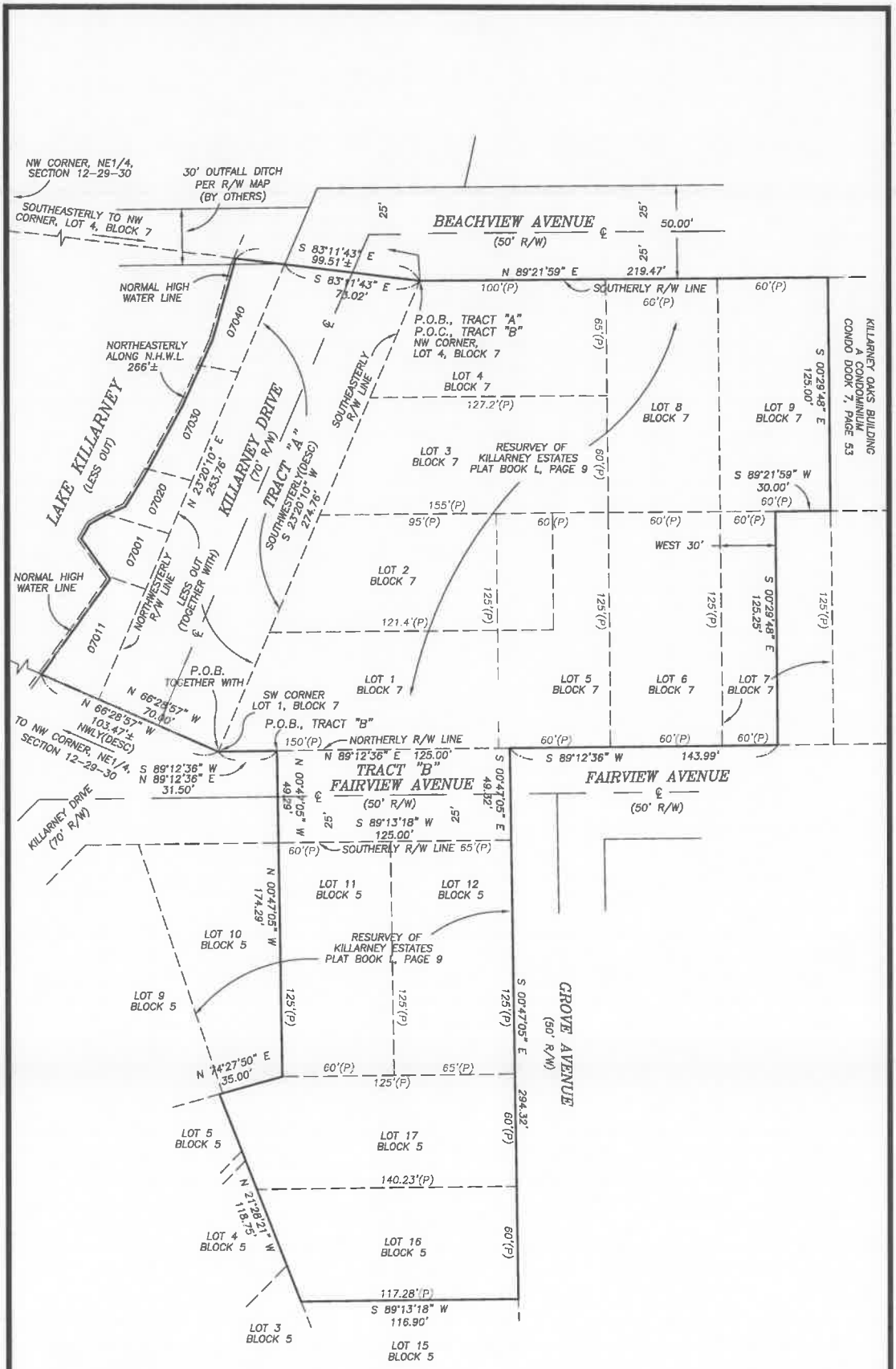
EXHIBIT “A”
Legal Description

EXHIBIT “B”
Legal Description

EXHIBIT “C”
New Tax Parcel

EXHIBIT "D"
_____ Properties

Building	**Address**



SHEET 2 OF 2

1"=60'

NORTH

Prepared by:
GRUSENMEYER-SCOTT & ASSOCIATES, INC. - LAND SURVEYORS
5400 EAST COLONIAL DRIVE - ORLANDO, FLORIDA 32807
Phone (407) 277-3232 - Fax (407) 658-1436

DATE	ORDER #
10-06-2020	20-6869-B

DESCRIPTION AS FOLLOWS:

Lots 11, 12, 16, 17, Block 5 and Lots 1, 2, 3, 4, 5, 6, 8, 9 and the West 30 feet of Lot 7, Block 7, RESURVEY OF KILLARNEY ESTATES, according to the plat thereof as recorded in Plat Book L, Page 9 of the Public Records of Orange County, Florida.

TOGETHER WITH

Begin at the Southwest corner of Lot 1, Block 7, RESURVEY OF KILLARNEY ESTATES, according to the plat thereof as recorded in Plat Book L, Page 9 of the Public Records of Orange County, Florida; thence run Northwesterly to the Northwest corner of the Northeast 1/4 of Section 12, Township 29 South, Range 30 East; thence run Southeasterly to the Northwesterly corner of Lot 4, said Block 7; thence Southwesterly to the Point of Beginning. LESS that part within the right-of-way and LESS that part that falls in Lake Killarney.

AND TOGETHER WITH

Part of Killarney Drive and Fairview Avenue, RESURVEY OF KILLARNEY ESTATES, as recorded in Plat Book L, Page 9, of the Public Records of Orange County, Florida, lying in Section 12, Township 22 South, Range 29 East, being Tracts "A" & "B" described as follows:

TRACT "A"
Begin at the Northwest corner of Lot 4, Block 7, said Resurvey of Killarney Estates; thence S.23°20'10"W., along the Southeasterly right-of-way line of said Killarney Drive, a distance of 274.76 feet to the Northerly right-of-way line of said Fairview Avenue; thence N.66°28'57"W., to the Northwesterly right-of-way line of said Killarney Drive; thence N.23°20'10"E., along said Northwesterly right-of-way line, a distance of 253.76 feet; thence S.83°11'43"E., a distance of 73.02 feet to the POINT OF BEGINNING.
Containing 18,498 square feet or 0.4247 acres, more or less.
AND
TRACT "B"
Commence at the Northwest corner of Lot 4, Block 7, said Resurvey of Killarney Estates; thence S.23°20'10"W., along the Southeasterly right-of-way line of said Killarney Drive, a distance of 274.76 feet to the Northerly right-of-way line of said Fairview Avenue; thence N.89°12'36"E., along said Northerly right-of-way line, 31.50 feet to the POINT OF BEGINNING; thence continue N.89°12'36"E., along said Northerly right-of-way line, a distance of 125.00 feet; thence S.00°47'05"E., a distance of 49.32 feet to the Southerly right-of-way line of said Fairview Avenue; thence S.89°13'18"W., a distance of 125.00 feet; thence N.00°47'05"W., a distance of 49.29 feet to the POINT OF BEGINNING. Containing 6,163 square feet or 0.1414 acres, more or less.

ALL THE ABOVE CONTAINS 2.9± ACRES

SKETCH & DERSCRIPTION FOR / CERTIFIED TO: *Winter Park Historic Hotels Group, Inc.*

SHEET 1 OF 2

GRUSENMEYER-SCOTT & ASSOC., INC. - LAND SURVEYORS

LEGEND

P	= PLAT	FH	= FIRE HYDRANT
F	= FIELD	P.O.L.	= POINT ON LINE
I.P.	= IRON PIPE	TYP.	= TYPICAL
I.R.	= IRON ROD	P.R.C.	= POINT OF REVERSE CURVATURE
C.M.	= CONCRETE MONUMENT	P.C.C.	= POINT OF COMPOUND CURVATURE
SET I.R.	= 1/2" I.R. w/BLB 4596	RAD.	= RADIAL
REC.	= RECOVERED	N.R.	= NON-RADIAL
P.O.B.	= POINT OF BEGINNING	W.P.	= WITNESS POINT
℄	= CENTERLINE	CALC.	= CALCULATED
N&D	= NAIL & DISK	P.R.M.	= PERMANENT REFERENCE MONUMENT
R/W	= RIGHT-OF-WAY	F.F.	= FINISHED FLOOR ELEVATION
ESMT.	= EASEMENT	B.S.L.	= BUILDING SETBACK LINE
DRAIN.	= DRAINAGE	B.M.	= BENCHMARK
UTIL.	= UTILITY	B.B.	= BASE BEARING
C.L.F.	= CHAIN LINK FENCE	U/B	= UTILITY BOX
W.F.C.	= WOOD FENCE	SIGN	= SIGN
C/B	= CONCRETE BLOCK	W.M.	= WATER METER
P.C.	= POINT OF CURVATURE	W.V.	= WATER VALVE
P.T.	= POINT OF TANGENCY	G.A.	= GUY ANCHOR
DESC.	= DESCRIPTION	O/H	= OVER HEAD LINE
R	= RADIUS	C/O	= CLEAN OUT
L	= ARC LENGTH	NO ID	= NO IDENTIFICATION
D	= DELTA		
C	= CHORD		
C.B.	= CHURD BEARING		
PP	= POWER POLE		
T.B.M.	= TEMPORARY BENCHMARK		
P.O.C.	= POINT OF COMMENCEMENT		
LP	= LIGHT POLE		
DI	= DRAINAGE INLET		
MH	= MANHOLE		
TR/TRAN	= TRANSFORMER		
BFP	= BACK FLOW PREVENTER		

5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)-277-3232 FAX (407)-658-1436
GRUSCOTT@GRUSCOTT.COM LB#4596

NOTES:
1. THE UNDERSIGNED DOES HEREBY CERTIFY THAT THIS SURVEY MEETS THE STANDARDS OF PRACTICE SET FORTH BY THE FLORIDA BOARD OF PROFESSIONAL SURVEYORS AND MAPPERS IN CHAPTER 5J-17 FLORIDA ADMINISTRATIVE CODE PURSUANT TO SECTION 472.027, FLORIDA STATUES
2. UNLESS EMBOSSED WITH SURVEYOR'S SIGNATURE AND ORIGINAL RAISED SEAL, THIS SURVEY MAP OR COPIES ARE NOT VALID.
3. THIS SURVEY WAS PREPARED FROM TITLE INFORMATION FURNISHED TO THE SURVEYOR. THERE MAY BE OTHER RESTRICTIONS OR EASEMENTS THAT AFFECT THIS PROPERTY.
4. NO UNDERGROUND IMPROVEMENTS HAVE BEEN LOCATED UNLESS OTHERWISE SHOWN.
5. THIS SURVEY IS PREPARED FOR THE SOLE BENEFIT OF THOSE CERTIFIED TO AND SHOULD NOT BE RELIED UPON BY ANY OTHER ENTITY.
6. DIMENSIONS SHOWN FOR THE LOCATION OF IMPROVEMENTS HEREON SHOULD NOT BE USED TO RECONSTRUCT BOUNDARY LINES.
7. BEARINGS, ARE BASED ASSUMED DATUM AND ON THE LINE SHOWN AS BASE BEARING (B.B.)
8. ELEVATIONS, IF SHOWN, ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988, UNLESS OTHERWISE NOTED.
9. CERTIFICATE OF AUTHORIZATION No. 4596.

CERTIFIED BY:

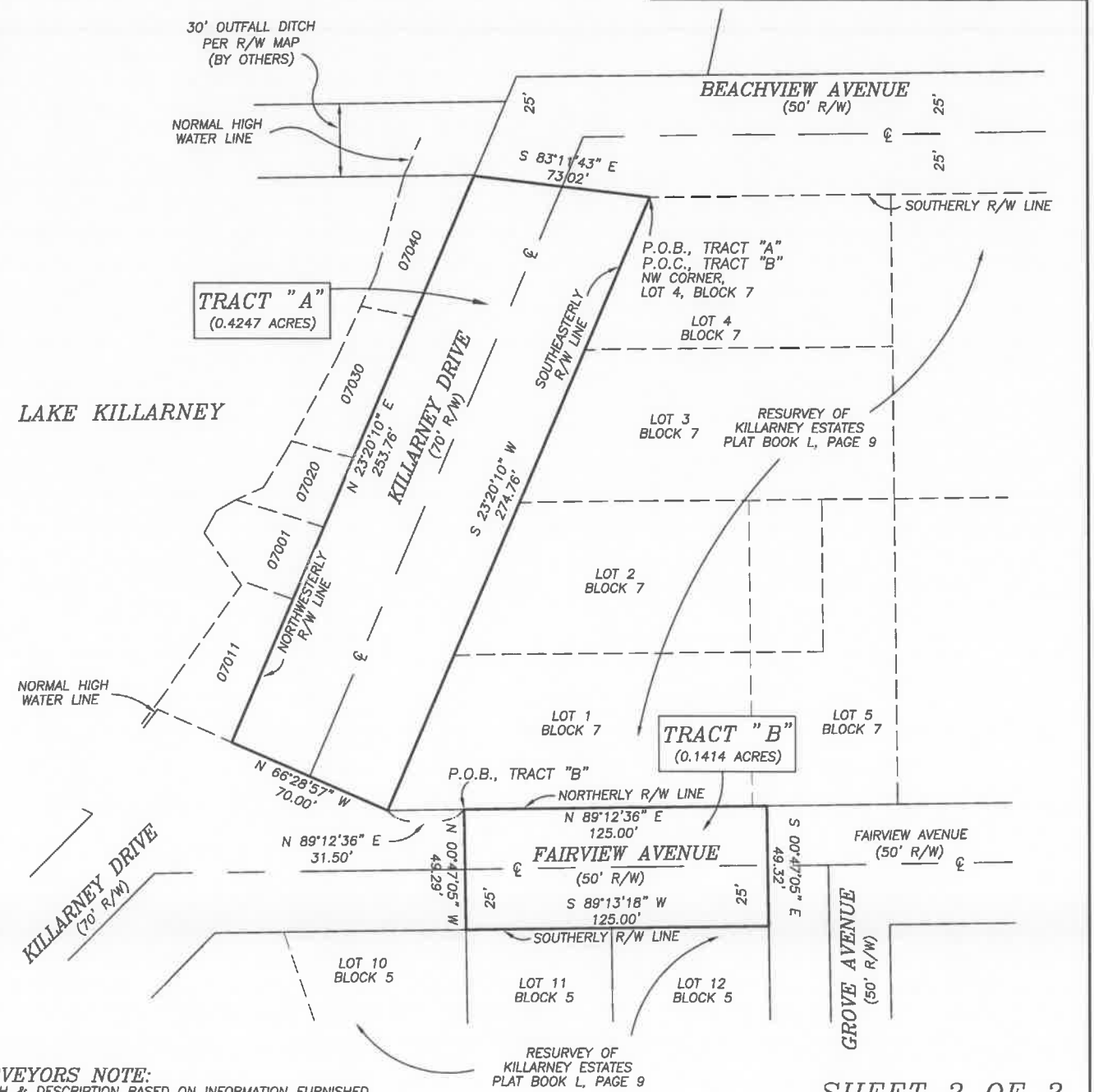
Digitally signed by Thomas X Grusenmeyer
Date: 2020.10.06 19:04:59 -04'00'
Adobe Acrobat Reader version: 2020.012.20048

TOM X. GRUSENMEYER, R.L.S. # 4714
JAMES W. SCOTT, R.L.S # 4801

SCALE 1" = 80'	DRAWN BY: ***
DATE 10-06-2020	ORDER No. 20-6869-B

SKETCH AND DESCRIPTION

NOT A BOUNDARY SURVEY



SURVEYORS NOTE:
SKETCH & DESCRIPTION BASED ON INFORMATION FURNISHED
NOT A BOUNDARY SURVEY

SHEET 2 OF 2

GRUSENMEYER – SCOTT & ASSOC., INC. – LAND SURVEYORS

5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)–277–3232 FAX (407)–658–1436
CERTIFICATE OF AUTHORIZATION – LB 4596

NORTH
SCALE: 1"=60'

SKETCH AND DESCRIPTION FOR/CERTIFIED TO: WINTER PARK HISTORIC HOTELS GROUP, INC.

DATE	ORDER #
10–06–2020	20–6869–A

SKETCH AND DESCRIPTION

NOT A BOUNDARY SURVEY

LEGAL DESCRIPTION

Part of Killarney Drive and Fairview Avenue, RESURVEY OF KILLARNEY ESTATES, as recorded in Plat Book L, Page 9, of the Public Records of Orange County, Florida, lying in Section 12, Township 22 South, Range 29 East, being Tracts "A" & "B" described as follows:

TRACT "A"
Begin at the Northwest corner of Lot 4, Block 7, said Resurvey of Killarney Estates; thence S.23°20'10"W., along the Southeasterly right-of-way line of said Killarney Drive, a distance of 274.76 feet to the Northerly right-of-way line of said Fairview Avenue; thence N.66°28'57"W., to the Northwesterly right-of-way line of said Killarney Drive; thence N.23°20'10"E., along said Northwesterly right-of-way line, a distance of 253.76 feet; thence S.83°11'43"E., a distance of 73.02 feet to the POINT OF BEGINNING.
Containing 18,498 square feet or 0.42 acres, more or less.
AND
TRACT "B"
Commence at the Northwest corner of Lot 4, Block 7, said Resurvey of Killarney Estates; thence S.23°20'10"W., along the Southeasterly right-of-way line of said Killarney Drive, a distance of 274.76 feet to the Northerly right-of-way line of said Fairview Avenue; thence N.89°12'36"E., along said Northerly right-of-way line, 31.50 feet to the POINT OF BEGINNING; thence continue N.89°12'36"E., along said Northerly right-of-way line, a distance of 125.00 feet; thence S.00°47'05"E., a distance of 49.32 feet to the Southerly right-of-way line of said Fairview Avenue; thence S.89°13'18"W., a distance of 125.00 feet; thence N.00°47'05"W., a distance of 49.29 feet to the POINT OF BEGINNING. Containing 6,163 square feet or 0.14 acres, more or less.

SURVEYORS NOTE:
SKETCH & DESCRIPTION BASED ON INFORMATION FURNISHED
NOT A BOUNDARY SURVEY

GRUSENMEYER – SCOTT & ASSOC., INC. – LAND SURVEYORS

5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)-277-3232 FAX (407)-658-1436
CERTIFICATE OF AUTHORIZATION – LB 4596

LEGEND –

P	=	PLAT
F	=	FIELD
I.P.	=	IRON PIPE
I.R.	=	IRON ROD
C.M.	=	CONCRETE MONUMENT
SET I.R.	=	1/2" I.R. w/#LB 4596
REC.	=	RECOVERED
P.O.B.	=	POINT OF BEGINNING
P.O.C.	=	POINT OF COMMENCEMENT
℄	=	CENTERLINE
N&D	=	NAIL & DISK
R/W	=	RIGHT-OF-WAY
ESMT.	=	EASEMENT
DRAIN.	=	DRAINAGE
UTIL.	=	UTILITY
CL.FC.	=	CHAIN LINK FENCE
WD.FC.	=	WOOD FENCE
C/B	=	CONCRETE BLOCK
P.C.	=	POINT OF CURVATURE
P.T.	=	POINT OF TANGENCY
DESC.	=	DESCRIPTION

LEGEND –

R	=	RADIUS
L	=	ARC LENGTH
D	=	DELTA
C	=	CHORD
C.B.	=	CHORD BEARING
P.O.L.	=	POINT ON LINE
TYP.	=	TYPICAL
P.R.C.	=	POINT OF REVERSE CURVATURE
P.C.C.	=	POINT OF COMPOUND CURVATURE
RAD.	=	RADIAL
N.R.	=	NON-RADIAL
W.P.	=	WITNESS POINT
CALC.	=	CALCULATED
P.R.M.	=	PERMANENT REFERENCE MONUMENT
F.F.	=	FINISHED FLOOR ELEVATION
B.S.L.	=	BUILDING SETBACK LINE
B.M.	=	BENCHMARK
B.B.	=	BASE BEARING
CONC.	=	CONCRETE
FIRM	=	FLOOD INSURANCE RATE MAP
I.D.	=	IDENTIFICATION

NOTES:

1. *THE UNDERSIGNED DOES HEREBY CERTIFY THAT THIS SKETCH MEETS THE MINIMUM TECHNICAL STANDARDS SET FORTH BY THE FLORIDA BOARD OF PROFESSIONAL LAND SURVEYORS IN CHAPTER 61G17-6 FLORIDA ADMINISTRATIVE CODE PURSUANT TO SECTION 472.027 FLORIDA STATUTES.*
2. *UNLESS EMBOSSED WITH SURVEYOR'S SEAL, THIS SKETCH IS NOT VALID AND IS PRESENTED FOR INFORMATIONAL PURPOSES ONLY.*
3. *THIS SKETCH WAS PREPARED FROM TITLE INFORMATION FURNISHED TO THE SURVEYOR. THERE MAY BE OTHER RESTRICTIONS OR EASEMENTS THAT AFFECT THIS PROPERTY.*
4. *NO UNDERGROUND IMPROVEMENTS HAVE BEEN LOCATED UNLESS OTHERWISE SHOWN.*
5. *THIS SKETCH IS PREPARED FOR THE SOLE BENEFIT OF THOSE CERTIFIED TO AND SHOULD NOT BE RELIED UPON BY ANY OTHER ENTITY.*
6. *DIMENSIONS SHOWN FOR THE LOCATION OF IMPROVEMENTS HEREON SHOULD NOT BE USED TO RECONSTRUCT BOUNDARY LINES.*
7. *BEARINGS, IF SHOWN, ARE BASED ASSUMED DATUM AND ON THE LINE SHOWN AS BASE BEARING (B.B.)*
8. *ELEVATIONS, IF SHOWN, ARE BASED ON NGVD 1929, UNLESS OTHERWISE NOTED.*

CERTIFIED BY: Digitally signed by Thomas X Grusenmeyer DN: c=US, o=Unaffiliated, ou=A01410C000001AC3548A0D100000B9C, cn=Thomas X Grusenmeyer Date: 2020.10.07 09:32:08 -04'00' Adobe Acrobat Reader version: 2020.012.20048

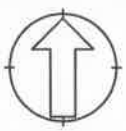
R.L.S. #

JAMES W. SCOTT, R.L.S # 4801 *TOM X. GRUSENMEYER, R.L.S. # 4714*

SKETCH AND DESCRIPTION FOR/CERTIFIED TO: WINTER PARK HISTORIC HOTELS GROUP, INC.

DATE	ORDER #
10-06-2020	20-6869-A

SCALE:
1" = 60'



NORTH

HENDERSON HOTEL REQUEST

Winter Park Historic Hotels Group is requesting:

- Vacating a portion of Killarney Drive and Fairview Avenue;
- Changes to the Comprehensive Plan Future Land Use plan map and Future Land Use policies;
- Changes to the Zoning Map and Zoning Code; and
- Conditional Use approval

to permit the construction of a 5-story, 132-room hotel, including a 235 space parking garage along with a public park and associate parking spaces on the 2.97 acres of property bounded by Lake Killarney Drive and Beachview Avenue, Fairview Avenue and Grove Avenue. There is also a Future Commercial parcel on S. Orlando Avenue which will remain Commerce/C-3.

	ALLOWED	**PROPOSED**
Land Use	Single Family/Office/Commerce	Commerce/ Open Space & Recreation
Zoning	R-1/O-2/C-3	C-3/PR
Height	Maximum 4 stories (55 feet)	Maximum 5 stories (55 feet*) *65' parapet & 73'Architectural Feature
Intensity	50% FAR	99% FAR

The approvals sought by the Applicant include:

1. Vacating a portion of Killarney Drive between Beachview and Fairview Avenues and a portion of Fairview Avenue between Grove Avenue and Killarney Drive.

2. Amending the Comprehensive Plan Future Land Use policy text to add a new policy for this site in order to:

   (a) Allow the Single Family and Office land use/zoning to change to Commerce (C-3) and Parks and Recreation.

(b) Allow a maximum Floor Area Ratio (FAR – building size) of 99% in lieu of the maximum 45% FAR.

(c) Allow a 5-story building (55 feet) in lieu of the current maximum 4-stories (55 feet).

3. Amending the Zoning Code C-3 text to provide for the allowances above for only this specific property.

4. Amending the Zoning Map from Single Family and Office on 2.09 acres of this site to Commercial/C-3 and on 0.88 acres to Recreation/PR.

5. Approval of a Conditional Use for the hotel project of a 5-story, 132-room hotel with associated restaurant, ballroom/meeting space, and 235 space parking garage and a park with associated surface parking.

Henderson Hotel FLU Policy

The subject property is an aggregation of approximately 10 properties which are generally bounded by Beachview Avenue on the north, Fairview Avenue on the south, U.S. Highway 17-92 (S. Orlando Avenue) on the east and Lake Killarney on the west. There are also 3 parcels on the west side of Grove Avenue.

The subject property has a mix of different uses Comprehensive Plan Future Land Use Designations and zoning categories including: Commercial/C-3; Office/O-1 and Single-Family Residential/R-1A. It is comprised of approximately 3 acres (Proposed Hotel/Park are approximately 2.97 acres and Future Commercial on S Orlando Ave. is 0.18 acres). The subject property's current use includes hotel, office, storage, parking and vacant single-family. It is located in "Planning Area J: U.S. Highway 17-92 Corridor." The City's stated policy is to improve the aesthetics of this corridor while maintaining the City's scale and character.

The Applicant proposes demolishing the existing uses and redeveloping the subject property as a traditional water-view hotel. The proposed design has been pulled off of S. Orlando Avenue and creates a park to buffer the neighborhood to the south. It will also be significantly setback from Lake Killarney in order to provide open space for the public enjoyment. The proposed development includes a publicly accessible walking/biking trail around the lake.

The proposed project will also address the single-family neighborhood concerns related to existing and potential cut-through traffic. The project will install permanent and attractive solutions to alleviate the cut-through traffic with strategies which may include, roundabouts, vacated right-of-way and other traffic calming measures.

The proposed project will work with its architects to create an aesthetically pleasing hotel which draws inspiration from Winter Park's Historic Lakefront Hotels such as The Seminole, The Virginia Inn and The Alabama.

The proposed project will also help the City achieve its goals related to the health of its lakes. The existing development on the subject property was built prior to City and Water Management District standards for stormwater treatment and attenuation. The proposed project will use, as needed, bio-swales and underground retention to treat water before it is discharged into the City's stormwater system. The project will also work with the City to improve the existing stormwater conveyance system at the end of Beachview Avenue in order to better treat the water and remove pollution and trash before it enters Lake Killarney. The project shall be required to remove invasive species along the Lake and plant appropriate littoral zone species.

The below subarea policy is suggested to be adopted in order to protect the neighborhood to the south and allow for the proposed project (along with a FLU change to Commercial and Open Space/Recreation, a rezoning to C-3 and PR and Conditional Use Permit):

Proposed New Policy1-J-15:

In order to encourage appropriate redevelopment and in exchange for additional open space and traffic calming improvements, the following standards shall apply to the redevelopment of this property as a hotel and park with ancillary uses such as parking, restaurant and meeting rooms.

- 44% Open Space which shall include walking/biking path;
- ISR 59%;
- FAR up to 0.99;
- Maximum height fifty-five feet (55') (with mansard/parapet roof up to 65')
- Up to four (4) architectural features seventy-three feet (73') in height;
- The building shall be seventy-five feet (75') setback from Lake Killarney at a height of only thirty-five feet (35'). Additional height must also meet the lakeside setback requirements in the City's Land Development Code;
- Public Use of Grove Avenue Park and associated parking spaces;
- The parking garage shall be concealed from view of single-family homes;
- Final traffic calming to be approved by City Commission at Conditional Use Permit; and
- Final architecture and massing to be approved by City Commission at Conditional Use Permit.

This Subarea Policy shall prevail over any inconsistent policies found in the City's Comprehensive Plan Policies. This proposed policy is in further of the City's Visions Statement to "Plan our growth through a collaborative process that protects our City's timeless scale and character." This proposed policy "recognize[s] unique areas of the city and provide[s] a collaborative planning process that ensures quality development while reflecting the context and heritage of the area." The project will "enhance walking, biking, and recreational activities through a connected and integrated network of open space." It will "foster sustainable public and private parks and open spaces using state of the art practices and techniques" and "increase the connection to nature by incentivizing public and private green space through the design and development process." The proposed project will also "invest in innovative infrastructure to ensure that our means to get around is safe and efficient, through prioritization of pedestrians, bicyclist, and transit users."

CURRENT TEST FIT

Planning & TRANSPORTATION
401 Park Avenue South • Winter Park, Florida 32789 • (407) 599-3399

KILLARNEY NEIGHBORHOOD ROADWAY MODIFICATIONS

CONCEPTUAL FINAL LAYOUT - OPTION #2
BROADVIEW AVENUE, DALLAS AVENUE & GROVE STREET

DATE 08/17/2020

1" = 100'

FIGURE 3

Lowndes

M. REBECCA WILSON

rebecca.wilson@lowndes-law.com
215 North Eola Drive, Orlando, Florida 32801-2028
T: 407-418-6250 | F: 407-843-4444
MAIN NUMBER: 407-843-4600

MERITAS® LAW FIRMS WORLDWIDE

October 6, 2020

SENT VIA EMAIL

Bronce Stephenson
Winter Park Planning Dept.
401 Park Avenue South
Winter Park, FL 32789

bstephenson@cityofwinterpark.org

Re: Request to Vacate

Dear Bronce:

As you know this firm represents the owners and contract purchaser of the properties generally located west of Orlando Avenue, north of Fairview Avenue, south of Beachview Avenue, and east of Lake Killarney. Adam is proposing to redevelop the property as a hotel and park. In consideration of the development, Kimley Horn conducted a traffic analysis of cut through traffic in the neighborhood (the "Study"). The Study has been included in our submittal package. The findings of such include the vacation of certain right-of-way in order to discourage cut through traffic. Accordingly, we are requesting the City vacate portions of Fairview Avenue and Killarney Drive as shown on the attached exhibit. We have included property owner consents along with Ownership and Encumbrance Reports for the properties impacted by the vacation of the rights of way.

Very truly yours,

M. Rebecca Wilson

MRW/nle

OCPA Web Map
Major Roads
Proposed Roa
Residential
Commercial/ Industrial/ Vacant Land
Parks
6
Lot Number
Florida Turnpike
Public Roads
Brick Road
Agriculture
Agricultural/ Cartilage
Lakes and Rivers
Parcel Number
Interstate 4
Gated Roads
Block Line
Commercial/ Institutional
Hydro
Building
3106
Parcel Address
Toll Road
Road Under Construction
Lot Line
Governmental/ Institutional/ Misc
Waste Land
E
Block Number
111.9
Parcel Dimensi
Courtesy Rick Singh, CFA, Orange County Property Appraiser
BEACHVIEW AVE
KILLARNEY DR
FAIRVIEW AVE
GROVE AVE
US HWY 17-92
SR 15/600
17
[4 Units]
W NEW
Created: 10/6/2020 14:41
This map is for reference only and is not a survey

Ownership and Encumbrance Report

First American Issuing Office: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Customer Reference Number: 230 Killarney Drive
***First American File Number:* 2037-4522403**

Prepared For: Nancy A. Rossman
3200 S. Hiawassee Road, Suite 205
Orlando, FL 32835

Legal Description:

Tract No. 1.
Lot 4, Block 7 of RESURVEY OF KILLARNEY ESTATES, according to the plat thereof recorded in Plat Book L, Page 9, being also known as Lot 4, Block 7 of KILLARNEY ESTATES, according to the plat thereof recorded in Plat Book K, Page 11, Public Records of Orange County, Florida.

Tract No. 2.
Begin at the Southwest corner of Lot 4, Block 7 of RESURVEY OF KILLARNEY ESTATES, according to plat thereof recorded in Plat Book L, Page 9, Public Records of Orange County, Florida, run thence Northwesterly to the Northwest corner of NW 1/4 of NE 1/4 of Section 12, Township 22 South, Range 29 East, thence Southeasterly to the Northwest corner of said Lot 4, thence Southwesterly along the front lot line of said Lot to point of beginning, less the Public Street known as Killarney Drive and less that portion which is covered by the waters of Lake Killarney.

1. ***Grantee(s) In Last Deed of Record:***

Lake Shore Motel, Inc., a Florida corporation

2. ***Encumbrances/Matters Affecting Title*** (Includes only mortgages, liens and claims of lien (if not specifically affecting other property only), judgments (certified only), federal tax liens, bankruptcy petitions, death certificates, court orders and decrees, divorce decrees, property settlement agreements, tax warrants, incompetency proceedings and probate proceedings which may affect the title to the property described above (attach an exhibit, if necessary)):

(X) Exhibit Attached () Exhibit Not Attached.

Type of Instrument	**O.R.Book**	**Page**

Copies of the Encumbrances/Matters Affecting Title (**X**) are () are not included with this Report.

Ad Valorem Real Estate Taxes for Tax Parcel Number 12-22-29-4172-07040 for tax year 2019
Gross Tax: $ 3725.90 (X) Paid ( ) Not Paid.
Unpaid Taxes for Prior Years (if none, state "none"): NONE
Map Code: Assessment:

Customer File Number: 230 Killarney Drive
FATIC File Number: 2037-4522403

Certificate

"This Report" is a search limited to the Official Records Books as defined in Sections 28.001(1) and 28.222, Florida Statutes, from **October 11, 1979** to **September 25, 2020** at 8:00 a.m.. The foregoing Report accurately reflects matters recorded and indexed in the Official Records Books of Orange County, Florida, affecting title to the property described therein.This report is not an opinion of title, title insurance policy, warranty of title, or any other assurance as to the status of title and shall not be used for the purpose of issuing title insurance. Pursuant to s. 627.7843, Florida Statutes, the maximum liability of the issuer of this property information report for errors or omissions in this property information report is limited to the amount paid for this property information report, and is further limited to the person(s) expressly identified in the property information report as the recipients of the property information report.

First American Title Insurance Company

as Vice President

Dated: September 29, 2020

Ownership and Encumbrance Report

First American Issuing Office: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Customer Reference Number: 240 Killarney Drive
First American File Number: **2037-4522406**

Prepared For: Nancy A. Rossman
3200 S. Hiawassee Road, Suite 205
Orlando, FL 32835

Legal Description:

Lot 3, Block 7, RESURVEY OF KILLARNEY ESTATES, according to the plat thereof recorded in Plat Book "L", Page 9, Public Records of Orange County, Florida, and

ALSO

Begin at the NW corner of said Lot 3, Block 7, RESURVEY OF KILLARNEY ESTATES and run Northwesterly to the NW corner of the NW¼ of NE¼ of Section 12, Township 22 South, Range 29 East, thence Southeasterly to the SW corner of said Lot 3, thence Northeasterly along the front lot line of said Lot 3 to the point of beginning, Less the Public Street Known as Killarney Drive, and Also Less that portion covered by the Waters of Lake Killarney.

1. ***Grantee(s) In Last Deed of Record:***

Lake Shore Motel, Inc., a Florida corporation

2. ***Encumbrances/Matters Affecting Title*** (Includes only mortgages, liens and claims of lien (if not specifically affecting other property only), judgments (certified only), federal tax liens, bankruptcy petitions, death certificates, court orders and decrees, divorce decrees, property settlement agreements, tax warrants, incompetency proceedings and probate proceedings which may affect the title to the property described above (attach an exhibit, if necessary)):

() Exhibit Attached (X) Exhibit Not Attached.

Type of Instrument	**O.R.Book**	**Page**
None		

Copies of the Encumbrances/Matters Affecting Title (**X**) are () are not included with this Report.

Ad Valorem Real Estate Taxes for Tax Parcel Number 12-22-29-4172-07030 for tax year 2019
Gross Tax: $ 4,283.37 (X) Paid ( ) Not Paid.
Unpaid Taxes for Prior Years (if none, state "none"): None
Map Code: Assessment:

Customer File Number: 240 Killarney Drive
FATIC File Number: 2037-4522406

Certificate

"This Report" is a search limited to the Official Records Books as defined in Sections 28.001(1) and 28.222, Florida Statutes, from **April 13, 1973** to **September 28, 2020** at 8:00 a.m.. The foregoing Report accurately reflects matters recorded and indexed in the Official Records Books of Orange County, Florida, affecting title to the property described therein.This report is not an opinion of title, title insurance policy, warranty of title, or any other assurance as to the status of title and shall not be used for the purpose of issuing title insurance. Pursuant to s. 627.7843, Florida Statutes, the maximum liability of the issuer of this property information report for errors or omissions in this property information report is limited to the amount paid for this property information report, and is further limited to the person(s) expressly identified in the property information report as the recipients of the property information report.

First American Title Insurance Company

as Vice President

Dated: September 29, 2020

AGENT AUTHORIZATION

Lake Shore Motel, Inc. as the owner of the real property described as follows: 12-22-29-4172-07-030 and 12-22-29-4172-07-040 on Killarney Drive, Winter Park, Florida does hereby authorize Adam Wonus to act as our agent, to execute any petitions or other documents necessary to affect the application approval of a Comprehensive Plan Amendment and Rezoning for the redevelopment of this property as hotel, parking garage and accessory use, and to appear on our behalf before any administrative or legislative body in the county considering this application and to act in all respects as our agent in matters pertaining to this application.

Lake Shore Motel, Inc.

Kimberly W. Woodham 10-1-20

Kimberly Woodham, President Date

STATE OF FLORIDA
COUNTY OF

The foregoing instrument was acknowledged before me by means of ✓ physical presence or ___ online notarization, this 1st of October, 2020 by Kimberly Woodham as President of Lake Shore Motel, Inc., a Florida corporation, on behalf of the corporation. He/She is personally known to me or who has produced _____ as identification.

CHRIS W. HAYES
Notary Public - State of Florida
Commission # GG 031248
My Comm. Expires Dec 15, 2020
Bonded through National Notary Assn.

Signature of Notary

Chris W. Hayes

Print Name or Stamp

Title or Rank

Serial Number (if any)

Lake Shore Motel, Inc.

1321 Fairview Ave.
Winter Park, Florida 32789

September 29, 2020

Dan Langley, Assistant City Attorney
City of Winter Park
401 South Park Avenue
Winter Park, FL 32789-4386

Dear Dan:

Lake Shore Motel, Inc. owns the property located at 230 Killarney Dr. and 240 Killarney Dr., Winter Park, Florida (the "Property"). Lake Shore Motel, Inc., consents to the application for the proposed Right-of-Way vacation and abandonment adjacent to the Property as shown on the attached sketch and description.

Thank you,

Lake Shore Motel, Inc.

By: Kimberly M Woodham
Its: President

Ownership and Encumbrance Report

First American Issuing Office: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Customer Reference Number: 250 Killarney Dr.
First American File Number: **2037-4990180**

Prepared For: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Legal Description:

Begin at a point on the Lot 2, Block 7, RESURVEY OF KILLARNEY ESTATES, 35 feet North of the Southwest corner of Lot 2, run thence Northerly along the West line of Lot 2, 35 feet more or less to the Northwest corner of said Lot 2, thence Northwesterly to the Northwest corner of the Northwest 1/4 of the Northeast 1/4 of 12-22-29, thence Southeasterly to the Point of Beginning; less road and Lake Killarney.

And

Lot 2 and the North 60 feet of the West 30 feet of Lot 5, Block 7, RESURVEY OF KILLARNEY ESTATES, according to the plat thereof as recorded in Plat Book L, Page 9, Public Records of Orange County, Florida.

1. ***Grantee(s) In Last Deed of Record:***

CentreCorp, Inc., a Florida corporation

2. ***Encumbrances/Matters Affecting Title*** (Includes only mortgages, liens and claims of lien (if not specifically affecting other property only), judgments (certified only), federal tax liens, bankruptcy petitions, death certificates, court orders and decrees, divorce decrees, property settlement agreements, tax warrants, incompetency proceedings and probate proceedings which may affect the title to the property described above (attach an exhibit, if necessary)):

() Exhibit Attached (X) Exhibit Not Attached.

Type of Instrument	**O.R.Book**	**Page**
NONE		

Copies of the Encumbrances/Matters Affecting Title (**X**) are () are not included with this Report.

Ad Valorem Real Estate Taxes for Tax Parcel Number 12-22-29-4172-07020 for tax year 2019
Gross Tax: $ 7,386.96 (X) Paid ( ) Not Paid.
Unpaid Taxes for Prior Years (if none, state "none"): None
Map Code: Assessment:

Customer File Number: 250 Killarney Dr.
FATIC File Number: 2037-4990180

Certificate

"This Report" is a search limited to the Official Records Books as defined in Sections 28.001(1) and 28.222, Florida Statutes, from **September 10, 2019** to **October 01, 2020** at 8:00 a.m.. The foregoing Report accurately reflects matters recorded and indexed in the Official Records Books of Orange County, Florida, affecting title to the property described therein.This report is not an opinion of title, title insurance policy, warranty of title, or any other assurance as to the status of title and shall not be used for the purpose of issuing title insurance. Pursuant to s. 627.7843, Florida Statutes, the maximum liability of the issuer of this property information report for errors or omissions in this property information report is limited to the amount paid for this property information report, and is further limited to the person(s) expressly identified in the property information report as the recipients of the property information report.

First American Title Insurance Company

as Vice President

Dated: October 05, 2020

AGENT AUTHORIZATION

CentreCorp, Inc. as the owner of the real property described as follows: 12-22-29-4172-07-020 on Killarney Drive, Winter Park, Florida does hereby authorize Adam Wonus to act as our agent, to execute any petitions or other documents necessary to affect the application approval of a Comprehensive Plan Amendment and Rezoning for the redevelopment of this property as hotel, parking garage and accessory use, and to appear on our behalf before any administrative or legislative body in the county considering this application and to act in all respects as our agent in matters pertaining to this application.

CentreCorp, Inc.

Nancy A. Rossman, President

October 1, 2020
Date

STATE OF FLORIDA
COUNTY OF ORANGE

The foregoing instrument was acknowledged before me by means of X physical presence or ___ online notarization, this 1 of October, 2020 by Nancy A. Rossman as President of CentreCorp, Inc., a Florida corporation, on behalf of the corporation. She is personally known to me or who has produced _____ as identification.

Signature of Notary

Michelle Phillips
Print Name or Stamp

MICHELLE PHILLIPS
MY COMMISSION # GG 922338
EXPIRES: October 13, 2023
Bonded Thru Notary Public Underwriters

Title or Rank

Serial Number (if any)

0215146\182347\8886016v1

Ownership and Encumbrance Report

First American Issuing Office: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Customer Reference Number: 1321 Fairview Ave.
***First American File Number:* 2037-4522417**

Prepared For: Nancy A. Rossman
3200 S. Hiawassee Road, Suite 205
Orlando, FL 32835

Legal Description:

Lot 1 and Lot 5 (less the N 60 ft. of the W 30 ft. of Lot 5) Block 7, RESURVEY OF KILLARNEY ESTATES, according to the plat thereof, recorded in Plat Book L, Page 9, Public Records of Orange County, Florida.

Begin at the NW corner of Lot 1, Block 7, RESURVEY OF KILLARNEY ESTATES, according to the Plat thereof, recorded in Plat Book L, Page 9, of the Public Records of Orange County, Florida; run thence Northwesterly to the NW corner of the NW 1/4 of the NE 1/4 of Section 12, Township 22, South, Range 29 East, thence Southeasterly to the SW corner of Lot 1, Block 7, of said subdivision, thence Northeasterly along the front line of said Lot 1 to the Point of Beginning. Less the public street known as Killarney Drive and Less that portion which is covered by the waters of Lake Killarney.

1. ***Grantee(s) In Last Deed of Record:***

Kimberly Woodham, as surviving Trustee of the David Henderson Trust, dated December 29, 2010

2. ***Encumbrances/Matters Affecting Title*** (Includes only mortgages, liens and claims of lien (if not specifically affecting other property only), judgments (certified only), federal tax liens, bankruptcy petitions, death certificates, court orders and decrees, divorce decrees, property settlement agreements, tax warrants, incompetency proceedings and probate proceedings which may affect the title to the property described above (attach an exhibit, if necessary)):

() Exhibit Attached (X) Exhibit Not Attached.

Type of Instrument	**O.R.Book**	**Page**
Last Will and Testament	5982	3565
Order of Summary Ancillary Administration	5982	3574
Order of Summary Ancillary Administration	5983	4324
Affidavit of No Florida Estate Tax Due	10455	7759
Florida Certificate of Death	10455	7760
Order Admitting Will to Probate	10467	8208
Letters of Administration	10467	8209
Last Will of Testament	10467	8256

Type of Instrument	**O.R.Book**	**Page**
Last Will and Testament	3826	4899

Letters of Adminstration	3826	4904
Order Of Discharge	3826	3561
Plat Map	L	9

1. Riparian and/or littoral rights are not insured.

2. This Policy does not insure title to any part of the land lying below the ordinary high water mark of any abutting body of water.

3. The right, title or interest, if any, of the public to use as a public beach or recreation area any part of the Land lying between the water abutting the Land and the most inland of any of the following: (a) the natural line of vegetation; (b) the most extreme high water mark; (c) the bulkhead line, or (d) any other line which has been or which hereafter may be legally established as relating to such public use.

Copies of the Encumbrances/Matters Affecting Title (**X**) are () are not included with this Report.

NOTE: The following is for informational purposes only and is given without assurance or guarantee:

Real Estate Taxes for Tax Parcel Number 12-22-29-4172-07011 for tax year 2019
Gross Tax: $ 5,923.07 (X) Paid ( ) Not Paid.
Unpaid Taxes for Prior Years: None
Map Code: Assessment: $

Customer Reference Number:
First American File Number: **2037-4522417**

Certificate

"This Report" is a search limited to the Official Records Books as defined in Sections 28.001(1) and 28.222, Florida Statutes, from April 15, 1970 to September 25, 2020 at 8:00 a.m.. The foregoing Report accurately reflects matters recorded and indexed in the Official Records Books of Orange County, Florida, affecting title to the property described therein. This report is not an opinion of title, title insurance policy, warranty of title, or any other assurance as to the status of title and shall not be used for the purpose of issuing title insurance. Pursuant to s. 627.7843, Florida Statutes, the maximum liability of the issuer of this property information report for errors or omissions in this property information report is limited to the amount paid for this property information report, and is further limited to the person(s) expressly identified in the property information report as the recipients of the property information report.

First American Title Insurance Company

Michael Abbey, as SVP, Division Region Manager

Dated:09/29/2020

AGENT AUTHORIZATION

David Henderson Trust as the owner of the real property described as follows: 12-22-29-4172-07-011 on Fairview Avenue, Winter Park, Florida does hereby authorize Adam Wonus to act as our agent, to execute any petitions or other documents necessary to affect the application approval of a Comprehensive Plan Amendment and Rezoning for the redevelopment of this property as hotel, parking garage and accessory use, and to appear on our behalf before any administrative or legislative body in the county considering this application and to act in all respects as our agent in matters pertaining to this application.

David Henderson Trust

Kimberly Woodham, Trustee

10-1-20
Date

STATE OF Florida
COUNTY OF Orange

The foregoing instrument was acknowledged before me by means of ✓ physical presence or ___ online notarization, this 1st of October, 2020 by Kimberly Woodham, who is personally known to me or who has produced _____ as identification.

Signature of Notary

CHRIS W. HAYES
Notary Public - State of Florida
Commission # GG 031248
My Comm. Expires Dec 15, 2020
Bonded through National Notary Assn.

Print Name or Stamp

Title or Rank

Serial Number (if any)

David Henderson Trust

1321 Fairview Ave.
Winter Park, Florida 32789

September 29, 2020

Dan Langley, Assistant City Attorney
City of Winter Park
401 South Park Avenue
Winter Park, FL 32789-4386

Dear Dan:

David Henderson Trust owns the property located at 1321 Fairview Ave., Winter Park, Florida (the "Property"). David Henderson Trust, consents to the application for the proposed Right-of-Way vacation and abandonment adjacent to the Property as shown on the attached sketch and description.

Thank you,

By: Kimberly M. Woodham
Its: Trustee

Ownership and Encumbrance Report

First American Issuing Office: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Customer Reference Number: 1310 Fairview Ave.
First American File Number: **2037-4990199**

Prepared For: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Legal Description:

Lot 11 and 12, Block 5 of RESURVEY KILLARNEY ESTATES, according to the Plat thereof as recorded in Plat Book L, Page(s) 9, of the Public Records of Orange County, Florida.

1. ***Grantee(s) In Last Deed of Record:***

Transtek International Co., Inc., a Florida corporation

2. ***Encumbrances/Matters Affecting Title*** (Includes only mortgages, liens and claims of lien (if not specifically affecting other property only), judgments (certified only), federal tax liens, bankruptcy petitions, death certificates, court orders and decrees, divorce decrees, property settlement agreements, tax warrants, incompetency proceedings and probate proceedings which may affect the title to the property described above (attach an exhibit, if necessary)):

(X) Exhibit Attached () Exhibit Not Attached.

Type of Instrument	**O.R.Book**	**Page**
None		

Copies of the Encumbrances/Matters Affecting Title (**X**) are () are not included with this Report.

NOTE: The following is for informational purposes only and is given without assurance or guarantee:

Real Estate Taxes for Tax Parcel Number 12-22-29-4172-05110 for tax year 2019
Gross Tax: $ 5,163.27 (X) Paid ( ) Not Paid.
Unpaid Taxes for Prior Years: None
Map Code: Assessment: $

Customer Reference Number:
First American File Number: **2037-4990199**

Certificate

"This Report" is a search limited to the Official Records Books as defined in Sections 28.001(1) and 28.222, Florida Statutes, from May 01, 1972 to September 30, 2020 at 8:00 a.m.. The foregoing Report accurately reflects matters recorded and indexed in the Official Records Books of Orange County, Florida, affecting title to the property described therein. This report is not an opinion of title, title insurance policy, warranty of title, or any other assurance as to the status of title and shall not be used for the purpose of issuing title insurance. Pursuant to s. 627.7843, Florida Statutes, the maximum liability of the issuer of this property information report for errors or omissions in this property information report is limited to the amount paid for this property information report, and is further limited to the person(s) expressly identified in the property information report as the recipients of the property information report.

First American Title Insurance Company

Michael Abbey, as SVP, Division Region Manager

Dated:10/01/2020

Ownership and Encumbrance Report

First American Issuing Office: First American Title Insurance Company
2301 Maitland Center Parkway, Suite 450
Maitland, FL 32751

Customer Reference Number: Killarney Drive
***First American File Number:* 2037-4522423**

Prepared For: Nancy A. Rossman
3200 S. Hiawassee Road, Suite 205
Orlando, FL 32835

Legal Description:

A strip of land lying West of Lot 2 between the street and Lake Killarney, in Block 7 of KILLARNEY ESTATE RESURVEY, as per Plat thereof recorded in Plat Book "L", Page 9, Public Records of Orange County, Florida; less the Public Street known as Killarney Drive and less that portion which is covered by the water of Lake Killarney.

Less any portion as described in Warranty Deed recorded in Official Records Book 1935, Page 768, of the Public Records of Orange County, Florida, as more particularly described as follows:

Begin at the Northwest corner of Lot 1, Block 7, KILLARNEY ESTATES RESURVEY, according to the Plat thereof, recorded in Plat Book "L", Page 9, of the Public Records of Orange County, Florida; run thence Northwesterly to the NW corner of the NW ¼ of the NE ¼ of Section 12, Township 22, South, Range 29 East, thence Southeasterly to the SW corner of Lot 1, Block 7, of said subdivision, thence Northeasterly along the front line of said Lot 1 to the Point of Beginning. Less the Public Street known as Killarney Drive and Less that portion which is covered by the waters of Lake Killarney.

Less property described in Deeds recorded in Official Records Book 2926, Page 1756, and in Book 2926, Page 1755, of the Public Records of Orange County, Florida, as more particularly described as follows:

Lot 2 and the North 60 feet of the West 30 feet of Lot 5, Block 7 of KILLARNEY ESTATE RESURVEY, according to the Plat thereof as recorded in Plat Book "L", Page 9, Public Records of Orange County, Florida; and

Begin at a point on the Lot 2, Block 7, KILLARNEY ESTATES RESURVEY, 35 feet North of the SW corner of Lot 2, run thence Northerly along the West line of Lot 2, 35 feet more or less to the NW corner of said Lot 2, thence Northwesterly to the NW corner of the NW ¼ of the NE ¼ of Section 12, Township 22 South, Range 29 East, thence Southeasterly to the Point of Beginning; less road and Lake Killarney.

NOTE: This is a proposed legal description.

1. ***Grantee(s) In Last Deed of Record:***

Transtek International Co., Inc., a Florida corporation

2. ***Encumbrances/Matters Affecting Title*** (Includes only mortgages, liens and claims of lien (if not specifically affecting other property only), judgments (certified only), federal tax liens, bankruptcy petitions, death certificates, court orders and decrees, divorce decrees, property settlement agreements, tax warrants, incompetency proceedings and probate proceedings which may affect the title to the property described above (attach an exhibit, if necessary)):

() Exhibit Attached (X) Exhibit Not Attached.

Type of Instrument	**O.R.Book**	**Page**
NONE		

Copies of the Encumbrances/Matters Affecting Title (**X**) are () are not included with this Report.

NOTE: The following is for informational purposes only and is given without assurance or guarantee:

Real Estate Taxes for Tax Parcel Number 12-22-29-4172-07001 for tax year 2019
Gross Tax: $ 320.53 (X) Paid ( ) Not Paid.
Unpaid Taxes for Prior Years:
Map Code: Assessment: $

Customer Reference Number:
First American File Number: **2037-4522423**

Certificate

"This Report" is a search limited to the Official Records Books as defined in Sections 28.001(1) and 28.222, Florida Statutes, from September 11, 1972 to September 25, 2020 at 8:00 a.m.. The foregoing Report accurately reflects matters recorded and indexed in the Official Records Books of Orange County, Florida, affecting title to the property described therein. This report is not an opinion of title, title insurance policy, warranty of title, or any other assurance as to the status of title and shall not be used for the purpose of issuing title insurance. Pursuant to s. 627.7843, Florida Statutes, the maximum liability of the issuer of this property information report for errors or omissions in this property information report is limited to the amount paid for this property information report, and is further limited to the person(s) expressly identified in the property information report as the recipients of the property information report.

First American Title Insurance Company

Michael Abbey, as SVP, Division Region Manager

Dated:09/29/2020

AGENT AUTHORIZATION

The undersigned as President of Transtek International Co., Inc. the owner of the real property described as follows:

12-22-29-4172-07-001 (Killarney Dr., Winter Park, Florida)

12-22-29-4172-05-160 (Grove Ave., Winter Park, Florida)

12-22-29-4172-05-170 (Grove Ave., Winter Park, Florida)

12-22-29-4172-05-110 (Fairview Ave., Winter Park, Florida) states that the above properties are under Contract with Winter Park Historic Hotels Group, LLC, Adam Wonus, Manager. As part of the consideration of the Contract, Transtek International Co., Inc. does hereby authorize Adam Wonus to act as our agent, to execute any petitions or other documents necessary to affect the application approval of a Comprehensive Plan Amendment and Rezoning for the redevelopment of this property as hotel, parking garage and accessory use, and to appear on our behalf before any administrative or legislative body in the county considering this application and to act in all respects as our agent in matters pertaining to this application. Should the approvals sought by Winter Park Historic Hotels Group, LLC not be obtained and should Winter Park Historic Hotels Group, LLC not ultimately close on the Property, then Transtek International Co, Inc. reserves the right to withdraw this authorization.

Transtek International Co., Inc.

Joseph J. Linartas, President

10-2-2020
Date

STATE OF FLORIDA
COUNTY OF Seminole

The foregoing instrument was acknowledged before me by means of ___ physical presence or ___ online notarization, this 2 of October, 2020 by Joseph J. Linartas as President of Transtek International Co., Inc., a Florida corporation, on behalf of the corporation. He is personally known to me or who has produced FL DL as identification.

Signature of Notary

LaToya Smith
Print Name or Stamp

PB
Title or Rank

LATOYA SMITH
Notary Public - State of Florida
Commission # HH 035078
My Comm. Expires Aug 23, 2024

Book1012/Page9 CFN#1925P00L009 Page 1 of 1

9

RESURVEY OF
KILLARNEY ESTATES
A SUBDIVISION OF

THE NW¼ OF THE NE¼, SECTION 12, Tp. 22, S. R. 29, E. AND ALSO BEGINNING AT THE N.E. CORNER OF THE W½ OF N.E¼ OF SAID SECTION, RUNNING N. 72 FT, THENCE WEST 390 FT TO LAKE KILLARNEY, THENCE SOUTHERLY ALONG MARGIN OF THE LAKE TO THE NORTH LINE OF THE W½ OF N.E¼ OF SAID SECTION 12, THENCE EAST ALONG SAID NORTH LINE OF SECTION 12 TO POINT OF BEGINNING. RESERVING FOR CONSTRUCTION, MAINTENANCE AND REPAIR OF SEVILLE COMPANY LINES, AN EASEMENT IN THE REAR OF ALL LOTS FOR THE DEPTH OF SAID LOTS. ORIGINAL PLAT RECORDED IN ORANGE COUNTY, FLA. IN PLAT BOOK "K" PAGE 1

OWNERS CERTIFICATE

STATE OF FLORIDA
COUNTY OF ORANGE

PERSONALLY APPEARED BEFORE ME C. W. CUMMING WHO UPON OATH DEPOSES AND SAYS THAT HE IS THE RESIDENT OF THE ASSOCIATED DEVELOPERS INC., OWNERS OF THE LAND DESCRIBED IN THE CAPTION AND HAS AUTHORIZED AND ACCEPTED THE SURVEY OF THE SAME.

By: C. W. Cumming

SUBSCRIBED AND SWORN TO THIS THE 20th DAY OF JUNE 25

By Ada Keeler NOTARY PUBLIC

MY COMMISSION EXPIRES DEC. 9TH 1927.

(N.S.)

ENGINEERS CERTIFICATE

STATE OF FLORIDA
COUNTY OF ORANGE

WE THE FLORIDA ASSOCIATED ENGINEERS HEREBY CERTIFY THAT THE PLAT SHOWN HEREON IS A TRUE REPRESENTATION OF OUR SURVEY.

FLORIDA ASSOCIATED ENGINEERS

By Chas. A. Brown

REGISTERED ENGINEER 1906-25 STATE OF FLORIDA

SUBSCRIBED AND SWORN TO THIS THE 20TH DAY JUNE 25.

By Ada Keeler NOTARY PUBLIC

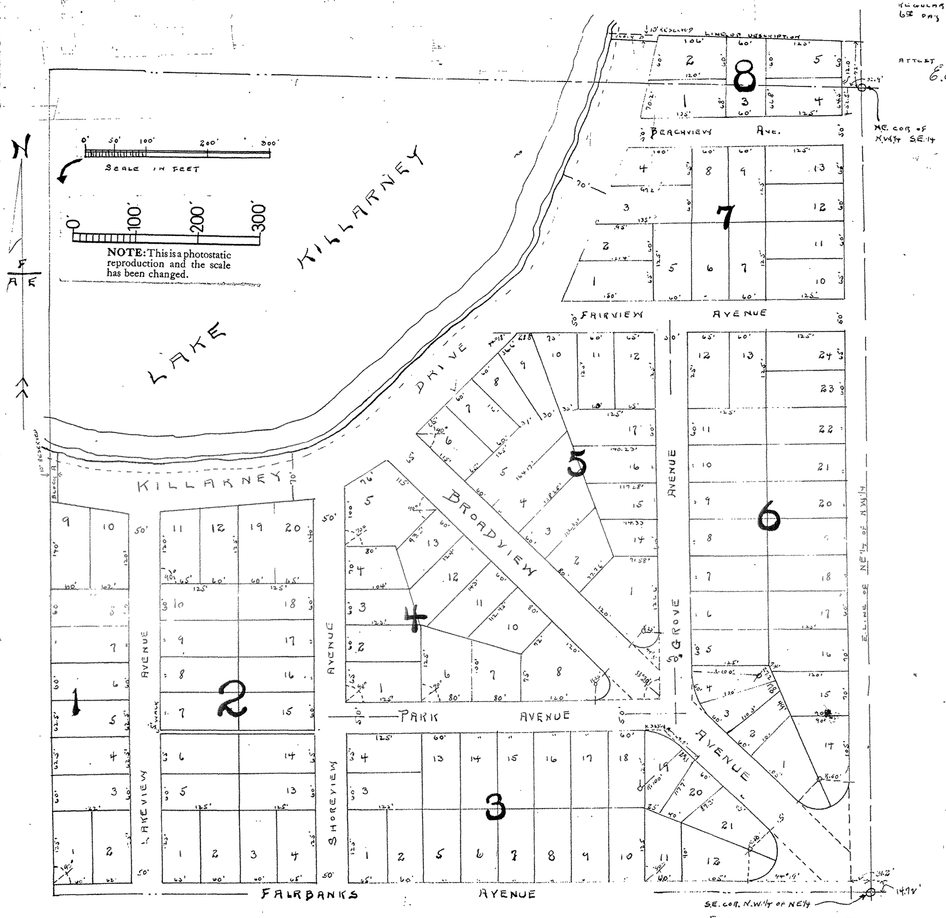
MY COMMISSION EXPIRES DEC. 9TH 1927.

(CHAS. A. BROWN SEAL) (N.S.)

THE PLAT HEREON EXAMINED, ALLOWED AND APPROVED BY THE TOWN COUNCIL OF THE TOWN OF WINTER PARK, ORANGE COUNTY IN REGULAR SESSION ON THIS THE 6TH DAY OF July 1925

H. C. Bannon
PRESIDENT OF COUNCIL

ATTEST E. R. Willcox
CITY CLERK



FILED AND RECORDED JULY 10 1925 AT 2.05 P.M.
B. M. Robinson CLERK
By M. A. Howard D.C.

L 19

Address	*Owner*	*Acres*	*Existing Zoning*
271 S Orlando Ave.	3014 Kilgore LLC	0.18	C-3
1298 Beachview Dr.	Lake Shore Motel Inc.	0.344	O-1
230 Killarney Dr.	Lake Shore Motel Inc.	0.22	R1-A
240 Killarney Dr.	Lake Shore Motel Inc.	0.24	R1-A
250 Killarney Dr.	Centrecorp Inc.	0.21	R1-A
1321 Fairview Ave.	David Henderson Trust	0.393	R1-A
1201 Fairview Ave.	Holm Investments WP LLC	0.26	O-1
1310 Fairview Ave.	Transtek International Co. Inc.	0.358	R1-A
337 Grove Ave.	Transtek International Co. Inc.	0.204	R1-A
349 Grove Ave.	Transtek International Co. Inc.	0.177	R1-A
Killarney Drive	Transtek International Co. Inc.	0.03	R1-A

Existing FLU	*Proposed zoning*	*Proposed FLU*
Commerce	No Change	No Change
Office Professional	C-3	Commerce
Single Family	C-3	Commerce
Single Family	C-3	Commerce
Single Family	C-3	Commerce
Single Family	C-3	Commerce
Office Professional	C-3	Commerce
Single Family	PR	Open Space Recreation
Single Family	PR	Open Space Recreation
Single Family	PR	Open Space Recreation
Single Family	C-3	Commerce

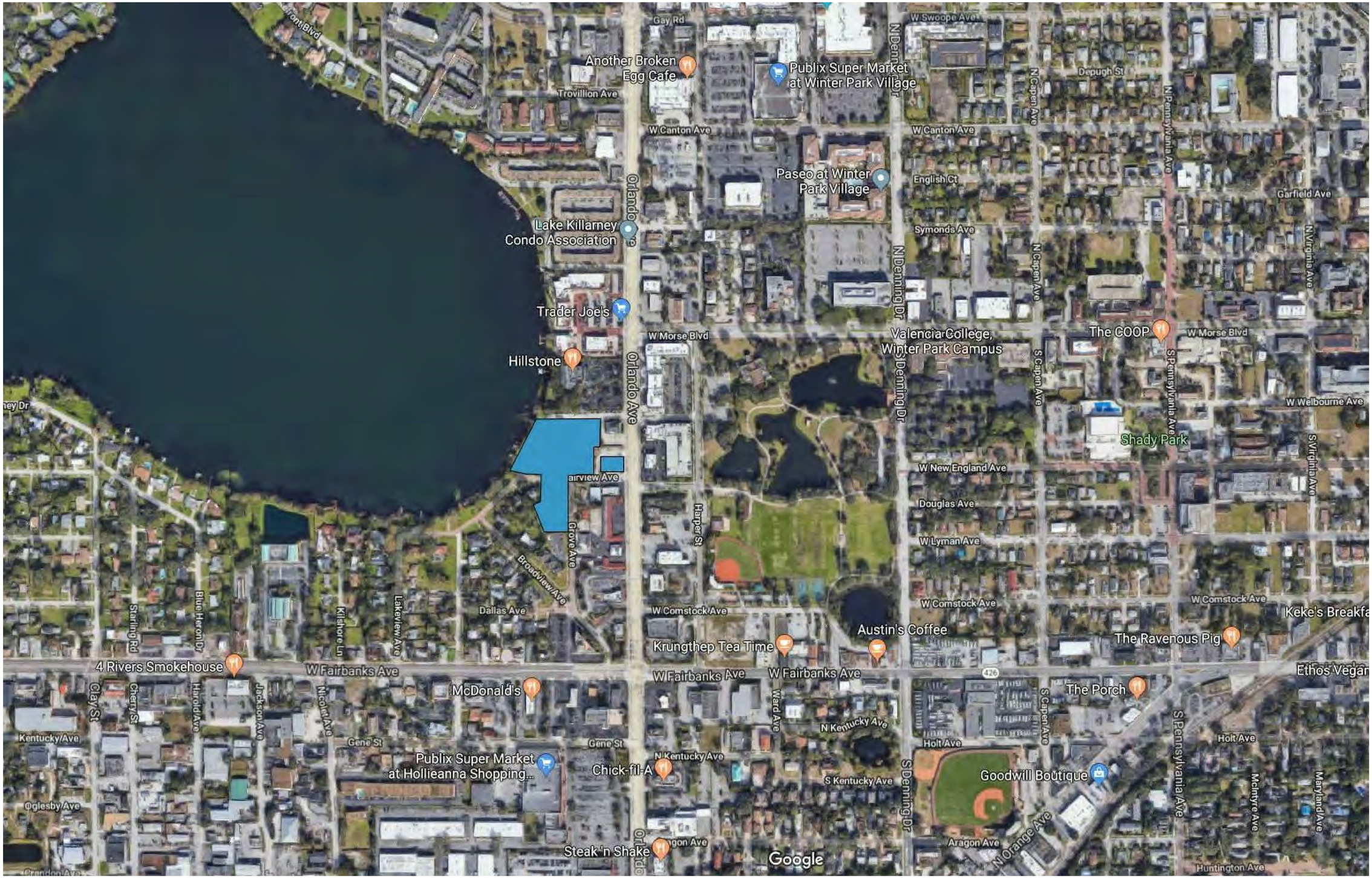
HENDERSON HOTEL

WINTER PARK, FLORIDA

CONDITIONAL USE PACKAGE
11/06/2020

COVER SHEET

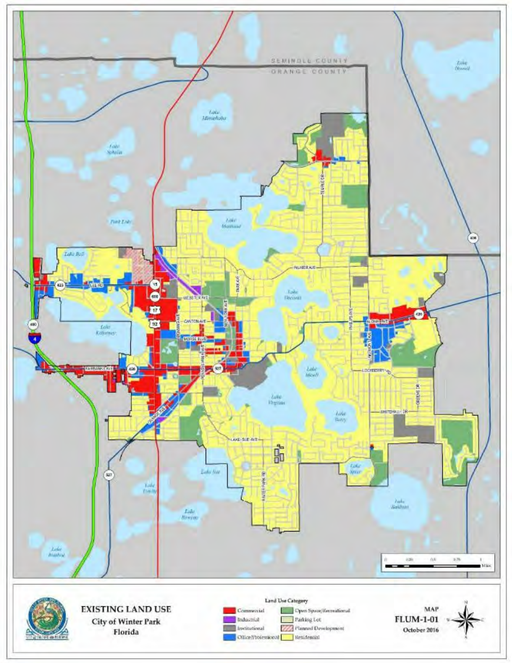
NOT FOR CONSTRUCTION



PROJECT SITE

N

NOT FOR CONSTRUCTION

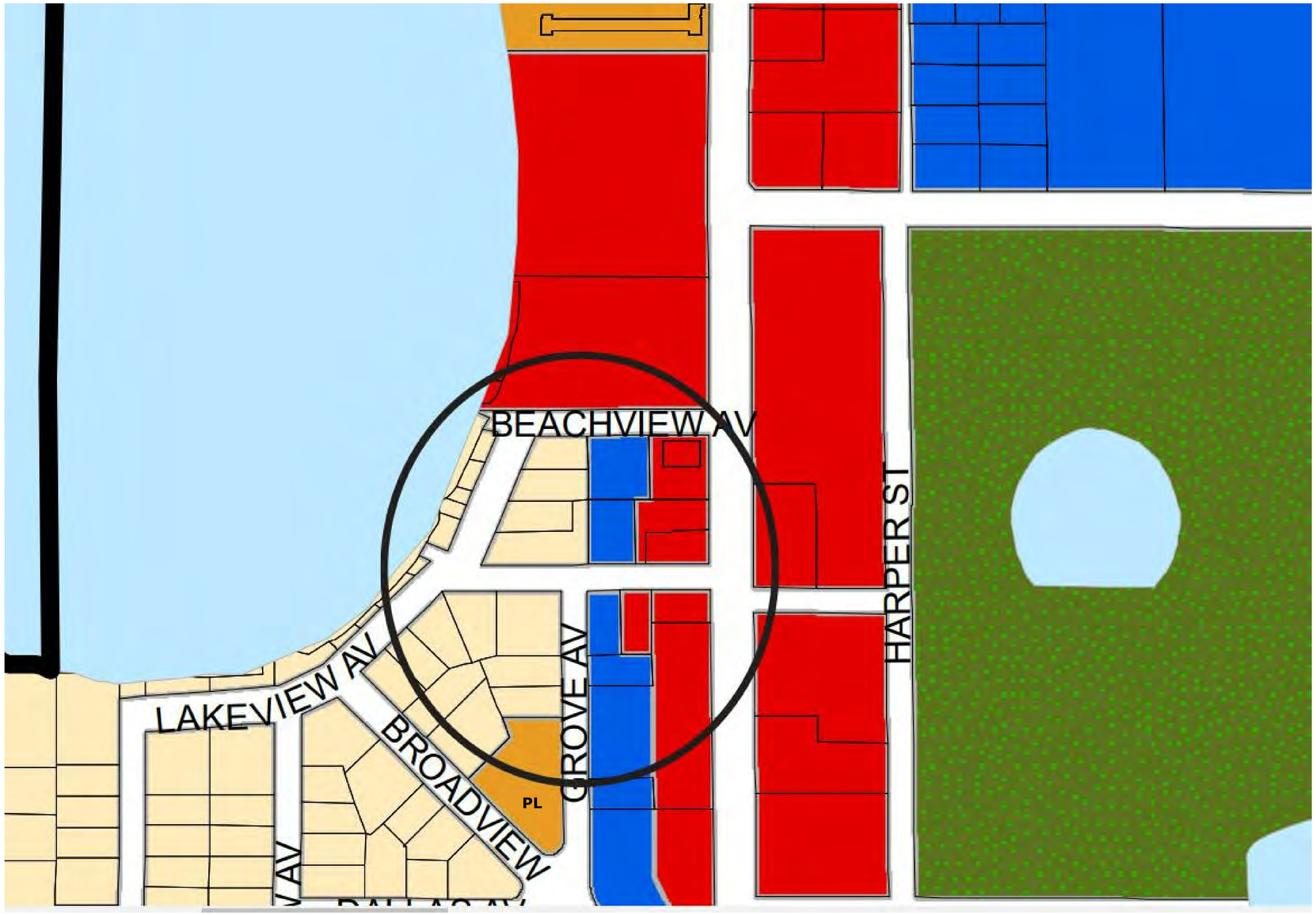


EXISTING LAND USE
City of Winter Park
Florida

Land Use Category
- Commercial
- Industrial
- Institutional
- Office/Professional
- Open Space/Recreational
- Parking Lot
- Planned Development
- Residential

MAP
FLUM-1-01
October 2016

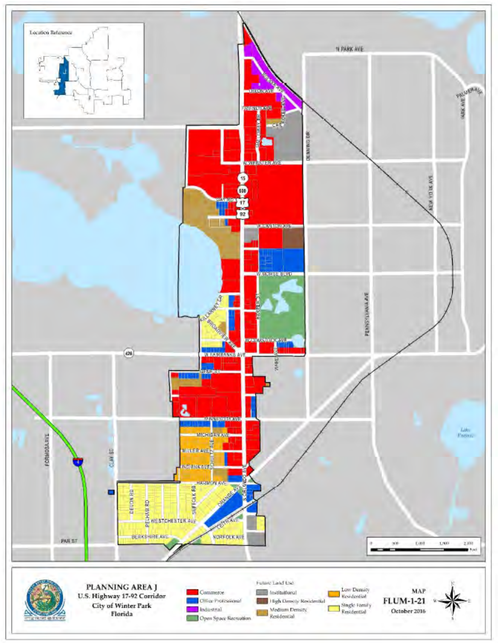
N
W
E
S

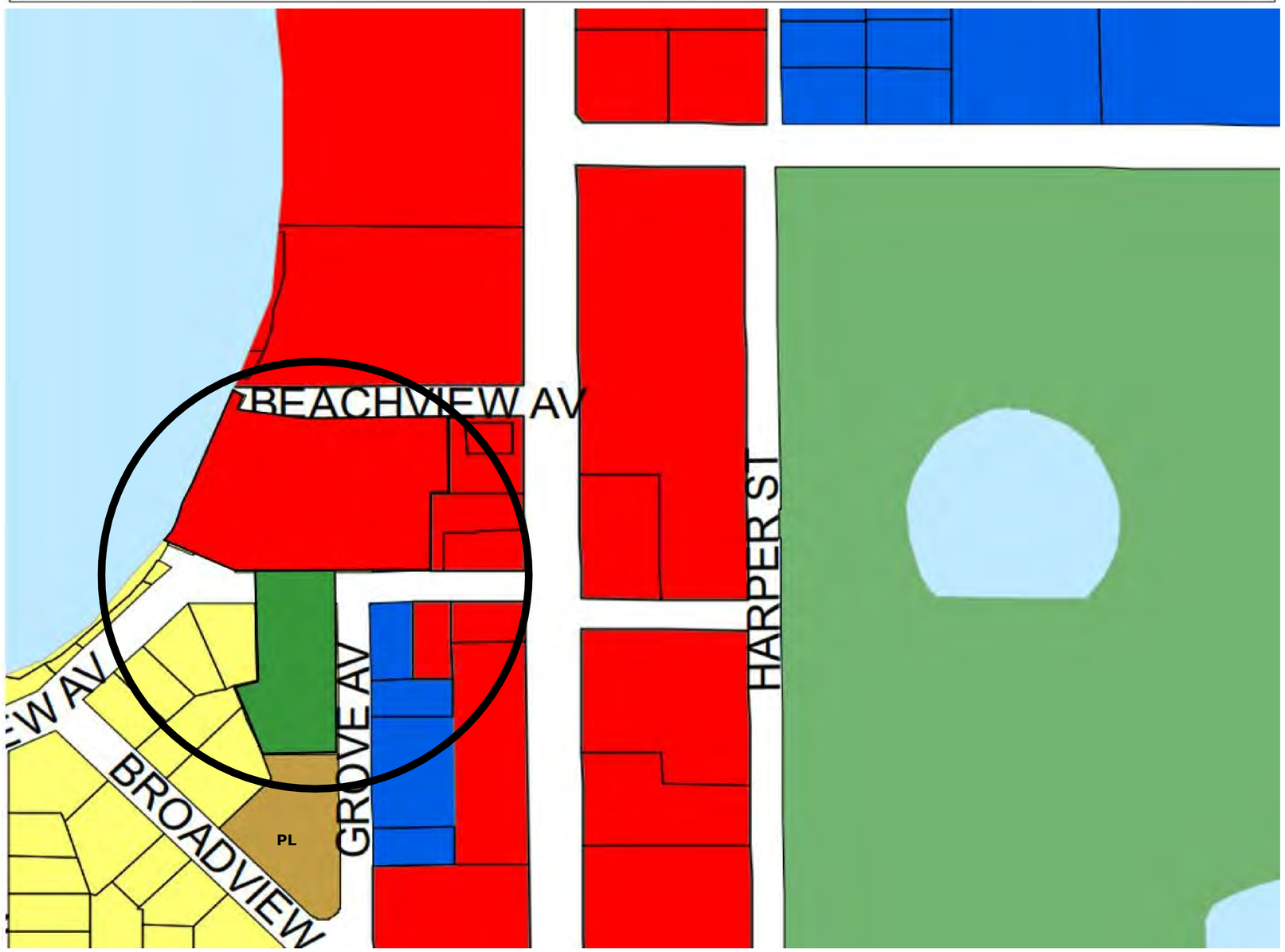


EXISTING LAND USE MAP

N

NOT FOR CONSTRUCTION

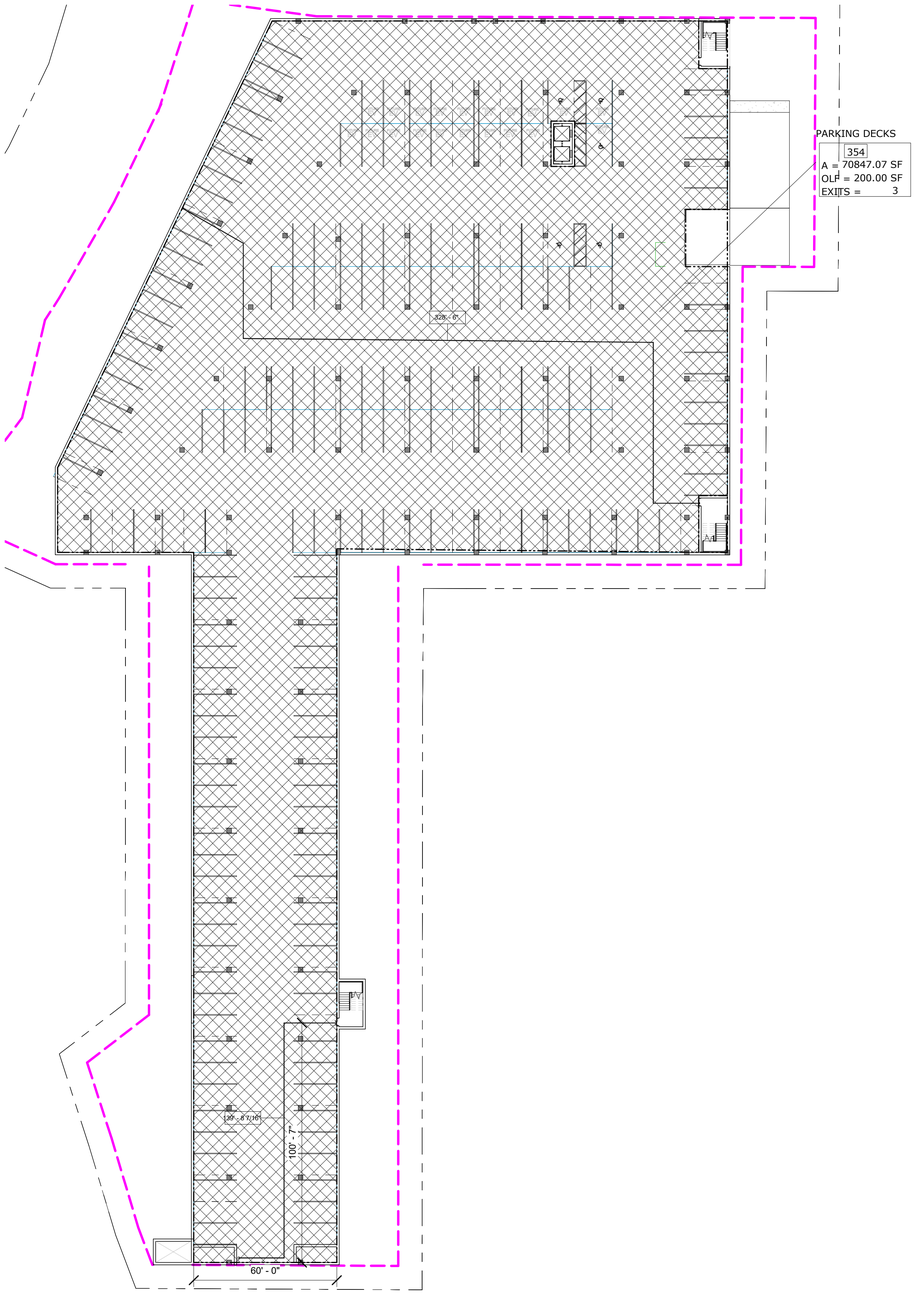




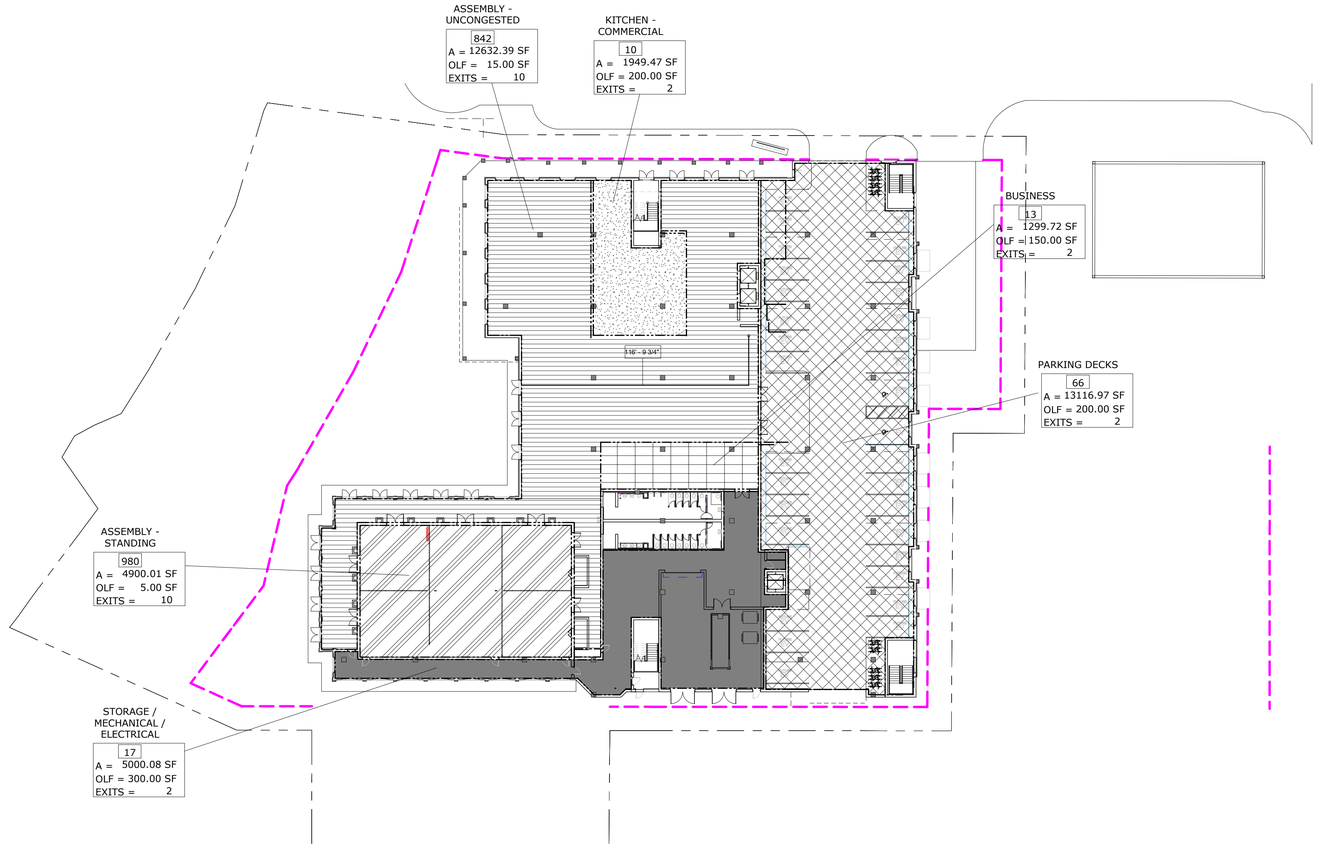
PROPOSED FUTURE LAND USE MAP

N

NOT FOR CONSTRUCTION



BASEMENT



LEVEL 01

LIFE SAFETY PLAN

N

SCALE: 1" = 30'

0 15 30 75 120'

NOT FOR CONSTRUCTION

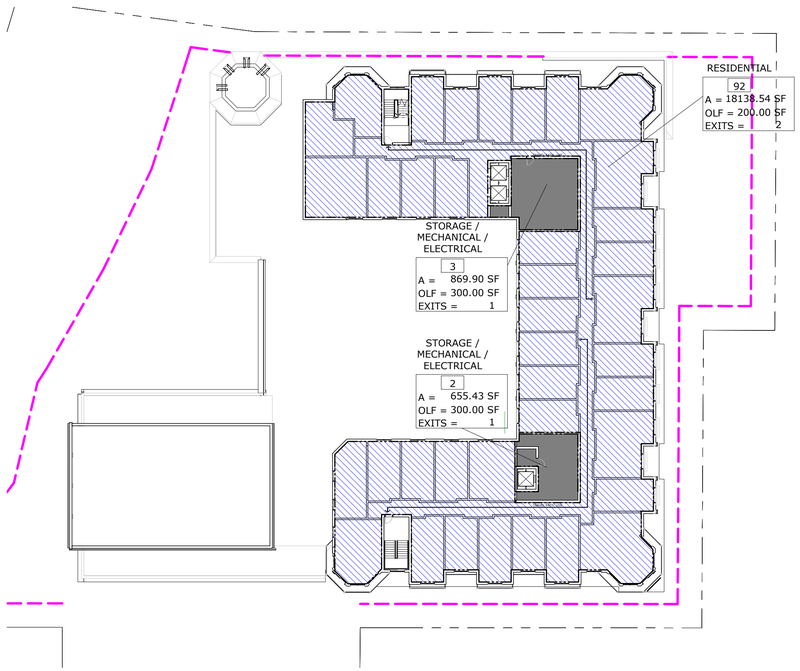
ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

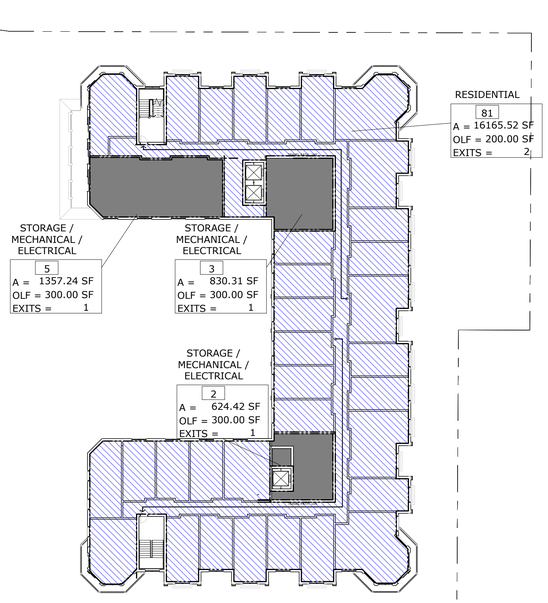
HENDERSON HOTEL
11.06.2020

G.103

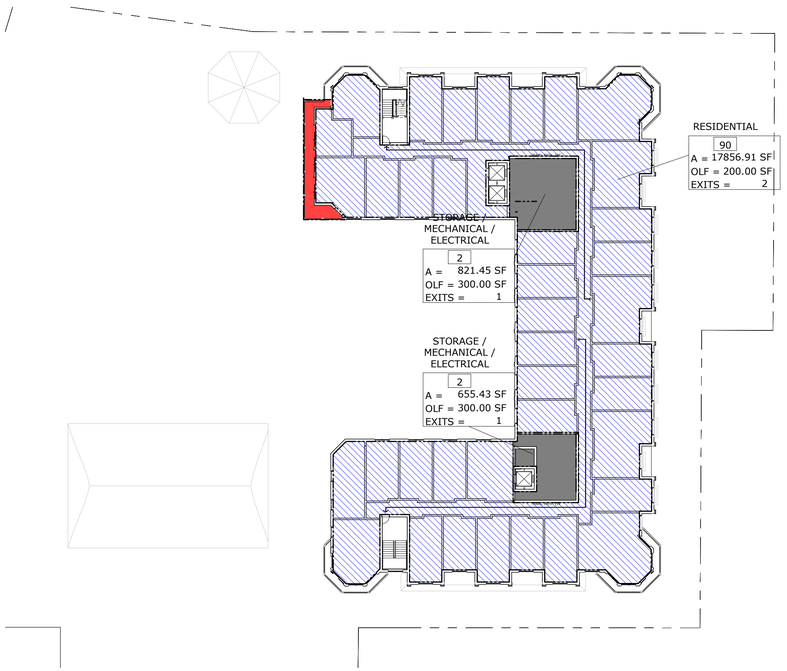
Baker Barrios
The shape of things to come.
AA0002981 + LC26000427



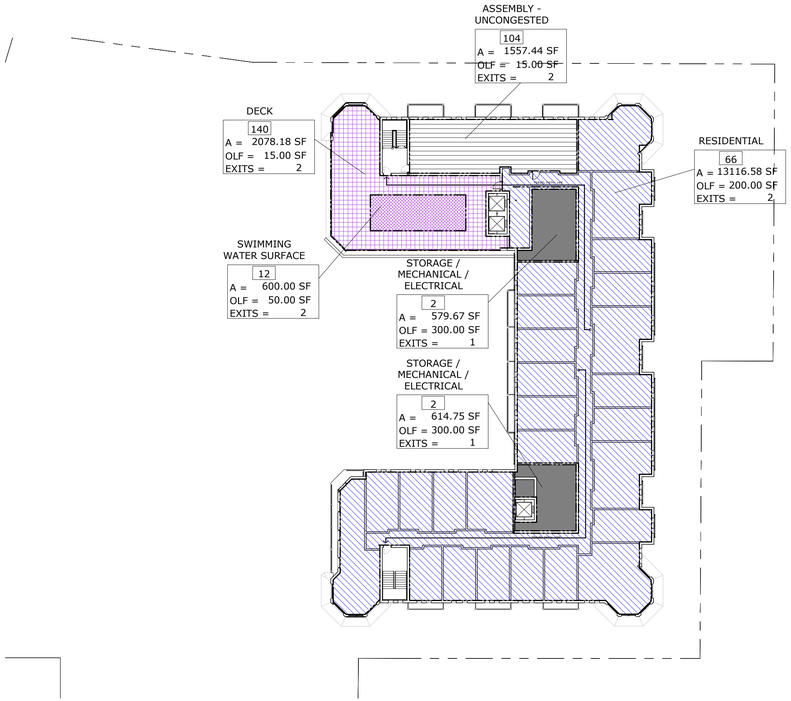
LEVEL 02



LEVEL 04



LEVEL 03



LEVEL 05

LIFE SAFETY PLAN

N

SCALE: 1" = 30'

0 15 30 75 120'

NOT FOR CONSTRUCTION

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427

Boundary Survey

ADDRESS

240 Killarney Drive, 250 Killarney Drive, 1321 Fairview Avenue, 230 Killarney Drive, 1201 Fairview Avenue, 231 S Orlando Avenue, 271 S Orlando Avenue, 1298 Beachview Avenue and 0 Killarney Drive (Pid:12-22-29-4172-07-001)
Winter Park, FL 32789

SCALE: 1"=20'

LEGEND

- □ = Concrete Monument (CM)
- ⌖ = Drill Hole (DH)
- ● = Iron Pipe & Cap (IP)
- ○ = Iron Rod & Cap (IR)
- ⊙ = Nail & Disk (N&D)
- PCP = Permanent Control Point
- PRM = Permanent Reference Monument
- OR Book = Official Record Book
- PB = Plat Book
- Pg = Page
- CONC = Concrete
- D = Central Angle
- L = Arc Length
- R = Radius
- CB = Chord Bearing
- CH = Chord Length
- FND = Found
- ID = Identification
- LB = Licensed Business
- LS = Licensed Surveyor
- PVC = Polyvinyl Chloride
- TYP. = Typical
- = Backflow Preventor
- = Cable TV Riser
- = Catch Basin/Inlet
- = Drainage Manhole
- = Electric Box
- = Fire Hydrant
- = Guy Wire
- = Light Pole
- = Mitered End Section
- = Sanitary Cleanout
- = Sanitary Manhole
- = Sign
- = Telephone Riser
- = Transformer
- = Power Pole
- ohu = Overhead Utilities
- = Well
- WM = Water Meter
- = Water Valve
- (C) = Calculated Distance
- (D) = Deed Dimension
- (F) = Field Measurement
- (R) = Radial
- (R/W) = Right of Way Map
- (P) = Plat Measurement
- (CB) = Condominium Book
- RCP = Reinforced Concrete Pipe
- on/off = On Site/Off Site
- = Grade Shot

BEACHVIEW AVENUE — 50' RIGHT OF WAY

FAIRVIEW AVENUE — 50' RIGHT OF WAY

STATE ROAD 15/600 — S ORANGE AVENUE — 80' RIGHT OF WAY

LAKE KILLARNEY — LESS THE LAKE

KILLARNEY DRIVE — LESS THE RIGHT OF WAY — 70' RIGHT OF WAY

S 83°11'43" E(F) 27.56'(C)

N 89°21'59" E(R/W) 329.53'(C)

S 66°28'57" E(C) 33.46'(F)

N 23°20'10" E(R/W) 253.76'(C)

N 23°20'10" E 274.76'(R/W)

S 89°12'36" W 441.32'(R/W)

N 31°18'49" E(C) 9.42'(C) 9.44'(R/W)

S 00°45'23" E 83.33'(R/W)

N 26°19'34" W(CB) N 27°05'29" W(C) 11.17'(C) 11.16'(CB)

L=148.54' R=34266.95' D=0°14'54"

L=115.00' R=34266.95' R=34417.50'(CB) D=0°11'32" D=0°11'29"(CB)

SITE BENCHMARK#1 ELEV: 89.59 (NAVD88) FND N&D LB 1 0.2'N 0.1'E

SITE BENCHMARK#2 ELEV: 89.90 (NAVD88) FND N&D LB 1 0.2'N 0.1'E

SITE BENCHMARK#3 ELEV: 86.36 (NAVD88) POINT OF BEGINNING SW CORNER OF LOT 1 (WATER PARCEL) FND 1"IP NO ID 0.4'N

SITE BENCHMARK#4 ELEV: 88.93 (NAVD88) FND 4x4 CM NO ID

NW CORNER OF LOT 4 FND XCUT NO ID 0.2'N

FND 1"IP NO ID 2.4'S 1.3'W

FND 1"IP NO ID 4.0'S 0.1'E

FND ½"IR NO ID 0.4'S 0.3'W

FND ½"IR LB 3505 0.2'S 0.3'W

FND ½"IR NO ID 1.5'N 0.2'E

FND ½"IR ILLEGIBLE 2.2'N 0.6'W

FND ⅝"IR LB 6994 0.2'N 2.2'W

SET ½"IR LB 7788

SET N&D LB 7788

FND N&D LB 1 0.2'N 0.1'E

FND N&D LB 3505 0.3'N 0.3'E

FND N&D LB 5237 0.2'N 0.3'E

LOT 1 — Pid:12-22-29-4172-07-030 (240 KILLARNEY DR)

LOT 2 — Pid:12-22-29-4172-07-020 (250 KILLARNEY DR)

LOT 3 — Pid:12-22-29-4172-07-011 (1321 FAIRVIEW AVE)

LOT 4 — Pid:12-22-29-4172-07-040 (230 KILLARNEY DR)

LOT 5

LOT 6 — Pid:12-22-29-4172-07-060 (1201 FAIRVIEW AVE)

LOT 7

LOT 8 — Pid:12-22-29-4172-07-080 (1298 BEACHVIEW AVE)

LOT 9

LOT 10 — Pid:12-22-29-4172-07-101 (271 S ORLANDO AVE)

LOT 11 — Pid:12-22-29-4172-07-110 (231 S ORLANDO AVE)

BLOCK 7 RESURVEY OF KILLARNEY ESTATES PLAT BOOK L, PG 9

KILLARNEY OAKS BUILDING A CONDOMINIUM CONDO BOOK 7, PG 53

Pid:12-22-29-4172-07-001 (0 KILLARNEY DR)

Pid:12-22-29-4172-07-011 (1321 FAIRVIEW AVE)

Pid:12-22-29-4172-07-030 (240 KILLARNEY DR)

Pid:12-22-29-4172-07-040 (230 KILLARNEY DR)

OUTFALL DITCH PER RIGHT OF WAY MAP

SOUTHEASTERLY TO NW CORNER LOT 4

NORTHWESTERLY TO SW CORNER OF NE1/4

SOUTHWESTERLY

4' CHAINLINK FENCE

CONC. RETAINING WALL

APPROXIMATE EDGE OF WATER ELEVATION 80.24 (NAVD88)

NORMAL HIGH WATER LINE ELEVATION 83.24 (NAVD88)

WATERLINE MARKER(TYP.)

GAS MARKER(TYP.)

CABLE MARKER(TYP.)

BRICK

4"CURB

WEST RIGHT OF WAY PER FDOT RIGHT OF WAY MAP SECTION 75030-408429-1, SHEET 8

RIGHT OF WAY TAKEN PER FDOT RIGHT OF WAY MAP SECTION 75030-408429-1, SHEET 8

24"cypress — 36"cypress — 24"tree — 36"tree cluster — 24"tree cluster — 12"magnolia — 12"oak — 6"oak — 16"oak — 14"oak — 12"palm — 8"palm — 10"palm — 18"palm — 20"palm — 6"camphor — 20"camphor — twin 12"camphor — 30"camphor

Legal Description (as furnished)

Lots 1-11, Block 7, Resurvey of Killarney Estates, according to the plat thereof as recorded in Plat Book L, Page 9, Public Records of Orange County, Florida.
Together with that parcel described as Killarney Oaks Building, a Condominium, according to Condominium Book 7, Page 53, Public Records of Orange County, Florida,
and Together with Beginning at the Southwest corner of Lot 1, Block 7, Resurvey of Killarney Estates, according to the plat thereof as recorded in Plat Book L, Page 9, Public Records of Orange County, Florida, thence run Northwesterly to the Northwest corner of Northeast ¼ of Section 12, Township 29 South, Range 30 East; thence run Southeasterly to the Northwesterly corner of said Lot 4, Block 7; thence Southwesterly to the Point of Beginning, LESS that part within the right of way and LESS that part that falls in Lake Killarney.

NOTE:
INTERIOR IMPROVEMENTS ARE NOT SHOWN HEREON, AT THE CLIENT'S REQUEST

NOTE:
ELEVATIONS SHOWN HEREON ARE BASED ON COUNTY BENCHMARK# I010004, ELEVATION 90.378 (NAVD88)

NOTE:
NORMAL HIGH WATER ELEVATION IS BASED ON ORANGE COUNTY LAKE FACT SHEET

ADDITIONAL NOTES:
3. Additions or deletions to this survey by anyone other than the signing party or parties is prohibited without the written consent of the signing party or parties.
4. The property shown hereon is subject to all easements, restrictions and reservations which may be shown or noted on the record plat and within the public records of the county the subject property is located. This survey only depicts survey related information such as easements and setbacks that are shown on a record plat or have been furnished to the Surveyor.
5. Building ties and dimensions for improvements should not be used to reconstruct boundary lines.

JOB #:	VLSR19-34237
CLIENT #:	
FIELD DATE: 05-13-19 / 06-03-19	CREW: 2100
DRAFTER:	BT
APPROVED:	JEW
SCALE:	1" = 20'

COPYRIGHT 2011

X Borrower's Acknowledgment and Acceptance

X Borrower's Acknowledgment and Acceptance

CERTIFIED TO (AS FURNISHED):
Peter Davis Neto
Attorney at Law
Chicago Title Insurance Company
Ronnie Landing, LLC

FLOOD ZONE:
SUBJECT PROPERTY SHOWN HEREON IS TO BE LOCATED IN FLOOD ZONE "X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON FLOOD INSURANCE RATE MAP NUMBER 12095C0535F, EFFECTIVE DATE SEPTEMBER 25, 2009, FOR F.I.R.M. PANEL NUMBER 0535 OF 0625). THE SURVEYOR HAS NOT PERFORMED ANY INVESTIGATION TO DETERMINE ACCURACY OF THE ABOVE INFORMATION. THE LOCAL F.E.M.A. AGENT SHOULD BE CONSULTED FOR INFORMATION.

LIST OF POSSIBLE ENCROACHMENTS:
SEAWALL AND DOCK CROSSES PROPERTY LINES
BOUNDARY LINES AND CORNERS ARE NOT MONUMENTED OR DETERMINED
SOME UTILITIES LIE WITHIN THE SUBJECT PROPERTY

BASIS OF BEARING:
ASSUMED BEARING ON THE WEST RIGHT OF WAY OF S ORLANDO AVENUE WHICH HAS A BEARING OF S 00°45'23" E PER FOOT RIGHT OF WAY MAP SECTION 75030-408429-1, SHEET 8

NOTES:
1. UNDERGROUND UTILITY INSTALLATIONS, UNDERGROUND IMPROVEMENTS AND ENCROACHMENTS, IF ANY, NOT LOCATED BY THIS SURVEY.
2. THE PURPOSE OF THIS SURVEY IS FOR USE IN DEFINING TITLE INSURANCE AND LENDER REQUIREMENTS.
THIS SURVEY IS PREPARED FOR THE EXCLUSIVE USE AND BENEFIT OF THE PARTIES LISTED HEREON AND IS NOT TRANSFERABLE TO ANY OTHER PARTY.

LB 7788

VISIONLAND
341 E Pennsylvania Ave, Winter Park, FL 32789 | (888) 556-8474

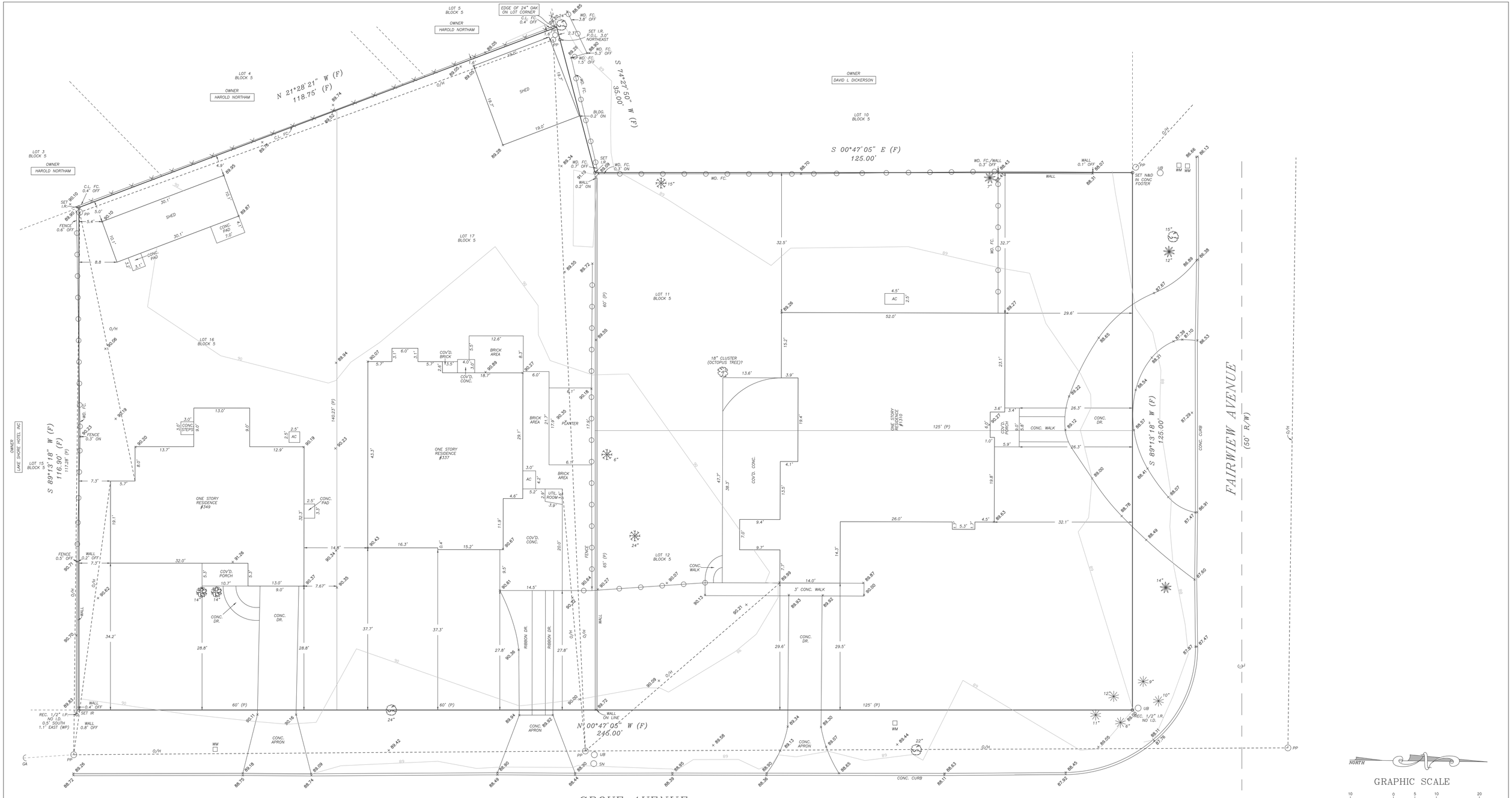
SURVEYOR'S CERTIFICATE
I HEREBY CERTIFY THAT THIS SURVEY IS A TRUE AND ACCURATE REPRESENTATION OF THE LAND HEREIN DESCRIBED AS DETERMINED BY ME OR UNDER MY DIRECTION AND SUPERVISION AND THAT IT MEETS THE MINIMUM TECHNICAL STANDARDS SET FORTH BY THE FLORIDA BOARD OF PROFESSIONAL SURVEYORS AND MAPPERS IN CHAPTER 5J-17 OF THE FLORIDA ADMINISTRATIVE CODE, PURSUANT TO SECTION 472.027 OF THE FLORIDA STATUTES.

DATE	REVISION
06/26/19	Add Encroachments
06/26/19	Add Area of Parcel
06/26/19	Add Area of Parcel

DATED: 05-14-19

STATE OF FLORIDA — No. 6879

Joseph E. Williamson, PLS
PROFESSIONAL LAND SURVEYOR
FLORIDA CERTIFICATE NO. 6879
NOT VALID WITHOUT THE ELECTRONIC SIGNATURE AND/OR ORIGINAL RAISED SEAL OF THE LISTED FLORIDA LICENSED SURVEYOR AND MAPPER.



DESCRIPTION AS FURNISHED: Lots 11, 12, 16 and 17, Block 5, KILLARNEY ESTATES, as recorded in Plat Book L, Page 9 of the Public Records of Orange County, Florida.

BOUNDARY SURVEY FOR / CERTIFIED TO: Winter Park Historic Hotels Group, Inc.

ALTA/NSPS Land Title Survey

To: Winter Park Historic Hotels Group, Inc.

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 6a, 7a, 8, 9, 13 and 16 of Table A thereof. The field work was completed on (11–27–2019)

Date: 12–03–2019

Thomas X. Grusenmeyer, RLS #4714
State of Florida

SURVEYOR'S NOTES

1) SUBJECT SITE CONTAINS: 32,206 SQ.FT. OR 0.7393 ACRES MORE OR LESS

ALTA TABLE A COMMENTS

ITEM 6: NO ZONING INFORMATION FURNISHED TO SURVEYOR

ITEM 9: NO STRIPED PARKING OBSERVED

ITEM 16: NO EVIDENCE OF RECENT EARTH WORK AND/OR CONSTRUCTION OBSERVED

TREE LEGEND

= OAK
= CEDAR
= CAMPHOR
= PALM
= PINE

GRUSENMEYER – SCOTT & ASSOC., INC. – LAND SURVEYORS

5400 E. COLONIAL DR. ORLANDO, FL. 32807 (407)–277–3232 FAX (407)–658–1436
CERTIFICATE OF AUTHORIZATION – LB 4596

LEGEND:
B.M.=BENCHMARK
℄=CENTERLINE
CALC=CALCULATED
CM=CONCRETE MONUMENT
C.L. FC.=CENTERLINE LINK FENCE
CONC.=CONCRETE
C/B=CONCRETE BLOCK
A/C=AIR CONDITIONER
COV'D.=COVERED
EL.=ELEVATION
ELEC.=ELECTRIC BOX
ELEV.=ELEVATION
EP=EDGE OF PAVEMENT
FH=FIRE HYDRANT
F.F.=FINISHED FLOOR ELEVATION
FM=FLOW METER
FC=FENCE CORNER
FW=FIRE HOSE CONNECTOR
GA=GUY ANCHOR
GM=GAS METER
GP=GUARD POST
GV=GAS VALVE
INV=INVERT
IR=IRON ROD
LP=LIGHT POLE
LP#=LIGHT POLE
WM=WATER METER
MH=MAN HOLE
N&D=NAIL AND DISK
O/H=OVERHEAD LINE
P=PLAT
U/P=UTILITY POLE
MES=METERED END SECTION
SV=SEWER VALVE
FO=FIBER OPTIC MARKER

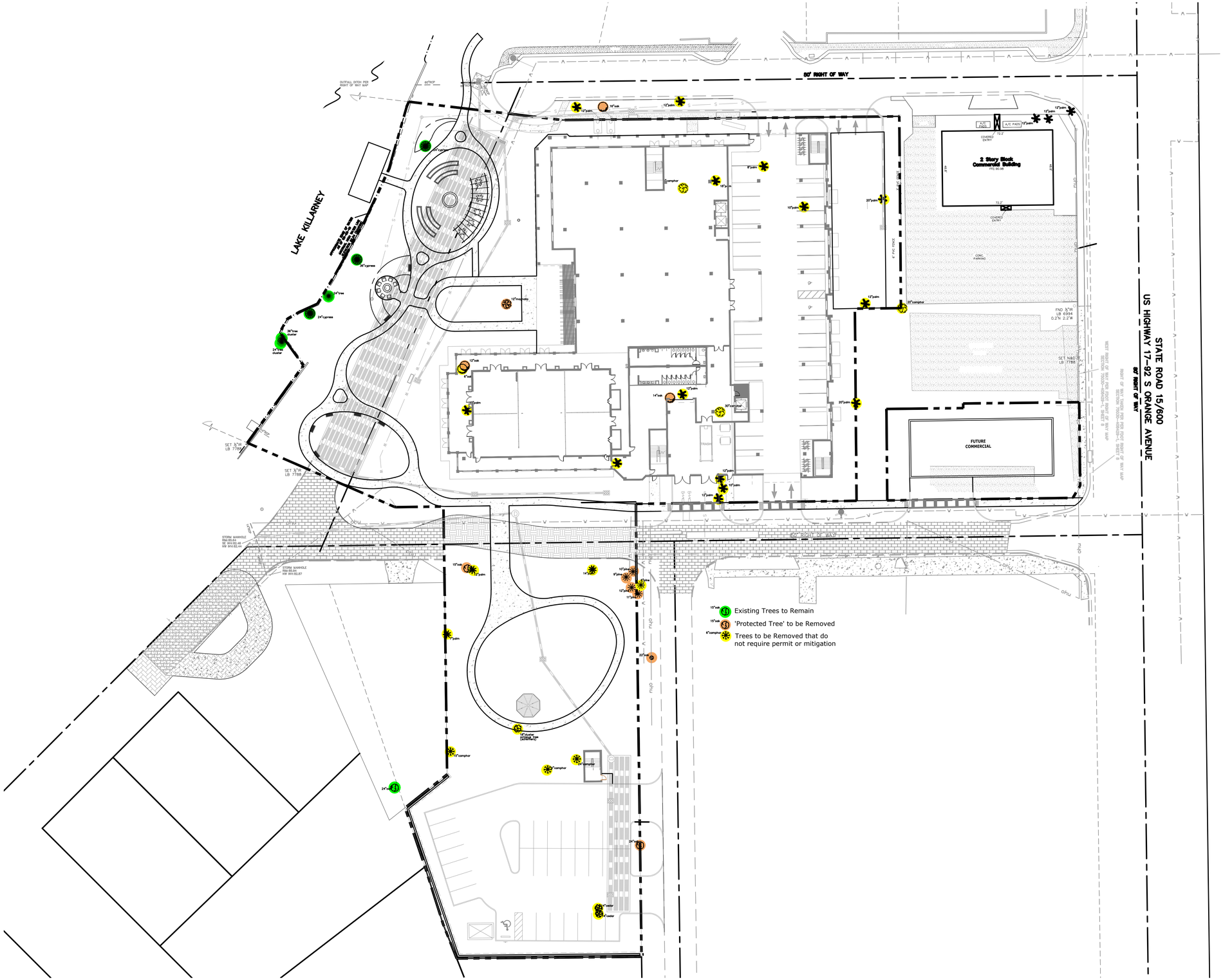
THIS BUILDING/PROPERTY DOES NOT LIE WITHIN A SPECIAL FLOOD HAZARD AREA AS PER "FIRM" ZONE "X" MAP # 12095C0325F (09–25–2009)

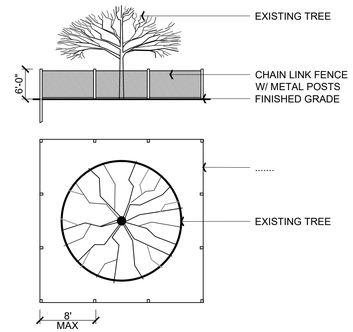
LEGEND:
POC=POINT OF COMMENCEMENT
POB=POINT OF BEGINNING
RCP=REINFORCED CONC PIPE
R/W=RIGHT OF WAY
REC.=RECOVERED
SET N&D=NAIL #4596
SIR=SET 5/8"x1/2" IRON ROD W/LB #4596
TB=TELEPHONE BOX
TYP=TYPICAL
WF=WOOD FENCE
WP=WITNESS POINT
WV=WATER VALVE
WM=WATER METER
TRANS(TR)=TRANSFORMER
TOB=TOP OF BANK
L = ARC LENGTH
CB = CHORD BEARING
Δ = DELTA
R = RADIUS
C = CHORD LENGTH
DI = DRAINAGE INLET
ID = IDENTIFICATION
BM = BENCHMARK
BB = BASE BEARING
TBM = TEMPORARY BENCHMARK
(NO I.D.) = NO CAP IDENTIFICATION
UB = UTILITY BOX
UV=UTILITY VAULT
± = MORE OR LESS
N/A=NOT APPLICABLE
BFP=BACK FLOW PREVENTER
SB=SIGNAL BOX
SN=SIGNAL NODE
PP=POWER POLE

NOTES:
1. THE UNDERSIGNED DOES HEREBY CERTIFY THAT THIS SURVEY MEETS THE STANDARDS OF PRACTICE AS SET FORTH BY THE FLORIDA BOARD OF PROFESSIONAL SURVEYORS AND MAPPERS IN CHAPTER 5J–17 (FLORIDA ADMINISTRATIVE CODE) PURSUANT TO SECTION 472.027 FLORIDA STATUTES.
2. UNLESS EMBOSSED WITH SURVEYOR'S SIGNATURE AND SEAL, THIS SURVEY IS NOT VALID AND IS PRESENTED FOR INFORMATIONAL PURPOSES ONLY.
3. THIS SURVEY WAS PREPARED FROM TITLE INFORMATION FURNISHED TO THE SURVEYOR. THERE MAY BE OTHER RESTRICTIONS OR EASEMENTS THAT AFFECT THIS PROPERTY.
4. NO UNDERGROUND IMPROVEMENTS HAVE BEEN LOCATED UNLESS OTHERWISE SHOWN.
5. THIS SURVEY IS PREPARED FOR THE SOLE BENEFIT OF THOSE CERTIFIED TO AND SHOULD NOT BE RELIED UPON BY ANY OTHER ENTITY.
6. DIMENSIONS SHOWN FOR THE LOCATION OF IMPROVEMENTS HEREON SHOULD NOT BE USED TO RECONSTRUCT BOUNDARY LINES.
7. BEARINGS, IF SHOWN, ARE BASED ASSUMED DATUM AND ON THE LINE SHOWN AS BASE BEARING (B.B.)
8. ELEVATIONS, IF SHOWN, ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988(UNLESS OTHERWISE NOTED)
9. BENCHMARK USED: ORANGE COUNTY # 1645020 ELEV.=94.555 (NAVD88)

CERTIFIED BY:
JAMES W. SCOTT, REGISTERED LAND SURVEYOR #4801
TOM X. GRUSENMEYER, REGISTERED LAND SURVEYOR #4714

FIELD DATE	ORDER #
BOUNDARY SURVEY 11–27–2019	7770–19
ADDED TREES 07–07–2020	20–4406





NOTES:
1. NO PARKING OR USE OF VEHICLES, EQUIPMENT OR MACHINERY INSIDE FENCED AREA.
2. NO STORING OR DUMPING ANY MATERIALS OR LIQUIDS INSIDE FENCED AREA.
3. NO CONSTRUCTION, EXCAVATION OR TRENCHING INSIDE FENCED AREA.
4. TREE PROTECTION TO REMAIN UNTIL SUBSTANTIAL COMPLETION OR AS DIRECTED BY THE LANDSCAPE ARCH.
5. TREE PROTECTION SIGNS PLACED EVERY 35' MIN.

EXISTING TREE PROTECTION DETAIL

SCALE: NTS

TREE PROTECTION & PRESERVATION PLAN

N

0 30 60 90 feet

1" = 30'

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

SCHEMATIC PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

L050

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427

LANDSCAPE PLAN

SCALE: 1" = 30'

0' 15' 30' 75' 120'

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

L.101

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427

TREE REPLACEMENT REQUIREMENTS

11 PROTECTED TREES ARE BEING REMOVED FROM THE SITE
PROTECTED TREES WITH DBH BETWEEN 9-19 INCHES = 1 REPLACEMENT TREE IS REQUIRED
PROTECTED TREES WITH DBH LARGER THAN 19 INCHES = 2 REPLACEMENT TREES REQUIRED
TREES TO BE REMOVED:

- 9 TREES BETWEEN 9-19 INCHES = 9 REPLACEMENT TREES
- 2 TREES LARGER THAN 19 INCES = 4 REPLACEMENT TREES
- TOTAL REPLACEMENT TREES REQUIRED: 13

REPLACEMENT TREES GUIDELINES:

- 1 SHADE TREE WITH A CALIPER OF 3" OR GREATER
- 3 UNDERSTORY TREES WITH A CALIPER OF 2" OR GREATER FOR TOTAL OF 6" CALIPER

REPLACEMENT TREES PROVIDED:
SHADE TREES WITH A CALIPER OF 3" OR GREATER: 43
UNDERSTORY TREES WITH A CALIPER OF 2" OR GREATER: 60 = 20 REPLACEMENT TREES

TOTAL REPLACEMENT TREES PROVIDED: 63

LANDSCAPE CODE REQUIREMENTS

BUILDING FACADE LANDSCAPING

NORTH: 189 LF OF BUILDING FACADE FACING R.O.W.
8' WIDE REQUIRED LANDSCAPE

- 1512 SF LANDSCAPE REQUIRED
- 1538 SF LANDSCAPE PROVIDED

5 UNDERSTORY TREES FOR EVERY 100 LF

- 10 TREES REQUIRED
- 10 TREES PROVIDED

SOUTH: 125 LF OF BUILDING FACADE FACING R.O.W.
8' WIDE REQUIRED LANDSCAPE

- 1000 SF LANDSCAPE REQUIRED
- 1064 SF LANDSCAPE PROVIDED

5 UNDERSTORY TREES FOR EVERY 100 LF

- 7 TREES REQUIRED
- 7 TREES PROVIDED

VEHICLE USE AREA LANDSCAPING - AT RESIDENTIAL

MIN 10' WIDE REQUIRED LANDSCAPE BETWEEN VEHICLE USE AREAS AND R.O.W.

- 260 LF OF VEHICULAR USE AREA ADJACENT TO RESIDENTIAL TOTAL
- 6' HEIGHT MASONRY WALL PROVIDED

1 SHADE TREE EVERY 60 LF

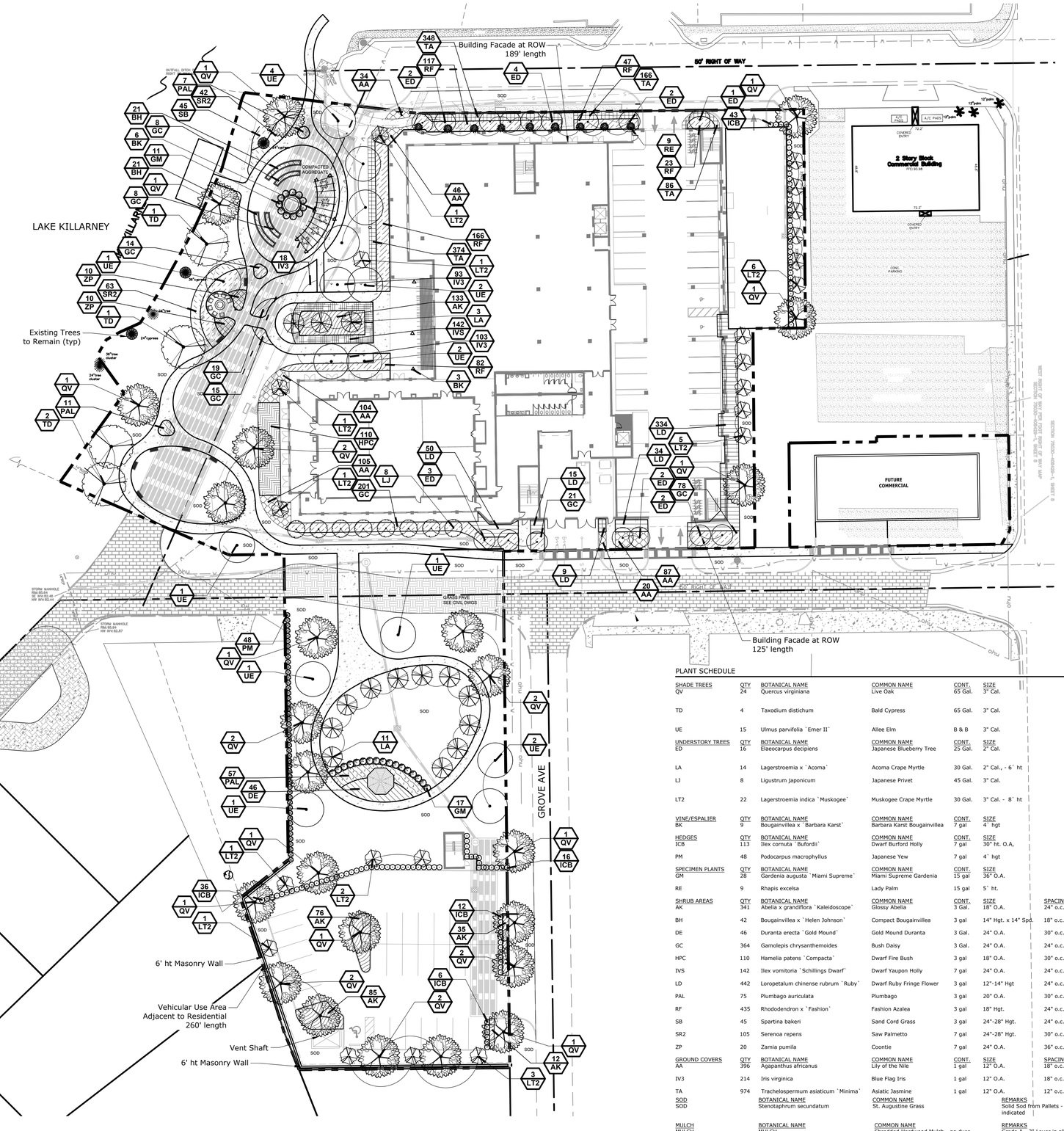
- 5 SHADE TREES REQUIRED
- 5 SHADE TREES PROVIDED

1 UNDERSTORY TREE EVERY 60 LF

- 5 UNDERSTORY TREES REQUIRED
- 5 UNDERSTORY TREES PROVIDED

NOTES

1. ALL LANDSCAPE AREAS WILL BE IRRIGATED WITH AN AUTOMATIC IRRIGATION SYSTEM WITH A RAIN SENSOR.
2. ALL LANDSCAPE MATERIAL SHALL BE 'FLORIDA NO. 1' OR GREATER AS ESTABLISHED IN THE LATEST FLORIDA GRADES ANDS TANDARDS MANUAL FOR NURSERY PLANTS BY STATE OF FLORIDA DEPARTMENT OF AGRICULTURE AND CONSUMER SERVICES, DIVISION OF PLANT INDUSTRIES.
3. TREE REMOVAL PERMIT REQUIRED BEFORE REMOVING AN 'PROTECTED' TREES AS DESIGNATED BY CITY OF WINTER PARK LAND DEVELOPMENT CODE.
4. ALL TREES LOCATED ON TOP OF UNDERGROUND PARKING WILL BE INSTALLED IN PLANTERS COORDINATED WITH THE STRUCTURAL ENGINEERING OF THE PARKING GARAGE.



PLANT SCHEDULE

SHADE TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT.	SIZE	REMARKS
QV	24	Quercus virginiana	Live Oak	65 Gal.	3" Cal.	Single / straight trunk / full, 12` hgt.min.
TD	4	Taxodium distichum	Bald Cypress	65 Gal.	3" Cal.	Single / straight trunk / full, 12` hgt. min.
UE	15	Ulmus parvifolia `Emer II`	Allee Elm	B & B	3" Cal.	12` hgt/ Single / straight trunk / full
UNDERSTORY TREES	**QTY**	**BOTANICAL NAME**	**COMMON NAME**	**CONT.**	**SIZE**	**REMARKS**
ED	16	Elaeocarpus decipiens	Japanese Blueberry Tree	25 Gal.	2" Cal.	8` hgt x 4` spd/ Single straight trunk / full
LA	14	Lagerstroemia x `Acoma`	Acoma Crape Myrtle	30 Gal.	2" Cal., - 6` ht	Standard, 4` spread
LJ	8	Ligustrum japonicum	Japanese Privet	45 Gal.	3" Cal.	7` hgt./ 7-9 main trunks; well developed specimen form, 3` CT,
LT2	22	Lagerstroemia indica `Muskogee`	Muskogee Crape Myrtle	30 Gal.	3" Cal. - 8` ht	Multi-trunk, 4-5 trunks / full, 4` spread
VINE/ESPALIER	**QTY**	**BOTANICAL NAME**	**COMMON NAME**	**CONT.**	**SIZE**	**REMARKS**
BK	9	Bougainvillea x `Barbara Karst`	Barbara Karst Bougainvillea	7 gal	4` hgt	Full
HEDGES	**QTY**	**BOTANICAL NAME**	**COMMON NAME**	**CONT.**	**SIZE**	**REMARKS**
ICB	113	Ilex cornuta `Bufordii`	Dwarf Burford Holly	7 gal	30" ht. O.A,	Full
PM	48	Podocarpus macrophyllus	Japanese Yew	7 gal	4` hgt	Full
SPECIMEN PLANTS	**QTY**	**BOTANICAL NAME**	**COMMON NAME**	**CONT.**	**SIZE**	**REMARKS**
GM	28	Gardenia augusta `Miami Supreme`	Miami Supreme Gardenia	15 gal	36" O.A.	Full
RE	9	Rhapis excelsa	Lady Palm	15 gal	5` ht.	Full / 7 canes min. / staggered hts.
SHRUB AREAS	**QTY**	**BOTANICAL NAME**	**COMMON NAME**	**CONT.**	**SIZE**	**SPACING** / **REMARKS**
AK	341	Abelia x grandiflora `Kaleidoscope`	Glossy Abelia	3 Gal.	18" O.A.	24" o.c. / Full
BH	42	Bougainvillea x `Helen Johnson`	Compact Bougainvillea	3 gal	14" Hgt. x 14" Spd.	18" o.c. / Bush
DE	46	Duranta erecta `Gold Mound`	Gold Mound Duranta	3 Gal.	24" O.A.	30" o.c. / Full
GC	364	Gamolepis chrysanthemoides	Bush Daisy	3 Gal.	24" O.A.	24" o.c. / Full
HPC	110	Hamelia patens `Compacta`	Dwarf Fire Bush	3 gal	18" O.A.	30" o.c. / Full
IVS	142	Ilex vomitoria `Schillings Dwarf`	Dwarf Yaupon Holly	7 gal	24" O.A.	24" o.c. / Full
LD	442	Loropetalum chinense rubrum `Ruby`	Dwarf Ruby Fringe Flower	3 gal	12`-14" Hgt	24" o.c. / very full
PAL	75	Plumbago auriculata	Plumbago	3 gal	20" O.A.	30" o.c. / Full
RF	435	Rhododendron x `Fashion`	Fashion Azalea	3 gal	18" Hgt.	24" o.c. / Full
SB	45	Spartina bakeri	Sand Cord Grass	3 gal	24"-28" Hgt.	24" o.c. / Full
SR2	105	Serenoa repens	Saw Palmetto	7 gal	24"-28" Hgt.	30" o.c. / Full
ZP	20	Zamia pumila	Coontie	7 gal	24" O.A.	36" o.c. / Full
GROUND COVERS	**QTY**	**BOTANICAL NAME**	**COMMON NAME**	**CONT.**	**SIZE**	**SPACING** / **REMARKS**
AA	396	Agapanthus africanus	Lily of the Nile	1 gal	12" O.A.	18" o.c. / Full
IV3	214	Iris virginica	Blue Flag Iris	1 gal	12" O.A.	18" o.c. / Full
TA	974	Trachelospermum asiaticum `Minima`	Asiatic Jasmine	1 gal	12" O.A.	12" o.c. / Full / 6 runners.
SOD		**BOTANICAL NAME**	**COMMON NAME**			**REMARKS**
SOD		Stenotaphrum secundatum	St. Augustine Grass			Solid Sod from Pallets - All disturbed areas unless otherwise indicated
MULCH		**BOTANICAL NAME**	**COMMON NAME**			**REMARKS**
MULCH		MULCH	Shredded Hardwood Mulch - no dyes			Grade A - 3" Layer in all landscape areas excluding sod. Provide 6' diameter tree rings for trees planted in sod areas.

CONCEPTUAL LANDSCAPE PLAN & PLANT SCHEDULE

0 30 60 90 feet
1" = 30'

N

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

SCHEMATIC PACKAGE
PROJ. #180264.00

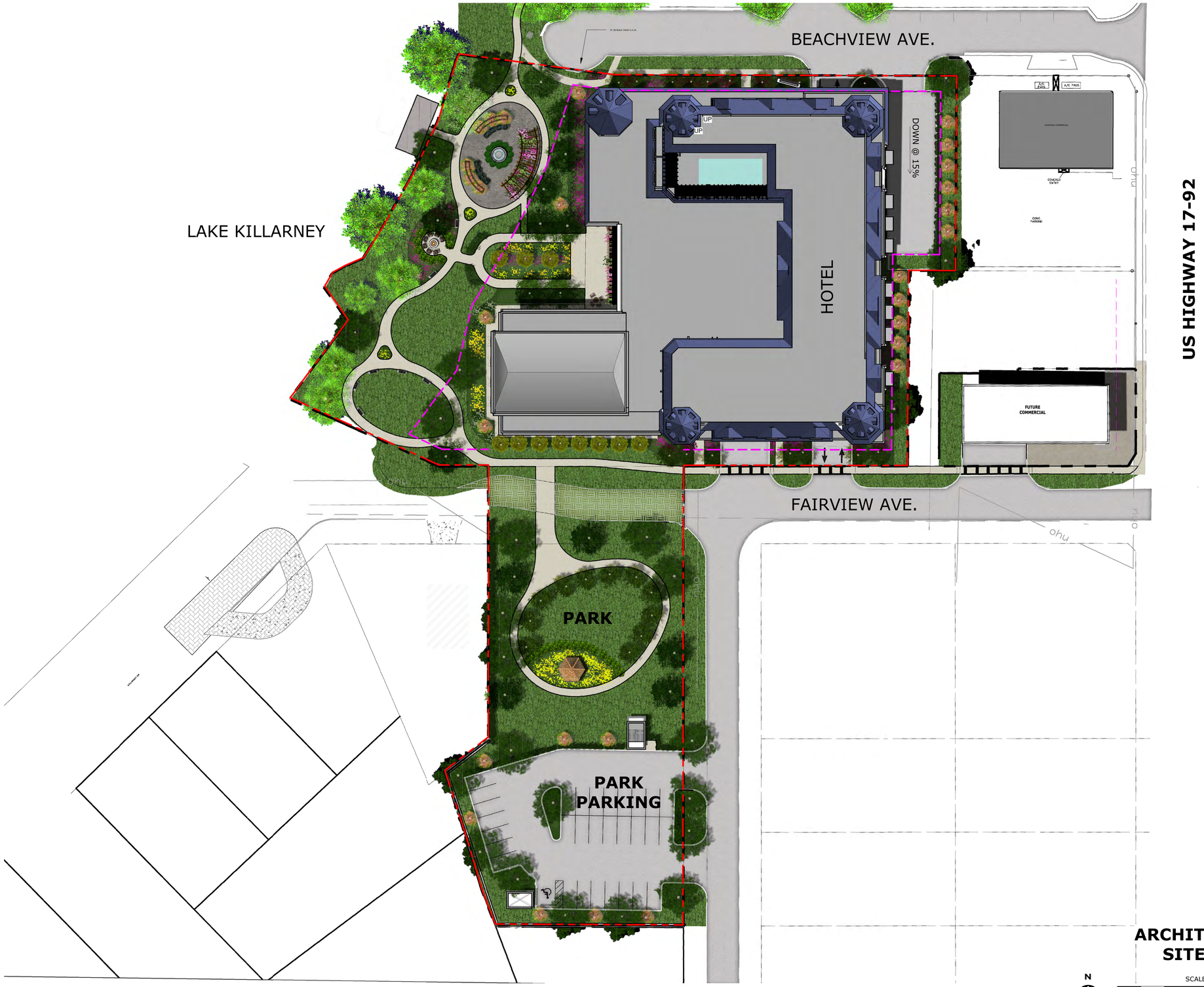
HENDERSON HOTEL
11.06.2020

L300

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427

BUILDING AREA SUMMARY

LEVEL 01	44,690 SF
LEVEL 02	22,235 SF
LEVEL 03	22,235 SF
LEVEL 04	21,650 SF
LEVEL 05	18,290 SF
TOTAL	129,100 SF



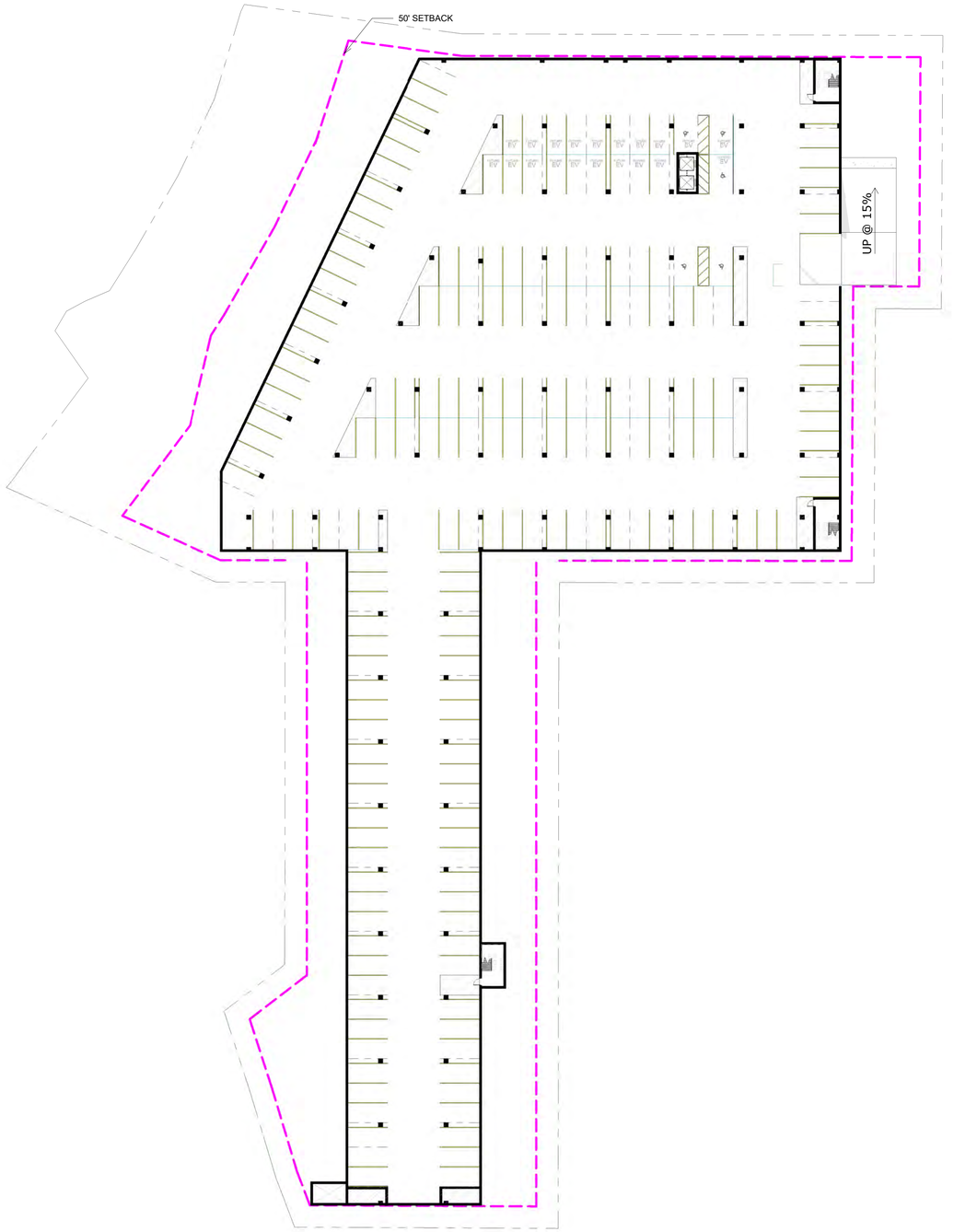
ARCHITECTURAL SITE PLAN

N

SCALE: 1" = 30'

0 15 30 75 120'

NOT FOR CONSTRUCTION



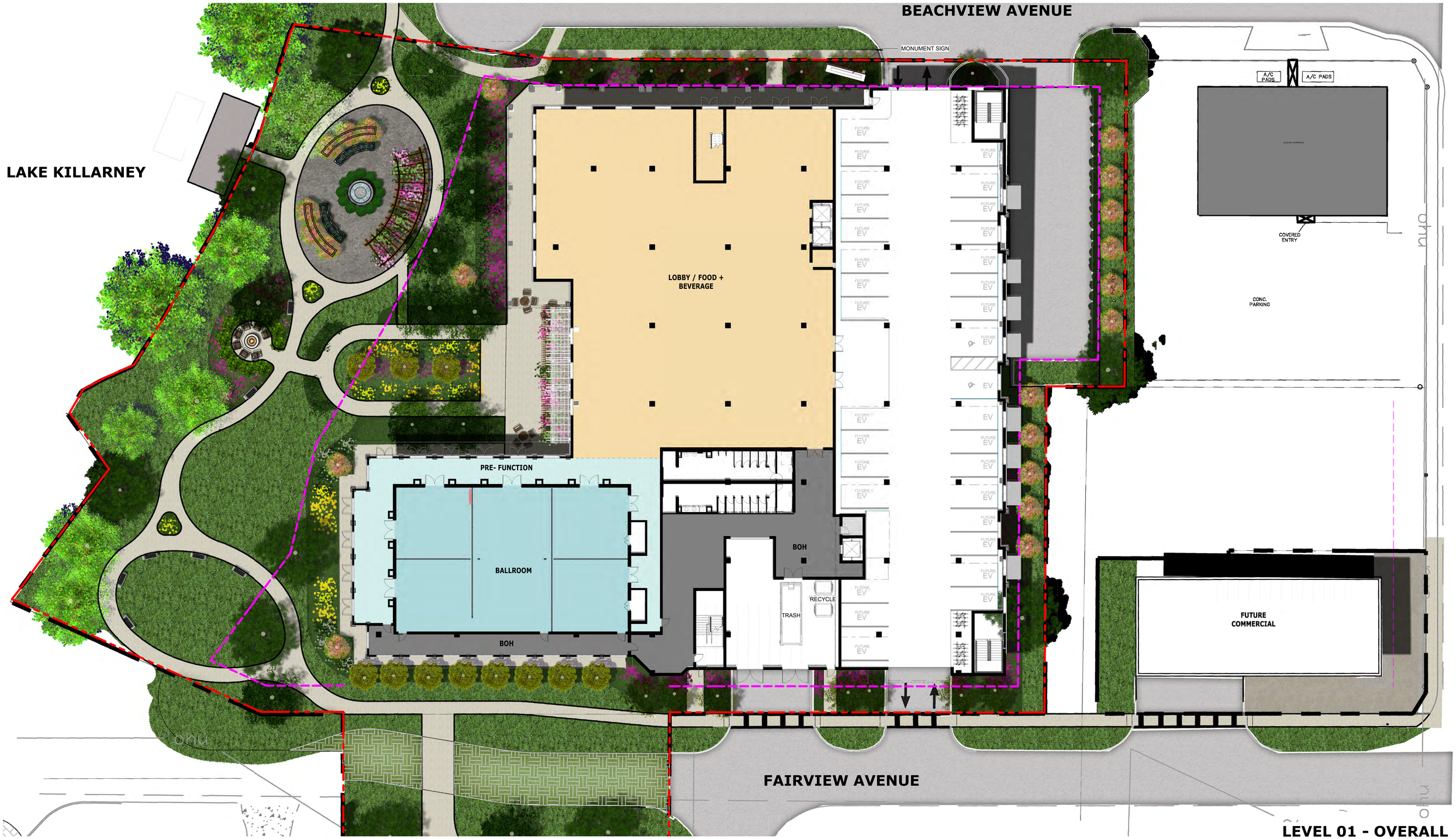
BASEMENT - OVERALL FLOOR PLAN

N

SCALE: 1" = 30'

0 15 30 75 120'

NOT FOR CONSTRUCTION



LEVEL 01 - OVERALL FLOOR PLAN

N

SCALE: 1/16" = 1'

0 8 16 40 64'

NOT FOR CONSTRUCTION

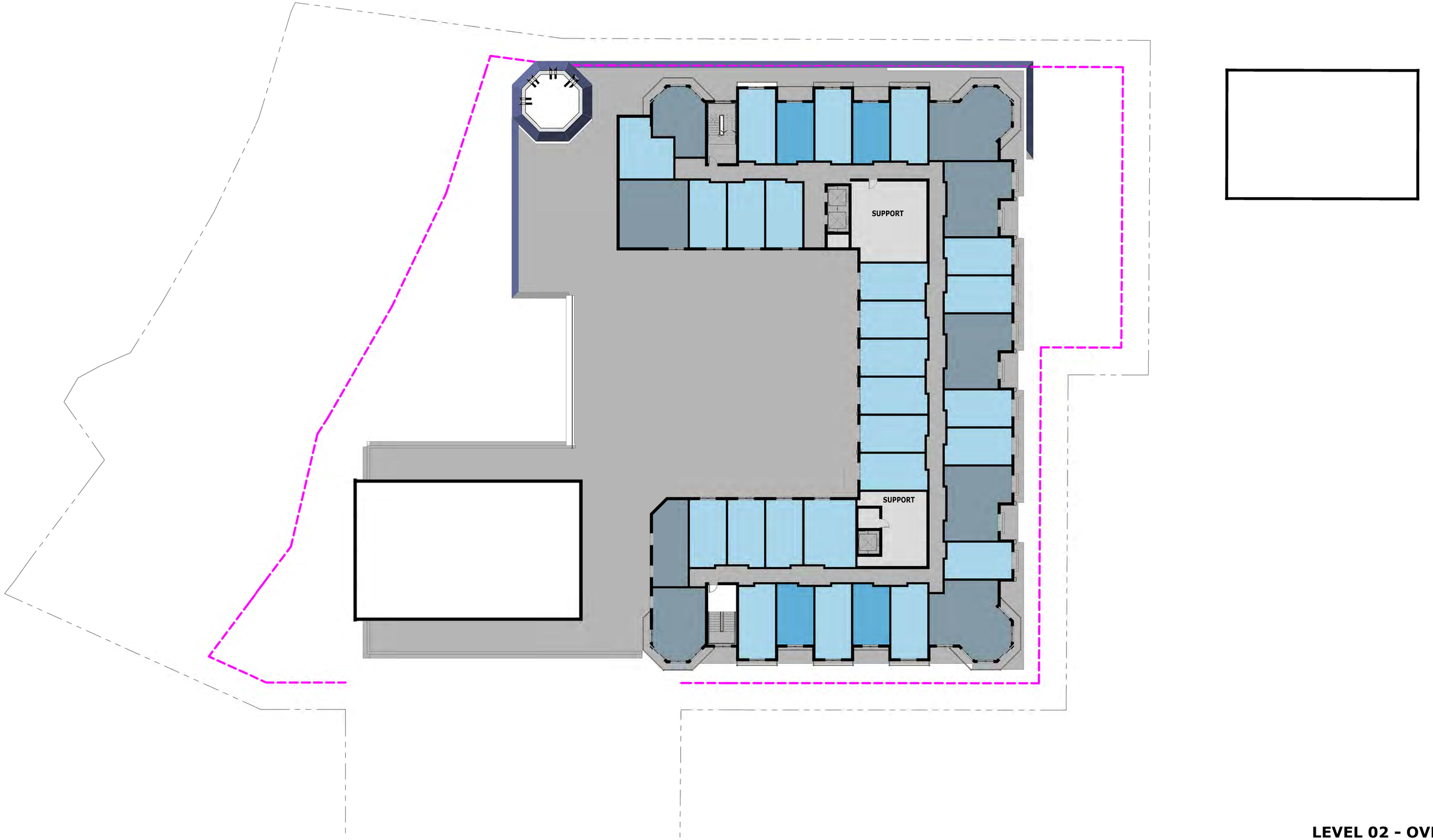
ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

A.101

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427



LEVEL 02 - OVERALL FLOOR PLAN

N

SCALE: 1/16" = 1'

0 8 16 40 64'

NOT FOR CONSTRUCTION

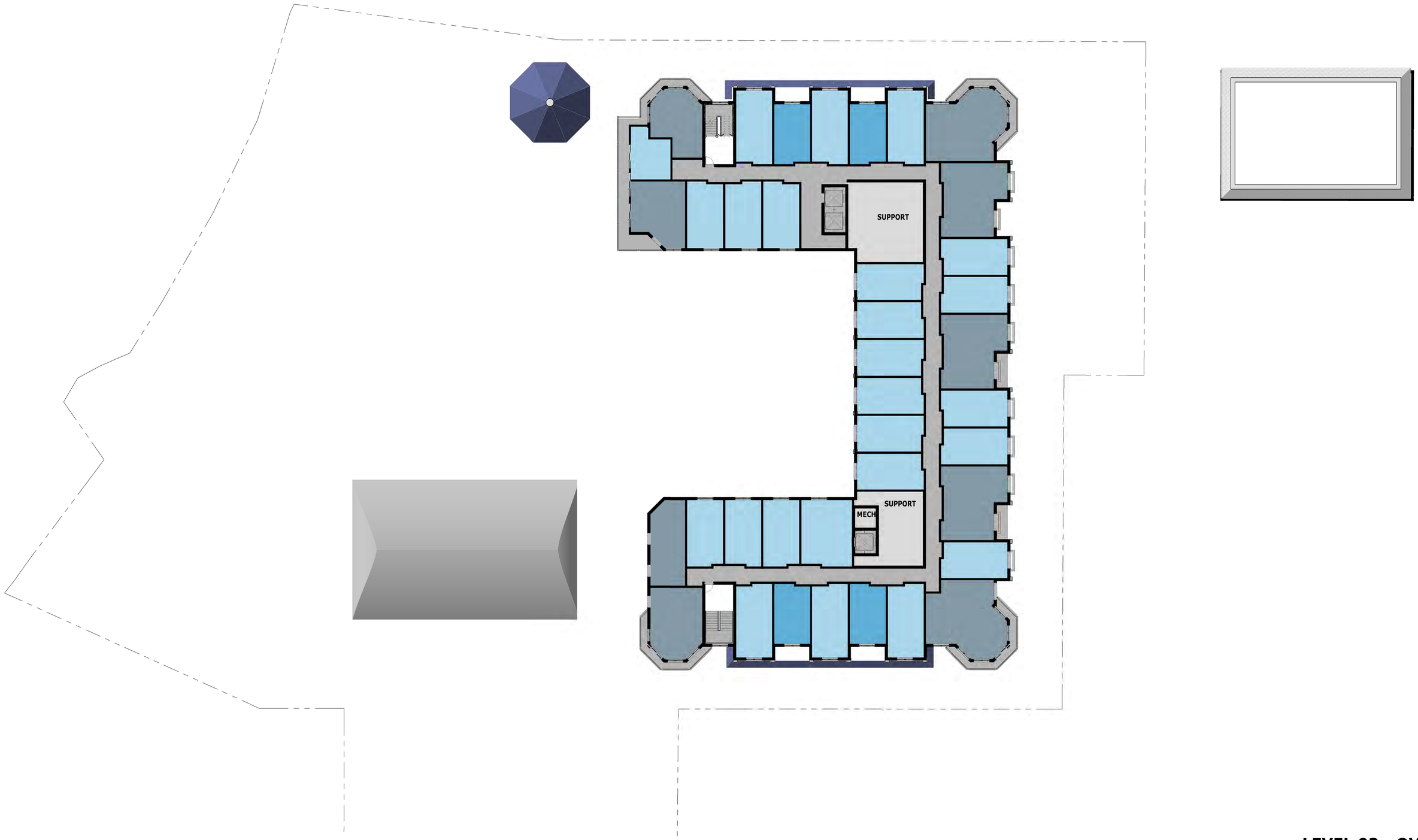
ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

A.102

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427



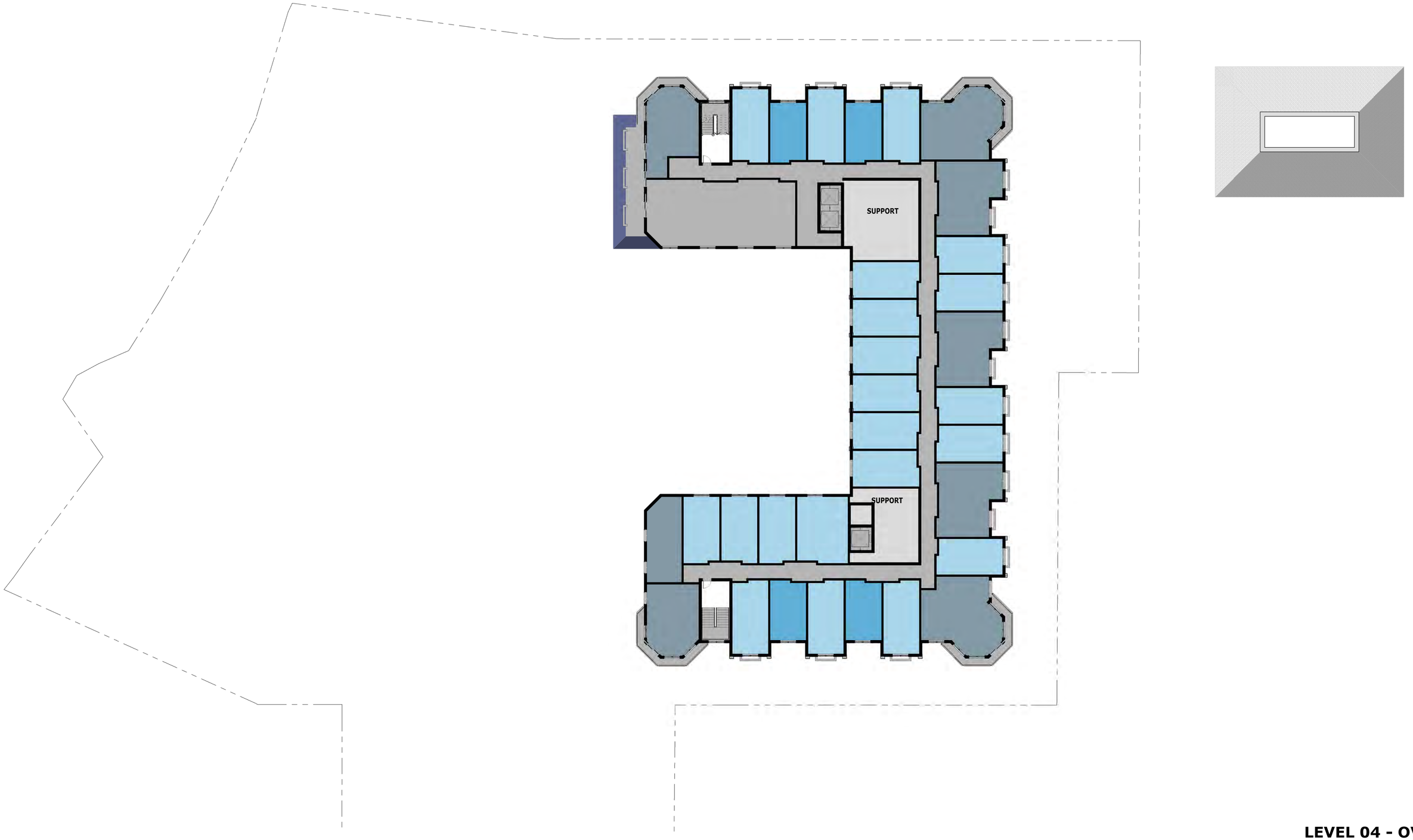
LEVEL 03 - OVERALL FLOOR PLAN

N

SCALE: 1/16" = 1'

0 8 16 40 64'

NOT FOR CONSTRUCTION



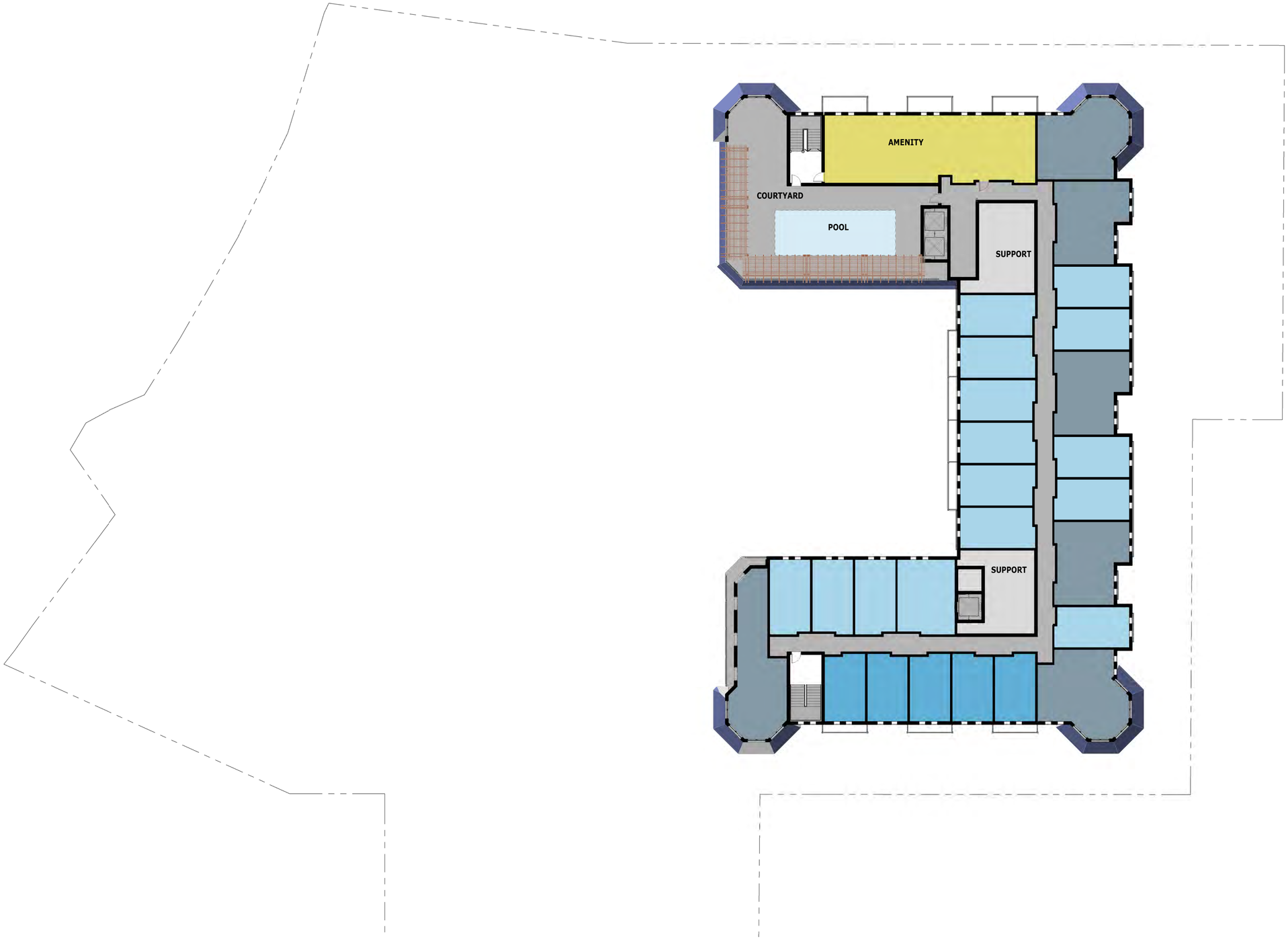
LEVEL 04 - OVERALL FLOOR PLAN

N

SCALE: 1/16" = 1'

0 8 16 40 64'

NOT FOR CONSTRUCTION



LEVEL 05 - OVERALL FLOOR PLAN

N

SCALE: 1/16" = 1'

0 8 16 40 64'

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

A.105

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427

2 NORTH / FRONT ELEVATION
1/16" = 1'-0"

1 SOUTH / BACK ELEVATION
1/16" = 1'-0"

BUILDING ELEVATIONS

N

SCALE: 1/16" = 1'
0 8 16 40 64'

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

A.201

Baker Barrios
The shape of things to come.
AA000298 | LC26000427

2 EAST / 17-92 STREET ELEVATION
1/16" = 1'-0"

1 WEST / LAKE ELEVATION
1/16" = 1'-0"

BUILDING ELEVATIONS

N

SCALE: 1/16" = 1'
0 8 16 40 64'

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

A.202

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427

2 **NORTH EAST CORNER (MILLS + BEACHVIEW)**
PERSPECTIVE

1 **SOUTH EAST PERSPECTIVE (MILLS + FAIRVIEW)**
PERSPECTIVE

3D PERSPECTIVES

N

SCALE: 1" = 30'

0 15 30 75 120'

NOT FOR CONSTRUCTION

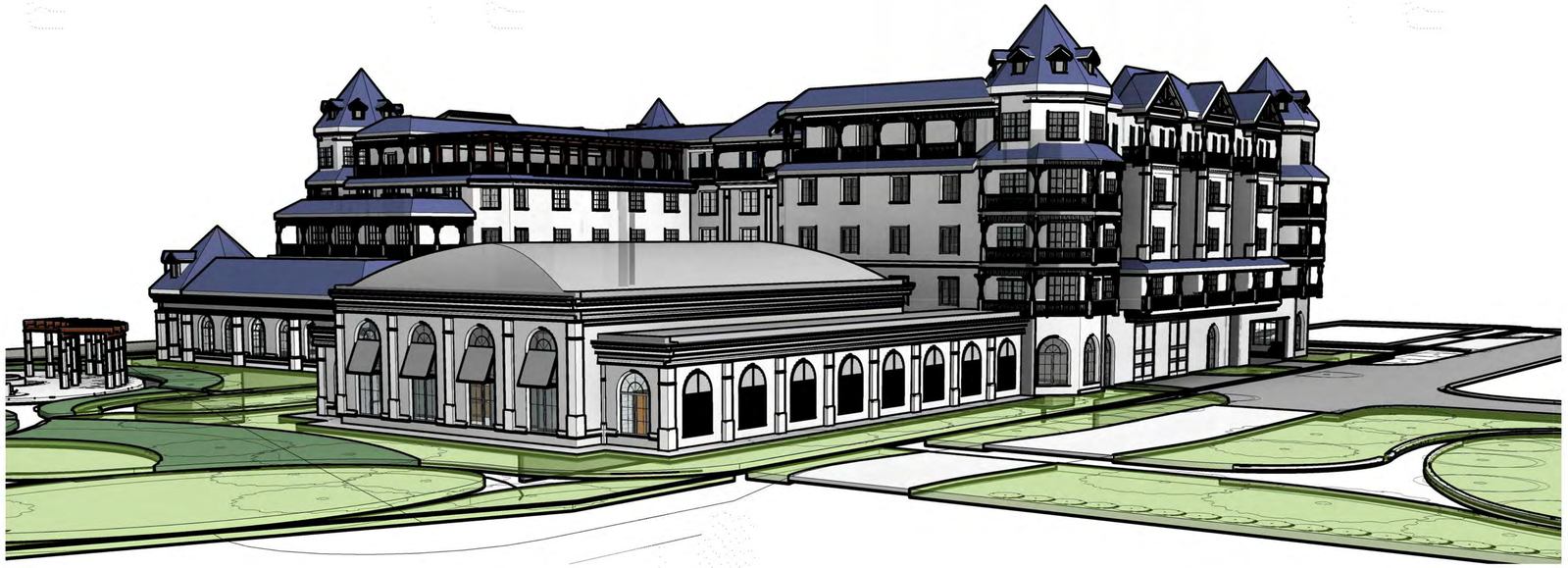
ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

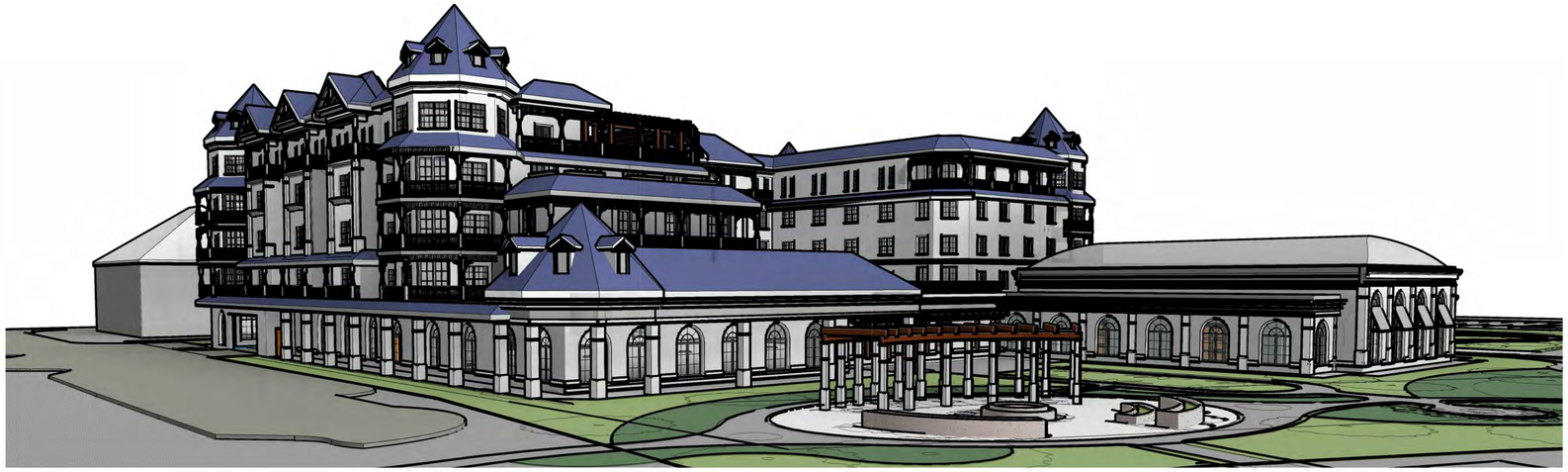
HENDERSON HOTEL
11.06.2020

A.301

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427



2 **SOUTH WEST CORNER PERSPECTIVE**



1 **NORTH WEST CORNER PERSPECTIVE**

3D PERSPECTIVES

N

SCALE: 1" = 30'

0 15 30 75 120'

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

A.302

Baker Barrios
The shape of things to come.

AA0002981 + LC26000427

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

HENDERSON HOTEL
11.06.2020

A.402

Baker Barrios
The shape of things to come.
AA000298 | LC26000427

SOUTH EAST PERSPECTIVE

Baker Barrios
AA0002981 + LC26000427

NOT FOR CONSTRUCTION

ATRIUM MANAGEMENT COMPANY

CONDITIONAL USE PACKAGE
PROJ. #180264.00

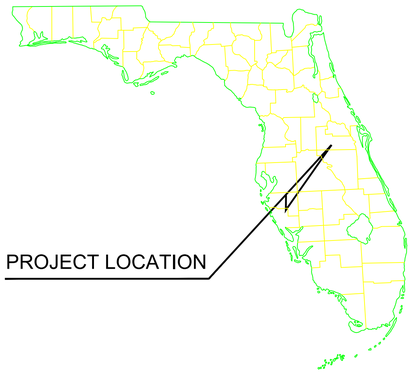
HENDERSON HOTEL
11.06.2020

A.403

Baker Barrios
The shape of things to come.
AA0002981 + LC26000427

NOT FOR CONSTRUCTION

PARK RENDER
Baker Barrios
NOT FOR CONSTRUCTION AA0000427



CONDITIONAL USE PLANS

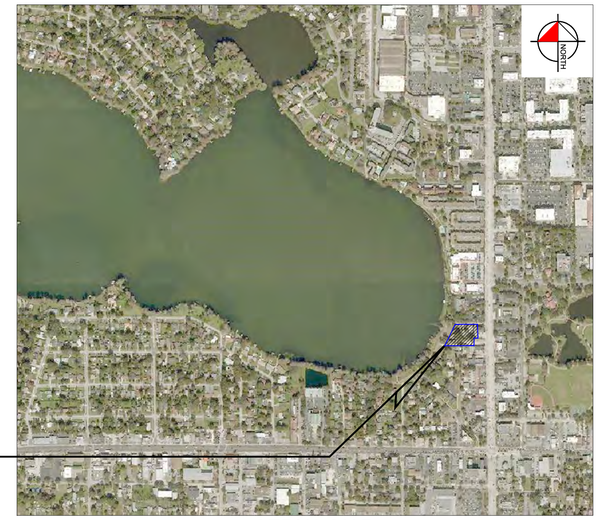
FOR

HENDERSON HOTEL

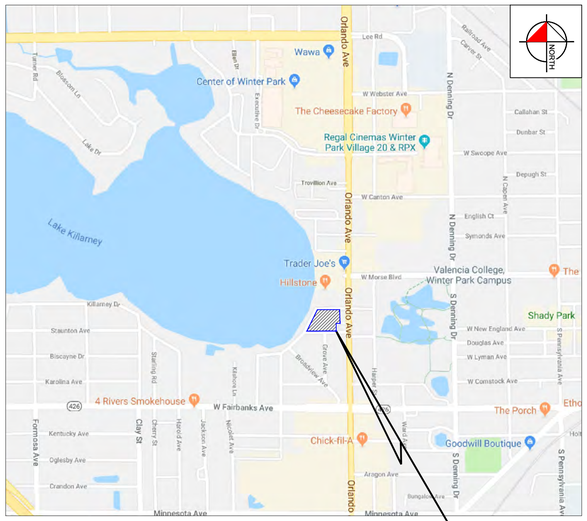
PARCEL #'S 12-22-29-4172-07-001
12-22-29-4172-07-011
12-22-29-4172-07-020
12-22-29-4172-07-030
12-22-29-4172-07-040
12-22-29-4172-07-060
12-22-29-4172-07-080
12-22-29-4172-05-110
12-22-29-4172-05-160
12-22-29-4172-05-170

CITY OF WINTER PARK, FLORIDA

NOVEMBER 5, 2020



AERIAL MAP
N.T.S.



VICINITY MAP
N.T.S.

SHEET INDEX

Sheet Number	Sheet Title
C0.0	COVER SHEET
C1.0	EXISTING CONDITIONS AND DEMOLITION PLAN
C2.0	SITE PLAN
C3.0	PAVING, GRADING, AND DRAINAGE PLAN
C4.0	UTILITY PLAN
C5.0	TRUCK ROUTE

LEGAL DESCRIPTION

LOTS 1-11, BLOCK 7, RESURVEY OF KILLARNEY ESTATES, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK L, PAGE 9, PUBLIC RECORD OF ORANGE COUNTY, FLORIDA, TOGETHER WITH THAT PARCEL DESCRIBED AS KILLARNEY OAKS BUILDING, A CONDOMINIUM, ACCORDING TO CONDOMINIUM BOOK 7, PAGE 53, PUBLIC RECORDS OF ORANGE COUNTY FLORIDA, AND TOGETHER WITH BEGINNING AT THE SOUTHWEST CORNER OF LOT 1, BLOCK 7, RESURVEY OF KILLARNEY ESTATES, ACCORDING TO THE PLAT THEREOF AS RECORDED IN PLAT BOOK L, PAGE 9, PUBLIC RECORDS OF ORANGE COUNTY, FLORIDA, THENCE RUN NORTHWESTERLY TO THE NORTHWEST CORNER OF NORTHEAST 1/4 OF SECTION 12, TOWNSHIP 29 SOUTH, RANGE 30 EAST; THENCE RUN SOUTHEASTERLY TO THE NORTHWESTERLY CORNER OF SAID LOT 4, BLOCK 7; THENCE SOUTHWESTERLY TO THE POINT OF BEGINNING, LESS THAT PART WITHIN THE RIGHT OF WAY AND LESS THAT PART FALLS IN LAKE KILLARNEY.

PROJECT TEAM

OWNER/DEVELOPER
ATRIUM MANAGEMENT COMPANY
3256 W. LAKE MARY BLVD, SUITE 1110
LAKE MARY, FL 32746
CONTACT: ADAM WONUS

SURVEYOR
VISIONLAND SERVICES
941 S. PENNSYLVANIA AVE,
WINTER PARK, FL 32789
PHONE: (888) 399-8474

ENGINEER
KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVE., SUITE 1000
ORLANDO, FL 32801
CONTACT: BROOKS A. STICKLER, P.E.
PHONE: (407) 898-1511
FAX: (407) 894-4791
EMAIL: brooks.stickler@kimley-horn.com

ARCHITECT
BAKER BARRIOS ARCHITECTS, INC.
189 S. ORANGE AVE., SUITE 1700
ORLANDO, FL 32801
PHONE: (407) 926-3000

PREPARED BY
Kimley»Horn

© 2020 KIMLEY-HORN AND ASSOCIATES, INC.

189 S. Orange Avenue, Suite 1000, Orlando, FL 32801
Phone: (407) 898-1511
WWW.KIMLEY-HORN.COM CA 00000696

BROOKS A. STICKLER, P.E.
FL. P.E. NO. 74031

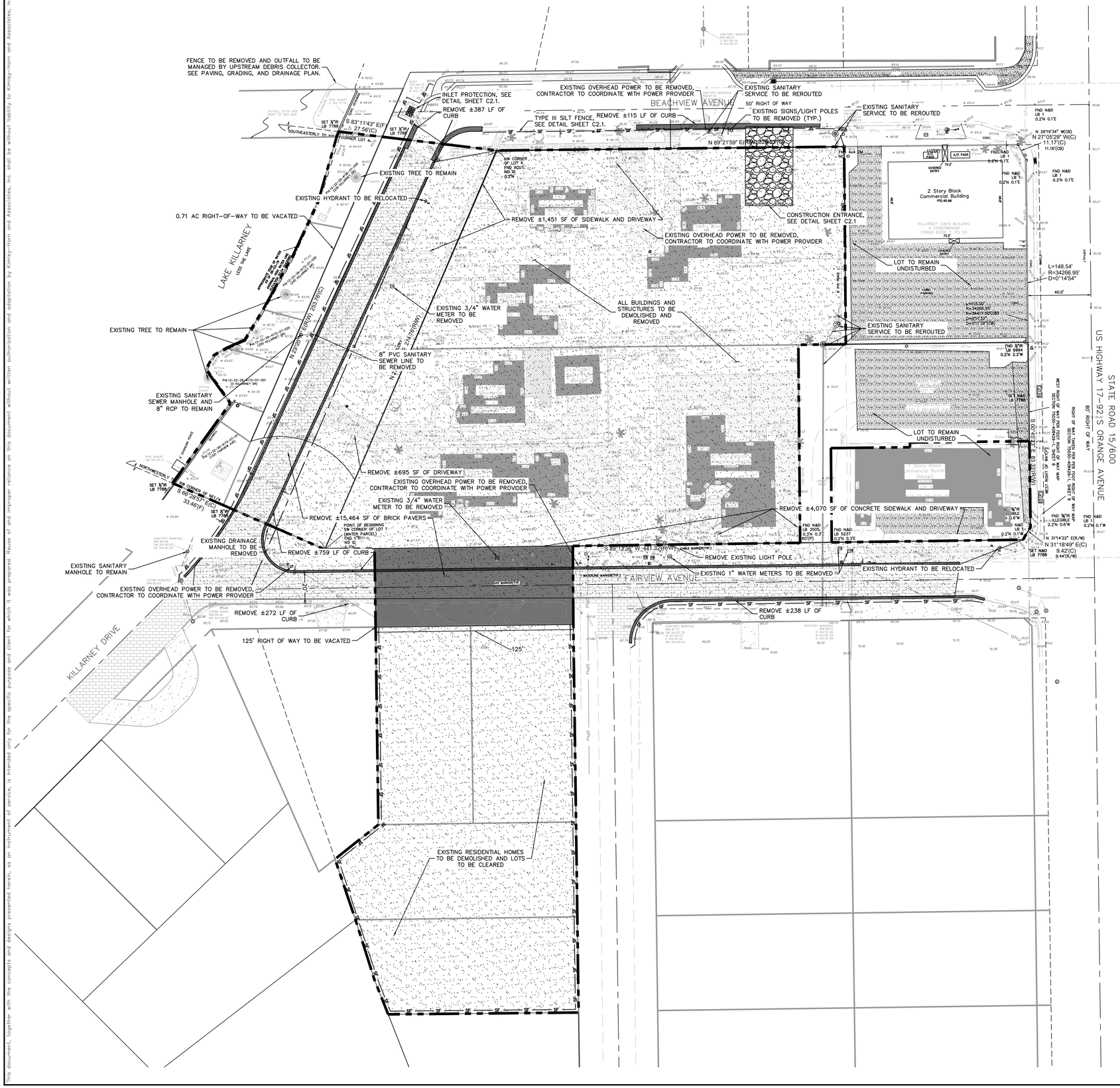
HENDERSON HOTEL
NOVEMBER 5, 2020
ENGINEER'S PROJECT No. 149309001

REVISIONS:

No.	DESCRIPTION	DATE	BY

Drawing name: K:\orl_civil\149309001-henderson hotel\CADD\conditional use\PlanSheets\C0.0 - COVER SHEET.dwg C0.0 COVER SHEET Nov 06, 2020 7:41am by: joshua.conradi

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



NORTH

GRAPHIC SCALE IN FEET
0 15 30 60

NOTES:

1. ALL WATER UTILITIES AND SANITARY UTILITIES LOCATED USING CITY OF WINTER PARK INFORMATION. CONTRACTOR TO FIELD LOCATE UTILITIES FOR EXACT LOCATIONS.
2. INSTALL AND MAINTAIN SILT FENCE AT LIMITS OF CONSTRUCTION. SEE DETAIL SHEET.
3. PROVIDE SOIL TRACKING PREVENTION DEVICE AT ALL CONSTRUCTION ACCESS POINTS. SEE DETAIL SHEET.
4. UTILIZE PERFORATED SOCK DRAIN (OR EQUIVALENT) IN FRONT OF EXISTING/PROPOSED CURB INLETS ADJACENT TO CONSTRUCTION ACTIVITIES. SEE DETAIL SHEET.
5. INSTALL AND MAINTAIN FILTER FABRIC UNDER GRATES OF EXISTING/PROPOSED INLETS, SEE SHEET.
6. ALL EXISTING ON-SITE INFRASTRUCTURE AND UTILITIES SHALL BE DEMOLISHED AND REMOVED COMPLETELY UNLESS OTHERWISE NOTED. CONTRACTOR SHALL COORDINATE WITH UTILITY PROVIDERS PRIOR TO DEMOLITION. THERE MAY BE ON-SITE UNDERGROUND UTILITIES (SUCH AS IRRIGATION, SANITARY SEWER, POTABLE WATER LINES, NATURAL GAS LINES, ELECTRIC, TELEPHONE AND CABLE LINES) THAT WERE NOT LOCATED OR IDENTIFIED BY THE PROJECT SURVEYOR. PRIOR TO CONSTRUCTION START, CONTRACTOR SHALL FIELD VERIFY ALL EXISTING ON-SITE UTILITIES.
7. CONTRACTOR TO VERIFY THAT THERE ARE NO EXISTING FLOWS CONTRIBUTING TO THE EXISTING ON-SITE SANITARY SYSTEM PRIOR TO REMOVAL.
8. ALL STREET BRICKS SHALL BE REMOVED, SALVAGED, AND PALLETIZED.
9. SELECTED AREAS TO BE OVERLAYED ARE INDICATED ON PAVING GRADING AND DRAINAGE PLANS.

CONSTRUCTION FENCE:

THE CONTRACTOR SHALL INSTALL A CONSTRUCTION FENCE AROUND THE PERIMETER OF THE SITE, ADJACENT TO THE SILT FENCE, AS NEEDED.

LEGEND

- PROPERTY LINE
- ◦ 60.00 EXISTING SPOT ELEVATION
- SF SF TYPE III SILT FENCE
- SOIL TRACKING PREVENTION DEVICE (SEE SHEET C2.1 FOR DETAIL)
- INLET PROTECTION (SEE SHEET C2.1 FOR DETAIL)
- AREA TO BE DEMOLISHED
- RIGHT-OF-WAY TO BE VACATED (0.43 AC)

Always call 811 two full business days before you dig to have underground utilities located and marked.
Sunshine811.com

No.	REVISIONS	DATE	BY

Kimley»Horn
© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
BROOKS A. STICKLER, P.E.
FLORIDA LICENSE NUMBER 74031

KHA PROJECT	149309001
DATE	11/05/2020
SCALE	AS SHOWN
DESIGNED BY	JAC
DRAWN BY	JAC
CHECKED BY	BAS

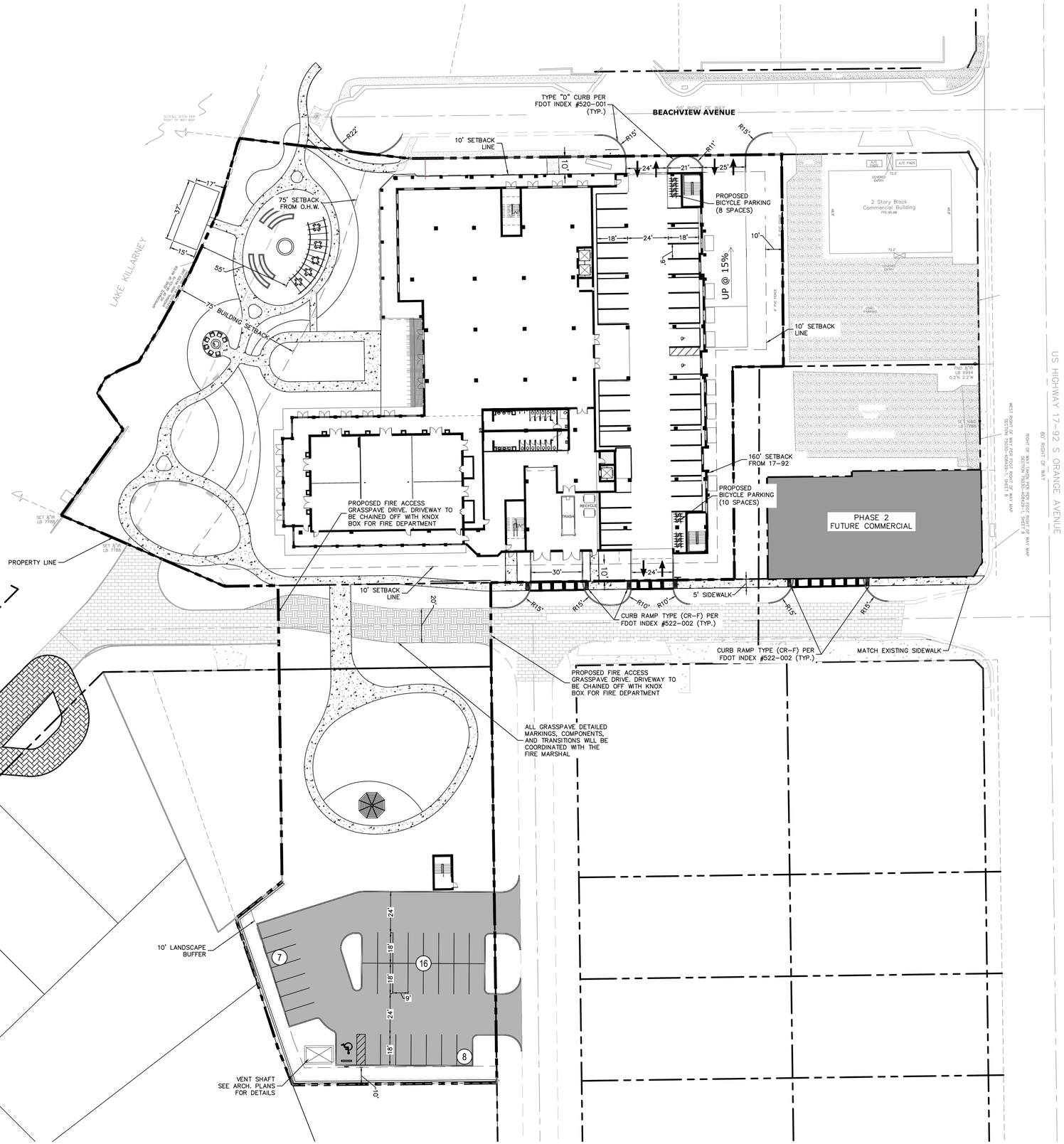
EXISTING CONDITIONS AND DEMOLITION PLAN

HENDERSON HOTEL

WINTER PARK FLORIDA

SHEET NUMBER
C1.0

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



SITE DATA: HENDERSON HOTEL

TOTAL AREA:	±2.97 ACRES
EXISTING ZONING:	C-3/O-1/R-1A
EXISTING USE:	COMMERCIAL, OFFICE, AND SINGLE FAMILY HOMES
PROPOSED LANDUSE:	HOTEL/PARK
PROPOSED BUILDING HEIGHT:	5 STORIES (55')
PROPOSED TOP OF PARAPET WALL HEIGHT:	65'
PROPOSED ARCHITECTURAL FEATURES HEIGHT:	73'
IMPERVIOUS AREA:	1.76 AC
PERVIOUS:	1.21 AC
PROPOSED ISR:	0.59
PROPOSED OPEN SPACE:	44%
PROPOSED FAR:	0.99
PROPOSED R.O.W TO BE VACATED:	0.56 ACRES
PROPOSED PARK AREA:	0.88 ACRES

BUILDING SETBACKS:	REQUIRED:	PROPOSED
EXISTING COMMERCIAL:	10 FT	10 FT
BEACHVIEW AVE.:	10 FT	10 FT
FAIRVIEW AVE.:	10 FT	15 FT
LAKESIDE:	75 FT	84 FT

REQUIRED PARKING:	
HOTEL ROOMS (132)	1 SPACE/ROOM
RESTAURANT (220 SEATS)	1SPACE/3 SEATS
BALLROOM (7500SF):	1 SPACE/250 SF
TOTAL REQUIRED:	235 SPACES
PROVIDED PARKING:	
STANDARD (9'X18'):	229 SPACES
ADA PARKING (12'X18'):	6 SPACES
TOTAL PROVIDED:	235 SPACES

REQUIRED BICYCLE PARKING:

1 SPACE/30 ROOMS:	132 ROOMS/30 =	5 SPACES
1 SPACE/50 EMPLOYEES:		1 SPACE
TOTAL REQUIRED:		6 SPACES

PROVIDED BICYCLE PARKING:
TOTAL PROVIDED: 18 SPACES

PHASING:
THIS PROJECT WILL BE CONSTRUCTED IN TWO (2) PHASES.

STORMWATER MANAGEMENT:
ALL STORMWATER RUNOFF ON-SITE WILL BE TREATED IN IN AN UNDERGROUND EXFILTRATION SYSTEM.

100 YEAR FLOODPLAIN:
100-YEAR FLOODPLAIN ELEVATION IS 84.4' PER FEMA PANEL #12095C0255F

SITE DATA: PHASE 2 COMMERCIAL

TOTAL AREA:	±0.18 ACRES
EXISTING ZONING:	C-3
EXISTING USE:	COMMERCIAL, HOTEL/MOTEL
PROPOSED LANDUSE:	COMMERCIAL

REQUIRED BUILDING SETBACKS:

S. ORANGE AVE. (US 17-92):	5 FT
EXISTING COMMERCIAL PROPERTY (SIDE):	5 FT
FAIRVIEW AVE.:	10 FT
EXISTING COMMERCIAL PROPERT (REAR):	30 FT

LEGEND

- PROPERTY LINE
- BUILDING SETBACK
- PROPOSED EASEMENT
- PROPOSED CONCRETE SIDEWALK
- PROPOSED BRICK ROAD
- PROPOSED PAVEMENT
- PROPOSED GRAVEPAVE FIRE ACCESS

Always call 811 two full business days before you dig to have underground utilities located and marked.
Sunshine811.com

No.	REVISIONS	DATE	BY

Kimley»Horn
© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
BROOKS A. STICKLER, P.E.
FLORIDA LICENSE NUMBER
74031
DATE: ----

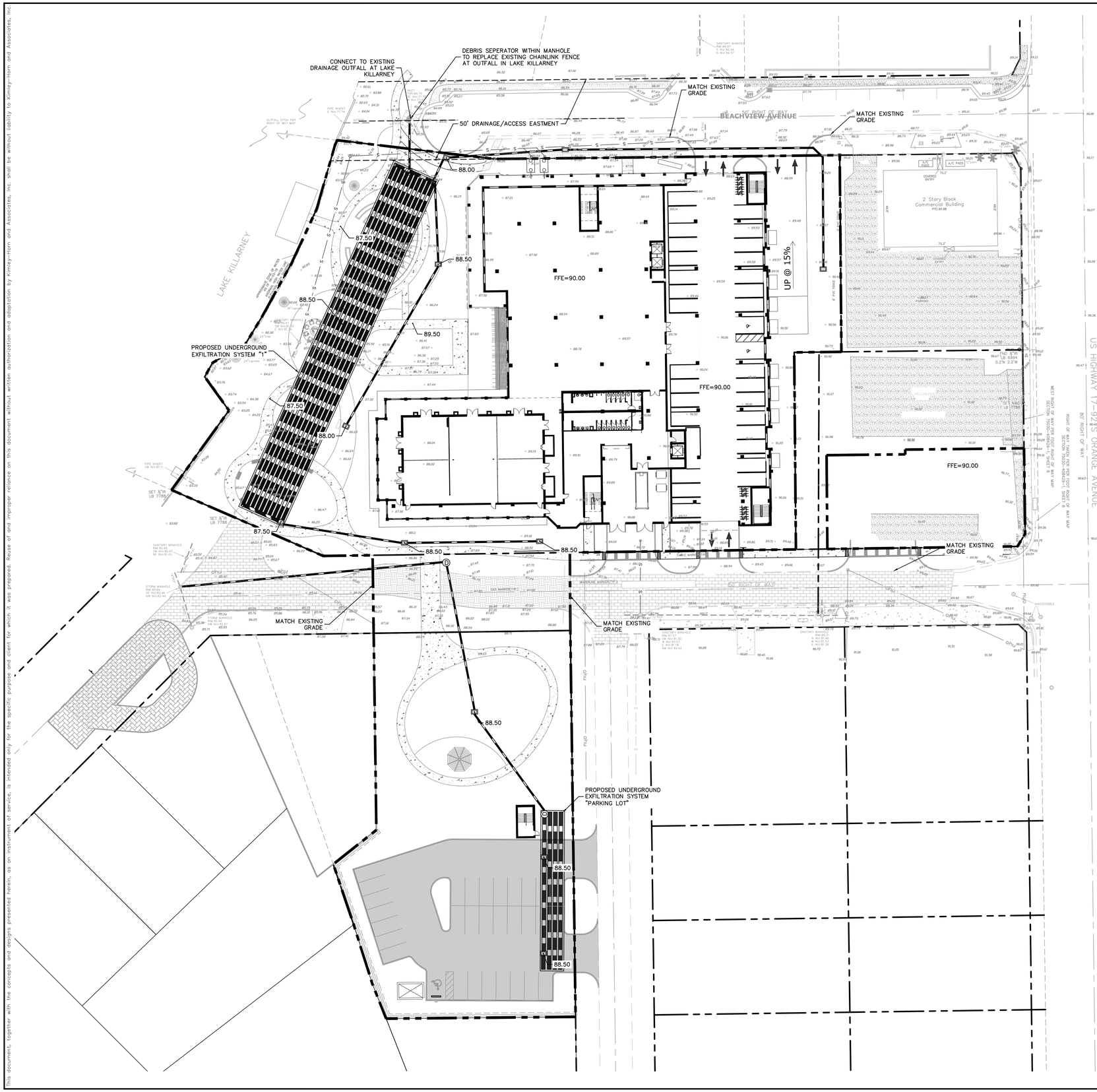
KHA PROJECT
149309001
DATE
11/05/2020
SCALE AS SHOWN
DESIGNED BY JAC
DRAWN BY JAC
CHECKED BY BAS

SITE PLAN

HENDERSON HOTEL
WINTER PARK FLORIDA

SHEET NUMBER
C2.0

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



GRAPHIC SCALE IN FEET
0 15 30 60

WET DETENTION PROPOSED EXFILTRATION 1

Littoral Zone Provided?	NO		
Discharge Directly to Class I, II, III or OFW?	NO		
Basin Area =		2.30	acres
Impervious Area (Excluding pond area) =		1.44	acres
TV = 1" of runoff over site = [(Basin Area)(1")]/12" =		0.19	acre-ft.
OR			
TV = 2.5" of runoff from imp. area = [(Imp Area)(2.5")]/12" =		0.30	acre-ft.

EXFILTRATION 1 STAGE STORAGE

	Stage (ft)	Area (sq.-ft.)	Volume (cu.-ft.)	Volume (ac.-ft.)
Trench Bottom	83.5	3,100.00	0.00	0.000
Bottom of Pipe	83.5	3,100.00	0.00	0.000
	83.7	4,898.56	865.06	0.020
	83.9	5,498.08	1,910.41	0.044
	84.1	5,847.35	3,048.06	0.070
	84.3	6,037.04	4,238.81	0.097
Middle of Pipe	84.5	6,097.60	5,454.31	0.125
	84.7	6,037.04	6,669.81	0.153
	84.9	5,847.35	7,860.56	0.180
	85.1	5,498.08	8,998.21	0.207
	85.3	4,898.56	10,043.56	0.231
Top of Pipe	85.5	3,100.00	10,908.62	0.250
Top of Trench	86.5	3,100.00	14,008.62	0.322

PROPOSED EXFILTRATION "PARKING LOT"

Littoral Zone Provided?	NO		
Discharge Directly to Class I, II, III or OFW?	NO		
Basin Area =		0.80	acres
Impervious Area (Excluding pond area) =		0.25	acres
TV = 1" of runoff over site = [(Basin Area)(1")]/12" =		0.07	acre-ft.
OR			
TV = 2.5" of runoff from imp. area = [(Imp Area)(2.5")]/12" =		0.05	acre-ft.

EXFILTRATION "PARKING LOT" STAGE STORAGE

	Stage (ft)	Area (sq.-ft.)	Volume (cu.-ft.)	Volume (ac.-ft.)
Trench Bottom	84.0	630.00	0.00	0.000
Bottom of Pipe	84	630.00	0.00	0.000
	84.3	989.55	218.74	0.005
	84.5	1,109.40	482.53	0.011
	84.8	1,179.22	769.38	0.018
	85.0	1,217.14	1,069.50	0.025
Middle of Pipe	85.3	1,229.25	1,375.81	0.032
	85.5	1,217.14	1,682.12	0.039
	85.8	1,179.22	1,982.24	0.046
	86.0	1,109.40	2,269.10	0.052
	86.3	989.55	2,532.89	0.058
Top of Pipe	86.5	630.00	2,751.62	0.063
Top of Trench	87.5	630.00	3,381.62	0.078

NOTES:

1. ALL PEDESTRIAN SIDEWALKS, PATHWAYS, AND CROSSWALKS SHALL BE CONSTRUCTED NOT TO EXCEED 2.0% CROSS SLOPE NOR 5% LONGITUDINALLY. GRADES IN ACCESSIBLE PARKING SPACES SHALL NOT EXCEED 2% IN ANY DIRECTION. IN CASES OF SIDEWALK LANDINGS AT BUILDING ENTRANCES, GRADES SHALL NOT EXCEED 2% IN ANY DIRECTION. ACCESSIBLE CURB RAMPS SHALL NOT EXCEED 6" IN LENGTH AND 1:12 SLOPE. LANDINGS AT CHANGES IN DIRECTION SHALL BE MINIMUM 60"x60" AND SHALL NOT EXCEED 2% SLOPE IN ANY DIRECTION
2. GRADES TO MATCH EXISTING ELEVATIONS AT PROPERTY LINE UNLESS OTHERWISE NOTED.
3. 100-YEAR FLOODPLAIN ELEVATION IS 84.4' PER FEMA PANEL #12095C0255F

LEGEND

XX.XX	PROPOSED SPOT ELEVATION
	PROPOSED STORM PIPE
	PROPOSED STORM STRUCTURE
D	PROPOSED STORM MANHOLE
◦ 60.00	EXISTING SPOT ELEVATION
	PROPOSED PAVEMENT
	PROPOSED BRICK ROAD
	PROPOSED DRAINAGE/ACCESS EASMENT

Always call 811 two full business days before you dig to have underground utilities located and marked.

Sunshine811.com

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2019 KIMLEY-HORN AND ASSOCIATES, INC.

189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
BROOKS A. STICKLER, P.E.
FLORIDA LICENSE NUMBER
74031
DATE: - - - -

KHA PROJECT
149309001
DATE
11/05/2020
SCALE AS SHOWN
DESIGNED BY JAC
DRAWN BY JAC
CHECKED BY BAS

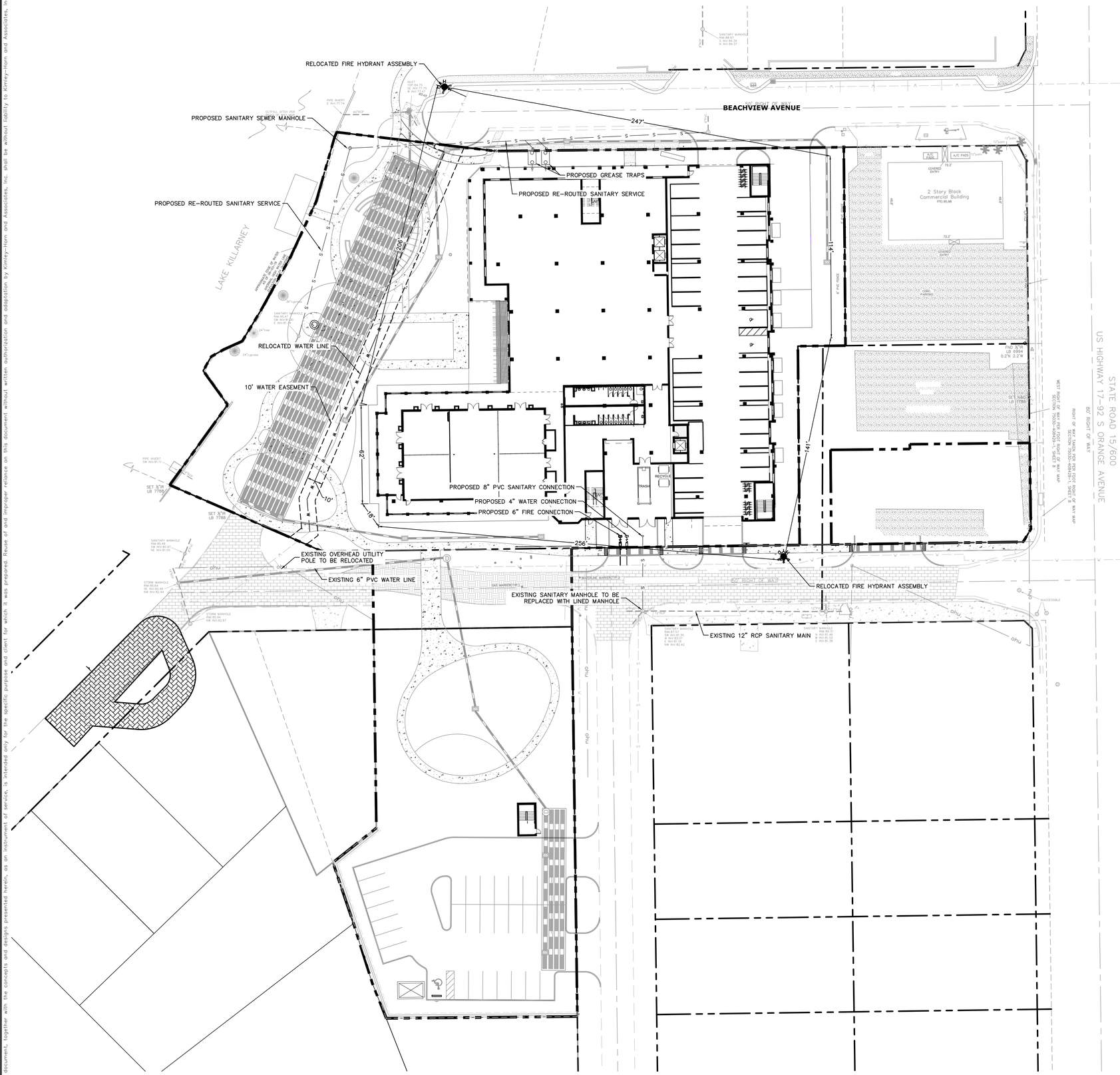
PAVING, GRADING, AND DRAINAGE PLAN

HENDERSON HOTEL

WINTER PARK FLORIDA

SHEET NUMBER
C3.0

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



NORTH

GRAPHIC SCALE IN FEET
0 15 30 60

NOTE:

1. ALL WATER MAINS ARE TO MEET DEP REQUIRED CLEARANCES AT CROSSINGS OF SANITARY AND STORM SEWER.
2. ALL WATER MAINS TO BE INSTALLED WITH MINIMUM 3' OF COVER FROM PROPOSED GRADES.
3. WATER MAIN OVER STORM/SANITARY WHERE POSSIBLE BUT STILL NEEDS TO HAVE MINIMUM 3' COVER.
4. ALL PRIVATE COMBINATION FIRE & WATER MAINS SHALL MEET REQUIREMENTS OF NFPA 24, 2013 EDITION & 6TH EDITION OF THE FLORIDA FIRE PREVENTION CODE. CONTRACTOR TO COORDINATE WITH THE CITY OF WINTER PARK FOR INSPECTION AND PERMITTING OF COMBINATION MAINS.
5. CONTRACTOR TO FOLLOW ALL SITE REQUIREMENTS OUTLINED IN NFPA 1-16.1 AND NFPA 241

CITY OF WINTER PARK NOTES:

CONTINUOUS SERVICE OF CITY OF WINTER WATER AND WASTEWATER UTILITIES IS REQUIRED. CONTRACTOR SHALL COORDINATE WITH CITY OF WINTER PARK UTILITIES INSPECTOR PRIOR TO PRE-APPROVED INTERRUPTIONS 7 DAYS IN ADVANCE. CITY OF WINTER PARK UTILITIES PERSONNEL SHALL OPERATE ALL CITY-OWNED VALVES AND EQUIPMENT. PROVIDE WRITTEN PROCEDURE FOR DISINFECTION AND TESTING FOR APPROVAL.

CONNECTIONS:

THE EXISTING WATER MAINS SHALL REMAIN ACTIVE DURING CONSTRUCTION AND SHALL NOT BE TAKEN OUT OF SERVICE UNTIL THE NEWLY CONSTRUCTED LINES ARE INSTALLED AND CLEARED BY DEP.

AT CONNECTION POINTS, CONTRACTOR SHALL COORDINATE WITH LOCAL UTILITY ON CONNECTIONS VIA WET TAPS AND LINESTOPS AND/OR TEMPORARY JUMPERS FOR INSTALLATION AND CONNECTION OF NEW WATER MAINS AND FORCEMAINS.

LEGEND

- PROPOSED WATER PIPE
- PROPOSED FIRE PIPE (BY OTHERS)
- PROPOSED HYDRANT
- PROPOSED SANITARY SEWER PIPE
- PROPOSED SANITARY SEWER MANHOLE
- PROPOSED STORM PIPE
- PROPOSED STORM MANHOLE
- PROPOSED STORM INLET
- EXISTING WATER PIPE
- EXISTING SANITARY SEWER PIPE
- PROPERTY LINE
- RIGHT OF WAY LINE
- PROPOSED WATER EASEMENT
- PROPOSED GREASE TRAP

Always call 811 two full business days before you dig to have underground utilities located and marked.

Sunshine811.com

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2019 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
BROOKS A. STICKLER, P.E.
FLORIDA LICENSE NUMBER
74031
DATE: ----

KHA PROJECT
149309001
DATE
11/05/2020
SCALE AS SHOWN
DESIGNED BY JAC
DRAWN BY JAC
CHECKED BY BAS

UTILITY PLAN

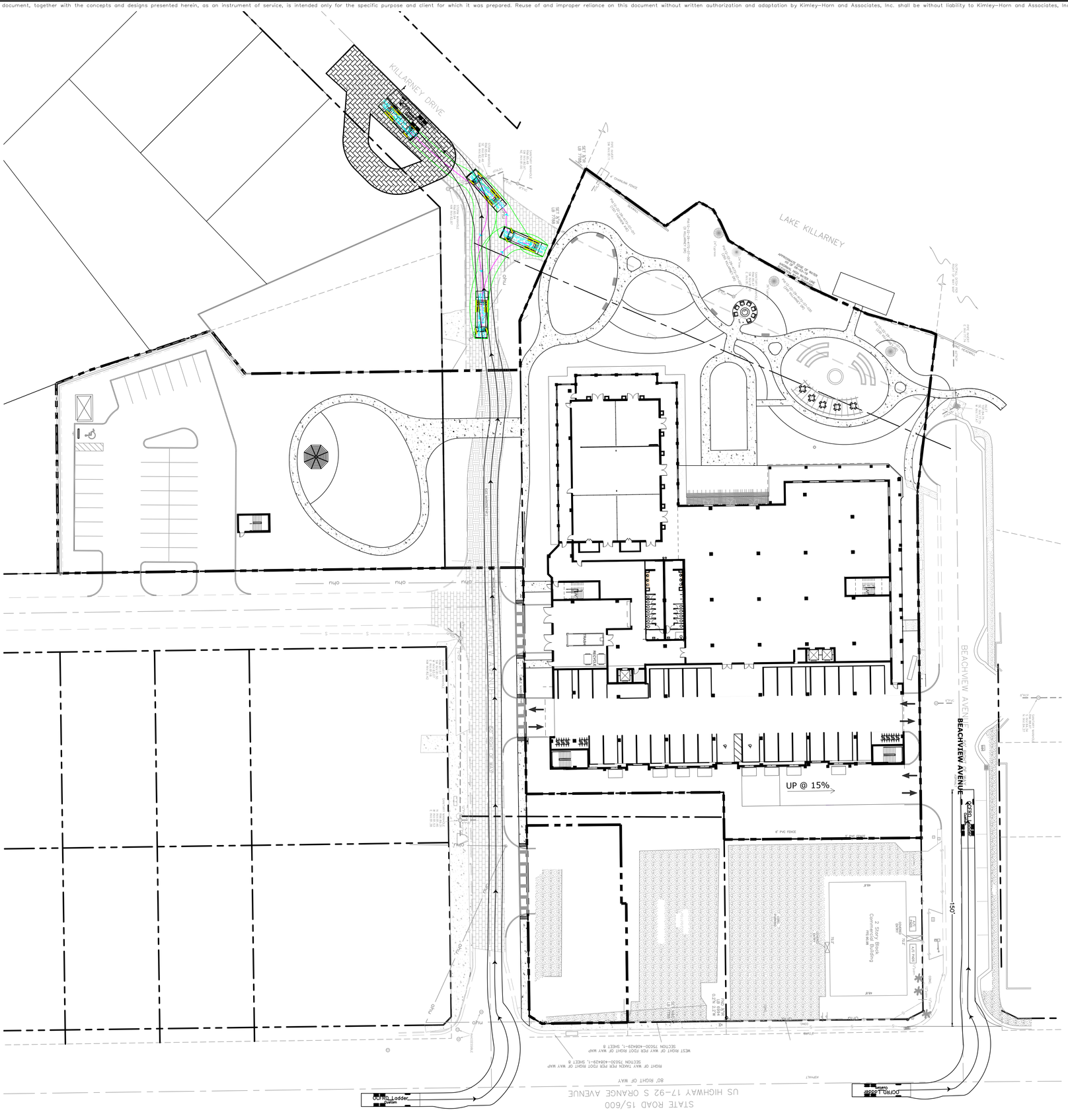
HENDERSON HOTEL

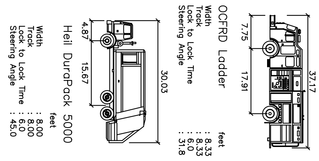
WINTER PARK FLORIDA

SHEET NUMBER
C4.0

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.





GRAPHIC SCALE IN FEET
0 15 30 60

NORTH

Always call 811 two full business days before you dig to have underground utilities located and marked.
sunshine811.com

SHEET NUMBER C5.0	HENDERSON HOTEL WINTER PARK FLORIDA	TRUCK ROUTE	KHA PROJECT 149309001 DATE 11/05/2020 SCALE AS SHOWN DESIGNED BY JAC DRAWN BY JAC CHECKED BY BAS	LICENSED PROFESSIONAL BROOKS A. STICKLER, P.E. FLORIDA LICENSE NUMBER 74031 DATE: ----	Kimley»Horn © 2019 KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-898-1511 WWW.KIMLEY-HORN.COM CA 00000696

No.	REVISIONS	DATE	BY

HENDERSON HOTEL

Traffic Impact Analysis

November 2020

Kimley»Horn

Table of Contents

1.0 INTRODUCTION 1

1.1 Study Area 1

2.0 EXISTING CONDITIONS ANALYSIS 3

2.1 Existing Traffic Counts 3

2.2 Existing Intersection Conditions 4

3.0 PROJECT DEVELOPMENT 5

3.1 Site Access 5

3.2 Trip Generation 5

3.3 Trip Distribution 7

3.4 Trip Assignment 9

4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2021 10

4.1 Background Traffic 10

4.2 Background Intersection Analysis 11

5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2021 12

5.1 Buildout Traffic 12

5.2 Buildout Intersection Analysis 13

6.0 CONCLUSION 14

Figures

Figure 1: Project Location and Study Area Intersections..... 2
Figure 2: Existing Intersection Volumes 3
Figure 3: Trip Distribution 8
Figure 4: Trip Assignment 9
Figure 5: Background Intersection Volumes..... 10
Figure 6: Buildout Intersection Volumes..... 12

Tables

Table 1: Existing Intersection Conditions (PM Peak Hour) 4
Table 2: Trip Generation..... 6
Table 3: Background Intersection Conditions (PM Peak Hour) 11
Table 4: Buildout Intersection Conditions (PM Peak Hour) 13

Appendices

Appendix A: Conceptual Site Plan
Appendix B: Turning Movement Counts
Appendix C: FDOT's Florida Traffic Online (FTO) Data
Appendix D: Turning Movement Volume Worksheets
Appendix E: Synchro Outputs
Appendix F: Killarney Estates Neighborhood Traffic Study
Appendix G: CFRPM Model Plot
Appendix H: Signal Timings

1.0 INTRODUCTION

Kimley-Horn has been retained by Winter Park Historic Hotel to analyze and document the traffic impacts associated with the development of Henderson Hotel, a proposed site redevelopment of several parcels generally located north of Fairview Avenue, south of Beachview Avenue, and west of US 17-92/Orlando Avenue in the City of Winter Park, Florida.

Current zoning of the parcels to be redeveloped include Commercial (C-3), Single Family Residential (R-1A) and Office (O-1). Proposed zoning is C-3 on all parcels. The site is composed of ten (10) separate parcels (Parcel ID #12-22-29-4172-07-001, 12-22-29-4172-07-011, 12-22-29-4172-07-020, 12-22-29-4172-07-030, 12-22-29-4172-07-040, 12-22-29-4172-07-060, 12-22-29-4172-07-080, 12-22-29-4172-05-110, 12-22-29-4172-05-160, and 12-22-29-4172-05-170). Project location is shown in Figure 1.

Based on 2020 property records from the Orange County Property Appraiser, the site is currently occupied by seven (7) single family homes and 4,920 square feet of office space. The applicant is proposing to redevelop the site to consist of 132 hotel rooms with structured parking onsite. The existing office and motel along S Orlando Avenue are planned to remain. The total area of the site being redeveloped is approximately 2.97 acres. The current conceptual site plan is included as Appendix A.

1.1 STUDY AREA

The study area includes the following offsite intersections as shown in Figure 1:

- US 17-92/Orlando Avenue & Beachview Avenue
- US 17-92/Orlando Avenue & Fairview Avenue
- US 17-92/Orlando Avenue & W Fairbanks Avenue

Figure 1 - Project Location Map & Study Area Intersections

Henderson Hotel | Traffic Impact Analysis

Kimley»Horn
© 2020 Kimley-Horn and Associates, Inc.
189 S Orange Ave, Suite 1000, Orlando FL 32801
Phone: (407) 898-1511

Date: November 2020

0 35 70 140 210 Feet

2.0 EXISTING CONDITIONS ANALYSIS

2.1 EXISTING TRAFFIC COUNTS

Turning movement counts (TMCs) were collected at the study intersections on Tuesday, May 21, 2019 during the PM (4:00PM – 6:00PM) peak period. Raw TMCs are provided in Appendix B.

Traffic counts were adjusted using the seasonal factor (SF) from FDOT's Florida Traffic Online (FTO) publication. Seasonal factor data is included in Appendix C. Figure 2 illustrates turning movement volumes for existing conditions at the study intersections. Adjusted turning movement volume worksheets for all intersections can be found in Appendix D.

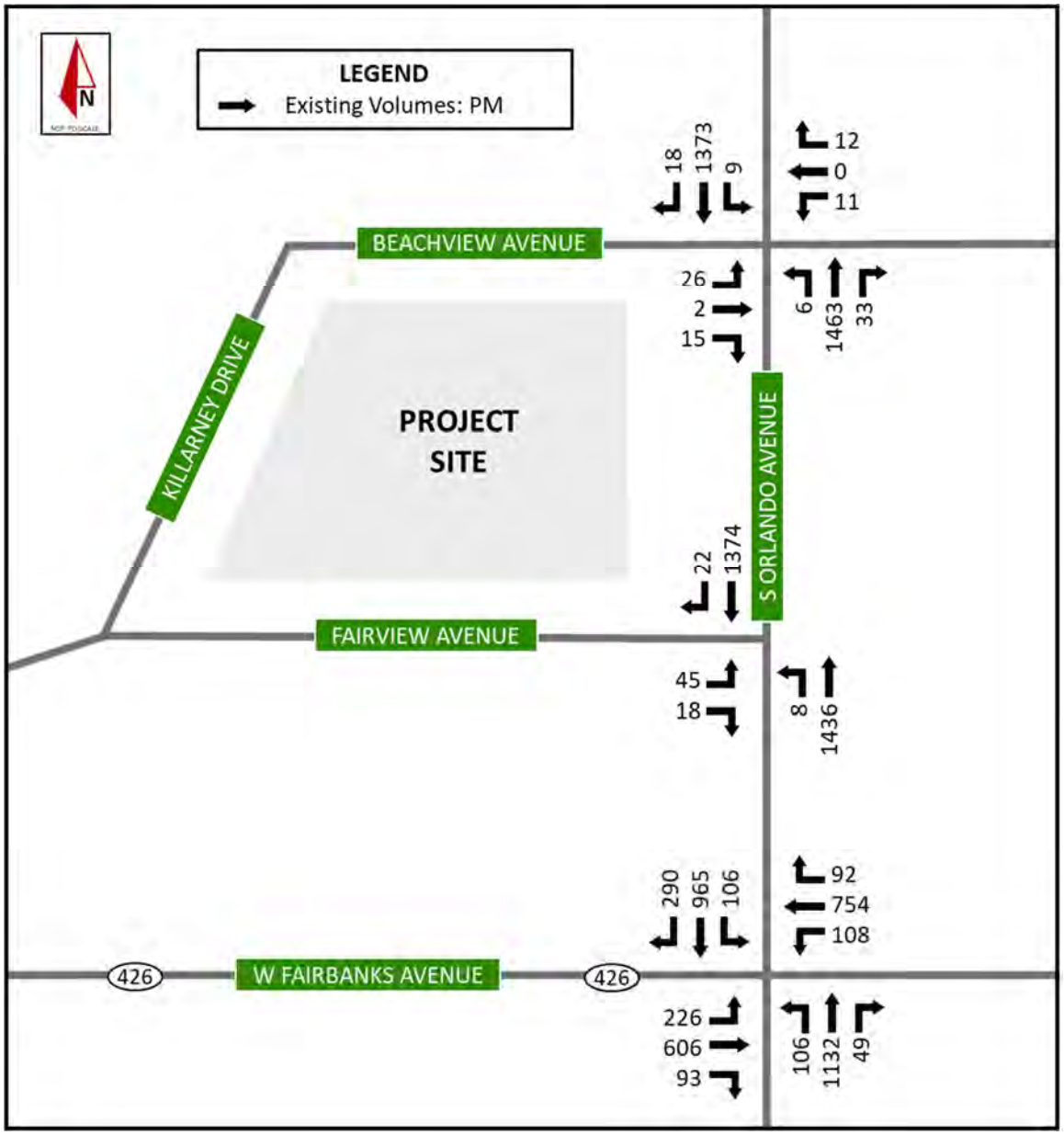


Figure 2: Existing Intersection Volumes

2.2 EXISTING INTERSECTION CONDITIONS

Intersection capacity analysis was performed for existing (2019) conditions using the operational analysis procedures outlined in the *Highway Capacity Manual 2010*. Specifically, *Synchro* (v10) software was used to evaluate existing operational conditions at study area intersections by reporting volume to capacity (v/c) ratios, delay, and queue length demands. Intersection level of service (LOS) and maximum volume to capacity (v/c) ratios for the existing conditions are provided in Table 1. Synchro outputs are provided in Appendix E and existing signal timings are provided in Appendix H.

Table 1: Existing Intersection Conditions (PM Peak Hour)

Existing Condition - 2019					
Intersection	Control Type	Approach	PM Peak		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
US 17-92 (Orlando Avenue) & Beachview Avenue	Unsignalized (TWSC)	EB	E	EBL	0.40
		WB	E	WBL	0.22
		NB (L)	B	NBL	0.01
		SB (L)	B	SBL	0.02
		Overall	-	EBL	0.40
US 17-92 (Orlando Avenue) & Fairview Avenue	Unsignalized (TWSC)	EB	E	EBL	0.51
		WB	-	-	-
		NB (L)	B	NBL	0.02
		SB (L)	-	-	-
		Overall	-	EBL	0.51
US 17-92 (Orlando Avenue) & W Fairbanks Avenue	Signalized	EB	F	EBL	0.89
		WB	F	WBT	1.08
		NB	E	NBT	0.88
		SB	E	SBL	0.74
		Overall	F *(84.8 sec)*	EBL	1.08

All study area intersections are shown to operate at an acceptable LOS with v/c ratios less than 1.0 in the existing PM peak hour conditions with the exception of US 17-92/Orlando Avenue and W Fairbanks Avenue, which currently operates with LOS 'F'. As provided in the Synchro outputs, the signalized intersection also shows all left turn movements operating at LOS 'F' with peak hour queue lengths in the eastbound, westbound, and northbound left turn lanes exceeding storage capacity.

3.0 PROJECT DEVELOPMENT

The applicant is proposing to redevelop the site to consist of 132 hotel rooms and amenity uses ancillary to the hotel use. The existing office and hotel along S Orlando Avenue are to remain. The total area of the site being redeveloped is approximately 2.97 acres. The latest industry standards were referenced to evaluate the amount of new external trips to be generated by the site at buildout.

3.1 SITE ACCESS

Access to the site is proposed via two (2) driveways (one at Fairview Avenue and one at Beachview Avenue) as shown in the site plan. It is anticipated that the existing segment of Killarney Drive between Beachview Avenue and Fairview Avenue will be vacated.

Additionally, connectivity to Killarney Drive south of the project will be vacated. This design is anticipated to reduce existing and future cut-through traffic through the adjacent Killarney Estates neighborhood. Existing cut-through traffic and proposed solutions were studied in conjunction with neighborhood residents' concerns with the Killarney Estates Neighborhood Cut-through Traffic Study. The study recommended vacating connectivity to Killarney Drive as a solution to reducing cut-through traffic through Killarney Estates. The Killarney Estates Neighborhood Cut-through Traffic Study is located in Appendix F.

3.2 TRIP GENERATION

Trip generation rates for the existing and proposed development scenarios were calculated using the 10th Edition of the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*. Land Use Codes (LUCs) used for the existing and proposed development scenarios include:

Existing

- LUC 210 – Single Family Detached Housing
- LUC 710 – General Office Building

Proposed

- LUC 310 – Hotel

Per City code requirement, Table 2 provides the Daily and PM peak hour trip generation summary for the existing and proposed development scenarios to show the additional vehicle trips anticipated to be added to the roadway network by the proposed redevelopment.

As shown in Table 2, the proposed redevelopment of the site is anticipated to generate 917 additional daily trips and 59 additional PM peak hour trips (31 inbound and 28 outbound) to the external roadway network at buildout.

Table 2: Trip Generation

PROPOSED REDEVELOPMENT																
Land Use	ITE LUC	Size	Units	Daily ITE Trip Rate[1]	PM ITE Trip Rate[1]	Daily Trip Generation					PM Peak Hour Trip Generation					
						Total	In[1]		Out[1]		Total	In[1]		Out[1]		
Hotel	310	132	Rooms	8.06	0.55	1,064	50%	532	50%	532	73	51%	37	49%	36	
New External Trips						1,064	532		532		73	37		36		
EXISTING DEVELOPMENT																
Land Use	ITE LUC	Size	Units	Daily ITE Trip Rate[1]	PM ITE Trip Rate[1]	Daily Trip Generation					PM Peak Hour Trip Generation					
						Total	In[1]		Out[1]		Total	In[1]		Out[1]		
Single Family Detached Housing	210	7	DU	12.86	1.13	90	50%	45	50%	45	8	63%	5	37%	3	
Office	710	4.92	KSF	11.61	1.32	57	50%	29	50%	28	6	16%	1	84%	5	
Total Generated Trips						147	74		73		14	6		8		
ADDITIONAL NEW EXTERNAL TRIPS																
						Daily Trip Generation					PM Peak Hour Trip Generation					
						Total	In[1]		Out[1]		Total	In[1]		Out[1]		
Net New External Trips						917	458		459		59	31		28		

[1] Vehicle trip rates and directional splits per data and procedures outlined in ITE Trip Generation Manual, 10th Edition

3.3 TRIP DISTRIBUTION

Projected traffic demand of project trips on study roadways was derived with use of the latest adopted regional travel demand model. Land use data for the project was entered into a new traffic analysis zone (TAZ) within the Central Florida Regional Planning Model (CFRPM v6) model set and situated within the existing roadway network to appropriately represent project access. The model was used to assign trips for all trip purposes between allocated origin and destination pairs using project build-out year model data. Trip distribution for the project was extracted from the completed model assignment and reviewed for logic. The resulting model plot showing percent of daily project distribution is provided in Appendix G.

Figure 3 displays the anticipated trip distribution for Henderson Hotel at buildout. Trip distribution percentages were used to assign new external project trips to the roadway network, as shown in Figure 4.

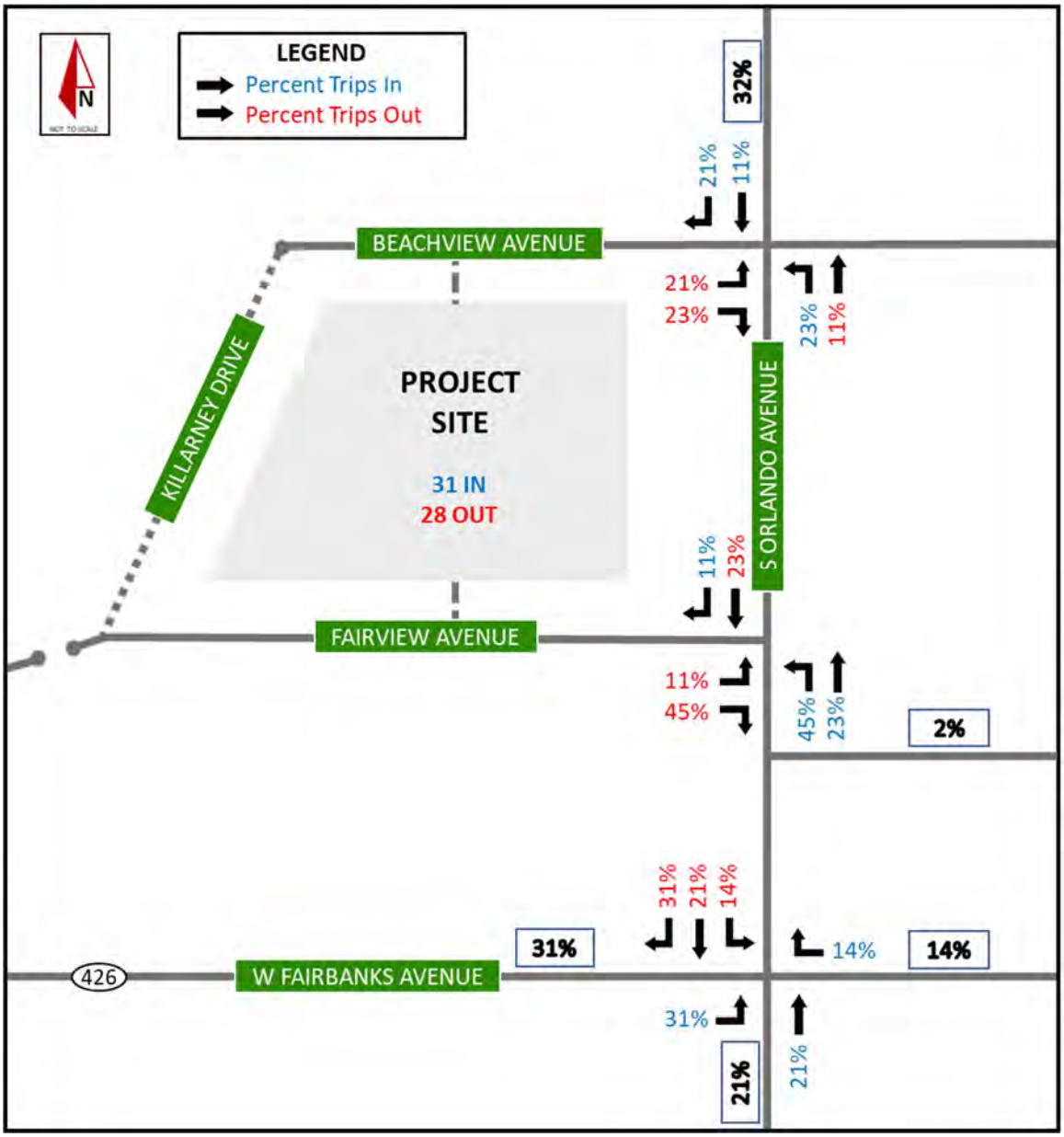


Figure 3: Trip Distribution

3.4 TRIP ASSIGNMENT

Site distribution percentages were used to assign anticipated project trips to the study area intersections and driveways. Figure 4 shows the anticipated PM peak hour project movements at study area intersections and driveways.

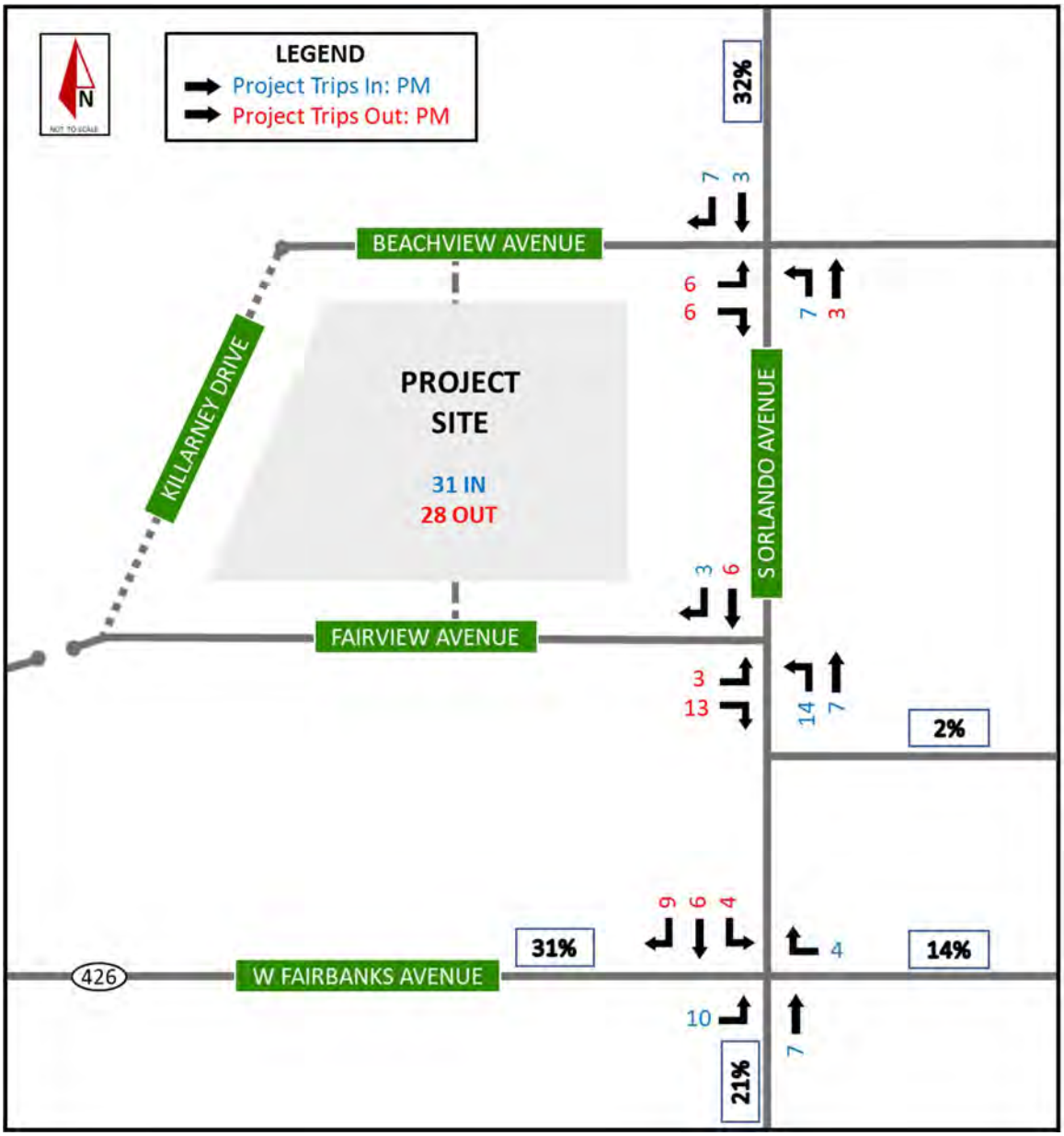


Figure 4: Trip Assignment

4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2021

4.1 BACKGROUND TRAFFIC

Traffic conditions were evaluated for year 2021 background conditions. Background volumes on study area intersections were derived by applying 2% annual growth to existing traffic counts. Figure 5 illustrates turning movement volumes for background conditions at the study intersections. Adjusted turning movement volume worksheets for all intersections can be found in Appendix D.

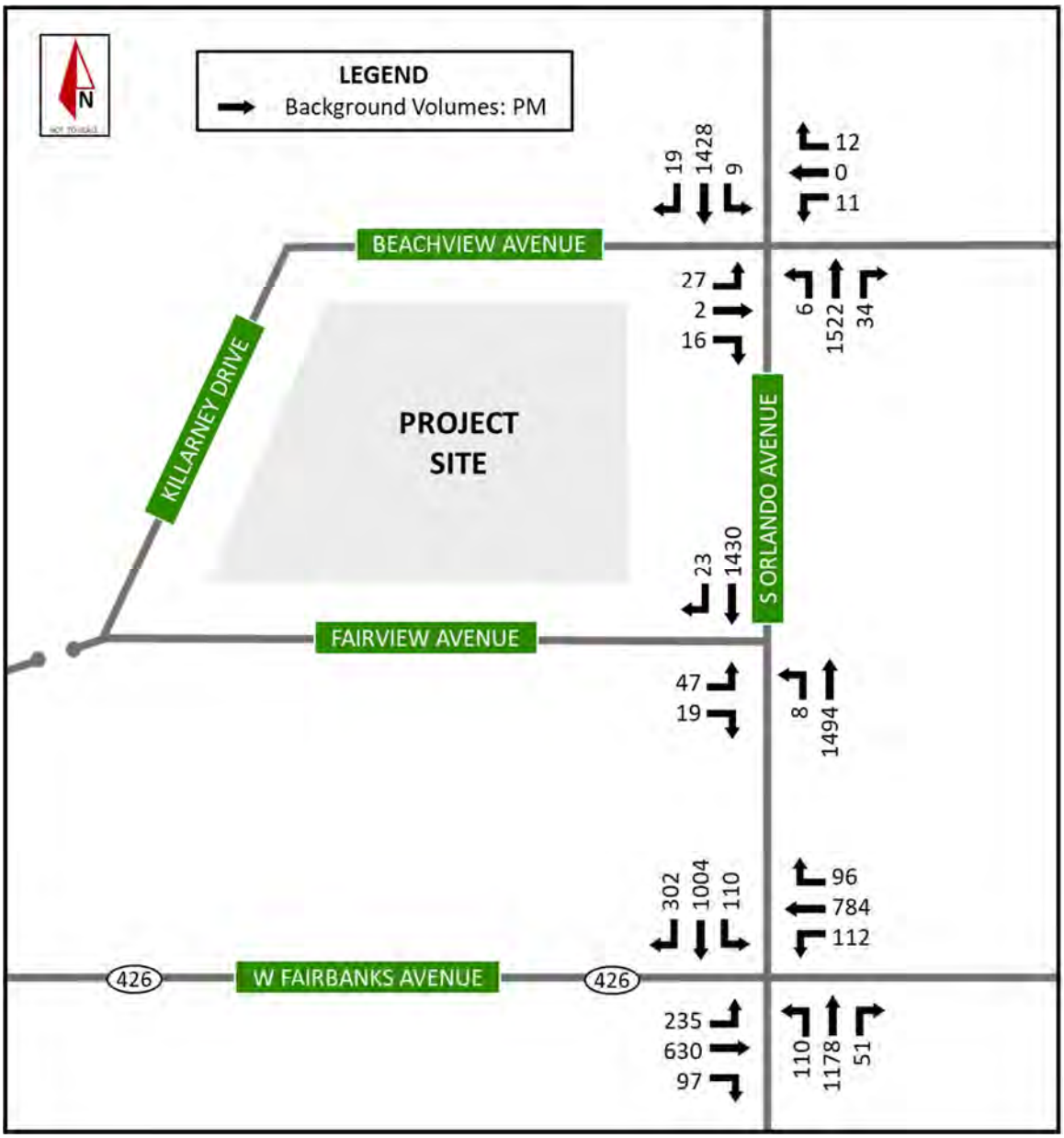


Figure 5: Background Intersection Volumes

4.2 BACKGROUND INTERSECTION ANALYSIS

Intersection operational analysis was performed for 2021 background conditions in the PM peak hour using procedures outlined in the *Highway Capacity Manual 2010* with *Synchro* (v10) software. Intersection level of service (LOS) and maximum volume to capacity (v/c) ratios for the background conditions are provided in Table 3. Synchro outputs are provided in Appendix E and existing signal timings are provided in Appendix H.

Table 3: Background Intersection Conditions (PM Peak Hour)

Background Condition - 2021					
Intersection	Control Type	Approach	PM Peak		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
US 17-92 (Orlando Avenue) & Beachview Avenue	Unsignalized (TWSC)	EB	F	EBL	0.44
		WB	E	WBL	0.24
		NB (L)	B	NBL	0.01
		SB (L)	B	SBL	0.02
		Overall	-	EBL	0.44
US 17-92 (Orlando Avenue) & Fairview Avenue	Unsignalized (TWSC)	EB	F	EBL	0.56
		WB	-	-	-
		NB (L)	B	NBL	0.02
		SB (L)	-	-	-
		Overall	-	EBL	0.56
US 17-92 (Orlando Avenue) & W Fairbanks Avenue	Signalized	EB	F	EBL	0.90
		WB	F	WBT	1.14
		NB	E	NBT	0.92
		SB	E	SBL & SBT	0.75
		Overall	F *(90.0 sec)*	EBL	1.14

All study area intersections are anticipated to operate at an acceptable LOS with v/c ratios less than 1.0 in the background PM peak hour conditions with the exception of US 17-92/Orlando Avenue and W Fairbanks Avenue, which is anticipated to operate with LOS 'F'. As provided in the Synchro outputs, the signalized intersection also shows all left turn movements are expected to operate at LOS 'F' with peak hour queue lengths in the eastbound, westbound, and northbound left turn lanes exceeding storage capacity.

5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2021

5.1 BUILDOUT TRAFFIC

Future traffic conditions for the proposed development were evaluated for year 2021 conditions. Buildout volumes were developed by adding anticipated project trips to background volumes. A determination of the impact of project traffic on the roadway network was made, including LOS conditions for the intersections and roadway segments within the study area. Figure 6 illustrates turning movement volumes for buildout conditions at the study intersections. Adjusted turning movement volume worksheets for all intersections can be found in Appendix D.

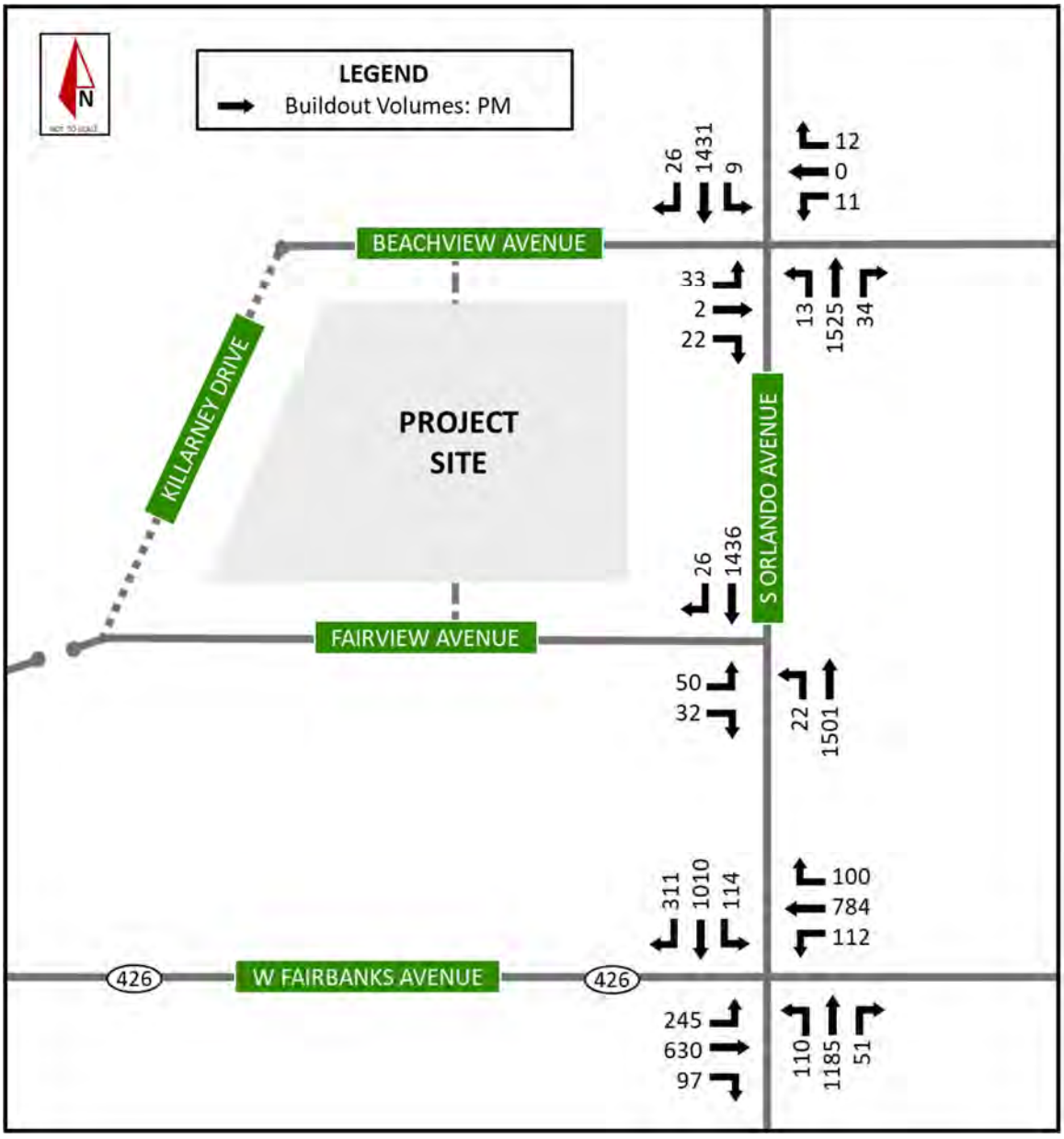


Figure 6: Buildout Intersection Volumes

5.2 BUILDOUT INTERSECTION ANALYSIS

Intersection operational analysis was performed for 2021 buildout conditions in the PM peak hour using procedures outlined in the *Highway Capacity Manual 2010* with *Synchro* (v10) software. Intersection level of service (LOS) and maximum volume to capacity (v/c) ratios for the buildout conditions are provided in Table 4. Synchro outputs are provided in Appendix E and existing signal timings are provided in Appendix H.

Table 4: Buildout Intersection Conditions (PM Peak Hour)

Buildout Condition - 2021					
Intersection	Control Type	Approach	PM Peak		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
US 17-92 (Orlando Avenue) & Beachview Avenue	Unsignalized (TWSC)	EB	F	EBL	0.56
		WB	E	WBL	0.24
		NB (L)	B	NBL	0.03
		SB (L)	B	SBL	0.02
		Overall	-	EBL	0.56
US 17-92 (Orlando Avenue) & Fairview Avenue	Unsignalized (TWSC)	EB	F	EBL	0.67
		WB	-	-	-
		NB (L)	B	NBL	0.05
		SB (L)	-	-	-
		Overall	-	EBL	0.67
US 17-92 (Orlando Avenue) & W Fairbanks Avenue	Signalized	EB	F	EBL	0.92
		WB	F	WBT	1.16
		NB	E	NBT	0.93
		SB	E	SBL	0.77
		Overall	F *(92.1 sec)*	EBL	1.16

No deficiencies are anticipated during the buildout PM peak hour with the exception of those mentioned in the background conditions intersection analysis. Project trips are not anticipated to add significant delay to the signalized intersection during the PM peak hour; delay per vehicle is expected to increase by only 2.1 seconds.

No new deficiencies were identified in the buildout condition as a result of project impact.

6.0 CONCLUSION

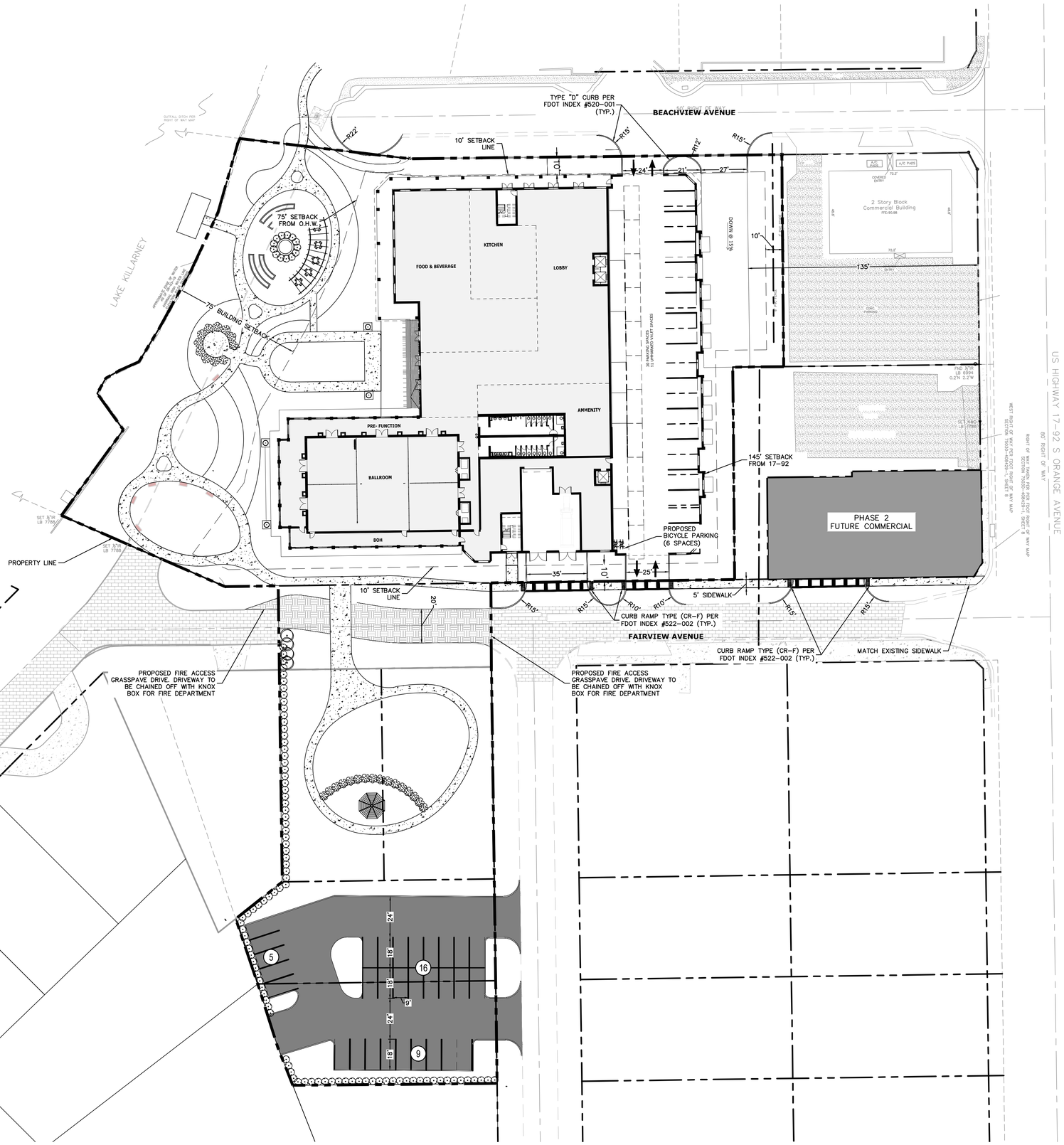
This traffic impact analysis was performed to assess the transportation impacts of Henderson Hotel, a proposed site redevelopment located northwest of US 17-92 and SR 426 (W Fairbanks Avenue) in the City of Winter Park, Florida. The site is currently occupied by seven (7) single family homes and 26,250 square feet of office space. The redevelopment, proposed for buildout in 2021, will consist of 132 hotel rooms with structured parking onsite. The proposed redevelopment of the site is anticipated to generate 917 additional daily trips and 59 additional PM peak hour trips (31 inbound and 28 outbound) to the external roadway network at buildout.

The site is to be accessed via two (2) driveways (one at Fairview Avenue and one at Beachview Avenue). Additionally, connectivity to Killarney Drive south of the project will be vacated, thus reducing cut-through traffic through the neighboring Killarney Estates.

An operational analysis performed for future background conditions (without project) show that all study area intersections are anticipated to operate at an acceptable LOS with v/c ratios less than 1.0 in the background PM peak hour conditions with the exception of US 17-92/Orlando Avenue and W Fairbanks Avenue, which is expected to operate with LOS 'F'.

No additional deficiencies were identified in the buildout condition as a result of project impact.

APPENDIX A
Conceptual Site Plan



NORTH

GRAPHIC SCALE IN FEET
0 15 30 60

SITE DATA: HENDERSON HOTEL

TOTAL AREA:	±2.97 ACRES
EXISTING ZONING:	C-3/O-1/R-1A
EXISTING USE:	COMMERCIAL, OFFICE, AND SINGLE FAMILY HOMES
PROPOSED LANDUSE:	HOTEL/PARK
PROPOSED BUILDING HEIGHT:	5 STORIES (55')
PROPOSED TOP OF PARAPET HEIGHT:	65'
PROPOSED ARCHITECTURAL FEATURES HEIGHT:	73'
IMPERVIOUS AREA:	1.76 AC
PERVIOUS:	1.21 AC
PROPOSED ISR:	0.59
PROPOSED OPEN SPACE:	35%
PROPOSED FAR:	0.99

REQUIRED BUILDING SETBACKS:

EXISTING COMMERCIAL PROPERTY (SIDE):	10 FT
BEACHVIEW AVE. (FRONT-STREET):	10 FT
FAIRVIEW AVE. (REAR-STREET):	10 FT
LAKESIDE (SIDE):	75 FT

PROPOSED BUILDING SETBACKS:

S. ORLANDO AVE. (SIDE):	115/145 FT
BEACHVIEW AVE. (FRONT-STREET):	10 FT
FAIRVIEW AVE. (REAR-STREET):	10 FT
LAKESIDE (SIDE):	75 FT

REQUIRED PARKING:

STANDARD:	235 SPACES

PROVIDED PARKING:

STANDARD (9'X18'):	229 SPACES
ADA PARKING (12'X18'):	6 SPACES
TOTAL PROVIDED:	235 SPACES

REQUIRED BICYCLE PARKING:

1 SPACE/30 ROOMS: 132 ROOMS/30 =	5 SPACES
1 SPACE/50 EMPLOYEES:	1 SPACE
TOTAL REQUIRED:	6 SPACES

PROVIDED BICYCLE PARKING:

TOTAL PROVIDED:	6 SPACES

PHASING:

THIS PROJECT WILL BE CONSTRUCTED IN TWO (2) PHASES.

STORMWATER MANAGEMENT:

ALL STORMWATER RUNOFF ON-SITE WILL BE TREATED IN IN AN UNDERGROUND EXFILTRATION SYSTEM.

100 YEAR FLOODPLAIN:

100-YEAR FLOODPLAIN ELEVATION IS 84.4' PER FEMA PANEL #12095C0255F

SITE DATA: PHASE 2 COMMERCIAL

TOTAL AREA:	±0.18 ACRES
EXISTING ZONING:	C-3
EXISTING USE:	COMMERCIAL, HOTEL/MOTEL
PROPOSED LANDUSE:	COMMERCIAL

REQUIRED BUILDING SETBACKS:

S. ORANGE AVE. (US 17-92):	5 FT
EXISTING COMMERCIAL PROPERTY (SIDE):	5 FT
FAIRVIEW AVE.:	10 FT
EXISTING COMMERCIAL PROPERT (REAR):	30 FT

LEGEND

- PROPERTY LINE
- BUILDING SETBACK
- PROPOSED EASEMENT
- PROPOSED CONCRETE SIDEWALK
- PROPOSED BRICK ROAD
- PROPOSED PAVEMENT
- PROPOSED GRAVEPAVE FIRE ACCESS

Always call 811 two full business days before you dig to have underground utilities located and marked.

Sunshine811.com

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2019 KIMLEY-HORN AND ASSOCIATES, INC.

189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
BROOKS A. STICKLER, P.E.
FLORIDA LICENSE NUMBER
74031

KHA PROJECT
149309001
DATE
07/07/2020
SCALE AS SHOWN
DESIGNED BY CJW
DRAWN BY CJW
CHECKED BY BAS
DATE: ----

SITE PLAN

HENDERSON HOTEL

FLORIDA
WINTER PARK

SHEET NUMBER
C2.0

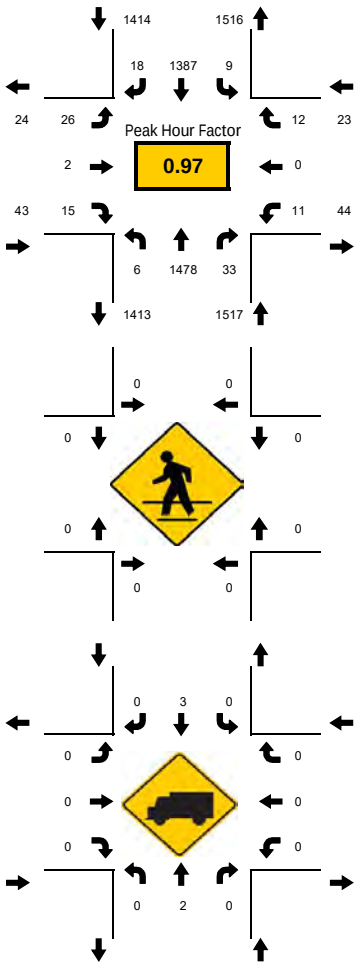
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

APPENDIX B
Turning Movement Counts

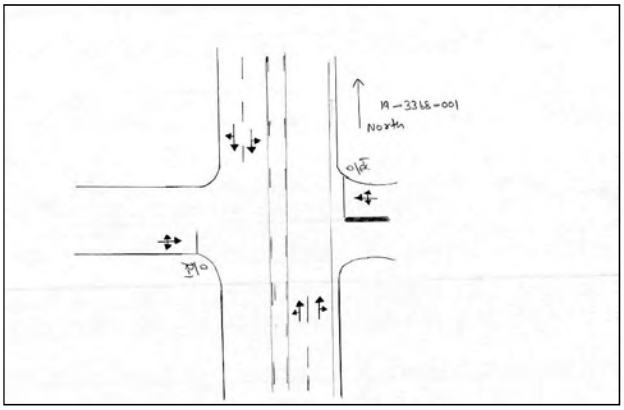
LOCATION: US 17/92/ Orlando Ave & Beachview Ave
CITY/STATE: Winter Park, FL

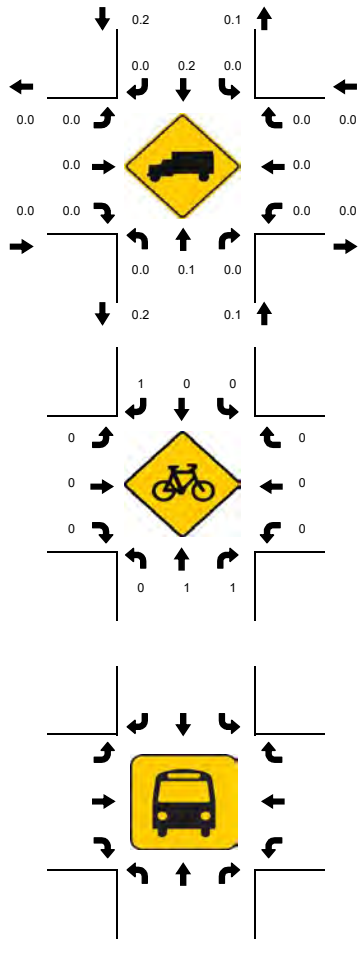
PROJECT ID: 19-03368-001
DATE: 05/21/2019

Peak-Hour: 04:45 PM - 05:45 PM
Peak 15-Minute: 05:15 PM - 05:30 PM



National Data & Surveying Services





15-Min Count Period Beginning At	US 17/92/ Orlando Ave Northbound					US 17/92/ Orlando Ave Southbound					Beachview Ave Eastbound					Beachview Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	2	322	5	1		0	317	2	0		1	0	4	0		2	0	3	0		659	2787
04:15 PM	2	375	6	0		2	307	6	0		9	0	1	0		0	0	0	0		708	2848
04:30 PM	0	322	5	0		1	325	2	0		4	1	4	0		2	0	5	0		671	2913
04:45 PM	2	377	12	0		1	342	2	0		6	1	2	0		1	0	3	0		749	2997
05:00 PM	0	358	8	0		3	335	1	0		7	0	2	0		3	0	3	0		720	2985
05:15 PM	1	382	7	0		3	357	5	0		8	1	4	0		2	0	3	0		773	2265
05:30 PM	3	361	6	0		2	353	10	0		5	0	7	0		5	0	3	0		755	1492
05:45 PM	1	370	9	0		4	339	1	0		8	1	3	0		0	0	1	0		737	737

Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	
All Vehicles	12	1528	48	0		12	1428	40	0		32	4	28	0		20	0	12	0		**3164**
Heavy Trucks	0	4	0			0	8	0			0	0	0			0	0	0			**12**
Pedestrians		0					0					0					0				**0**
Bicycles	0	4	4			0	0	4			0	0	0			0	0	0			**12**
Railroad																					
Stopped Buses																					

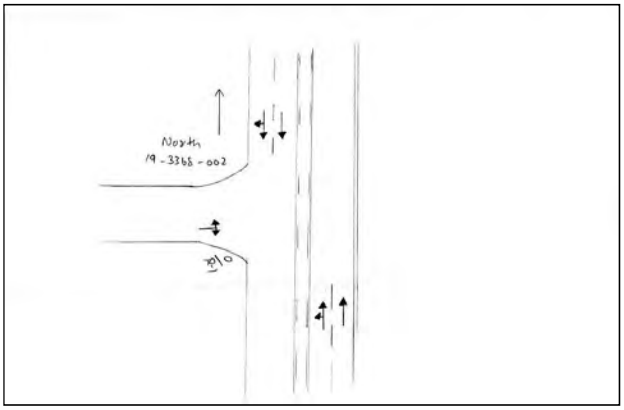
LOCATION: US 17/92/ Orlando Ave & Fairview Ave
CITY/STATE: Winter Park, FL

PROJECT ID: 19-03368-002
DATE: 05/21/2019

Peak-Hour: 05:00 PM - 06:00 PM
Peak 15-Minute: 05:15 PM - 05:30 PM

Peak Hour Factor: 0.95

National Data & Surveying Services



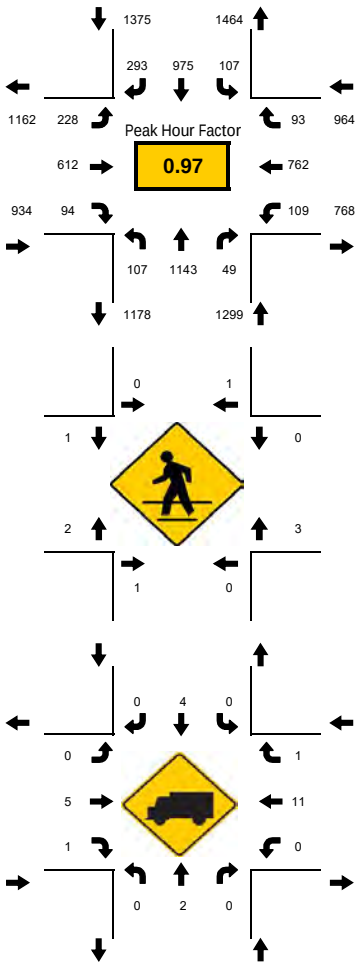
15-Min Count Period Beginning At	US 17/92/ Orlando Ave Northbound					US 17/92/ Orlando Ave Southbound					Fairview Ave Eastbound					Fairview Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	0	328	0	0		0	324	2	0		6	0	3	0		0	0	0	0		663	2764
04:15 PM	3	384	0	1		0	302	3	0		5	0	8	0		0	0	0	0		706	2798
04:30 PM	1	316	0	0		0	332	3	0		4	0	1	0		0	0	0	0		657	2864
04:45 PM	3	384	0	0		0	334	4	1		9	0	3	0		0	0	0	0		738	2915
05:00 PM	2	343	0	0		0	330	5	0		14	0	3	0		0	0	0	0		697	2932
05:15 PM	1	385	0	0		0	365	5	1		11	0	4	0		0	0	0	0		772	2235
05:30 PM	2	349	0	0		0	342	4	0		6	0	5	0		0	0	0	0		708	1463
05:45 PM	3	373	0	0		0	351	8	0		14	0	6	0		0	0	0	0		755	755

Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	
All Vehicles	12	1540	0	0		0	1460	32	4		56	0	24	0		0	0	0	0		**3128**
Heavy Trucks	0	8	0			0	8	0			0	0	0			0	0	0			**16**
Pedestrians		4						0				4					0				**8**
Bicycles	0	4	0			0	0	0			0	0	0			0	0	0			**4**
Railroad																					
Stopped Buses																					

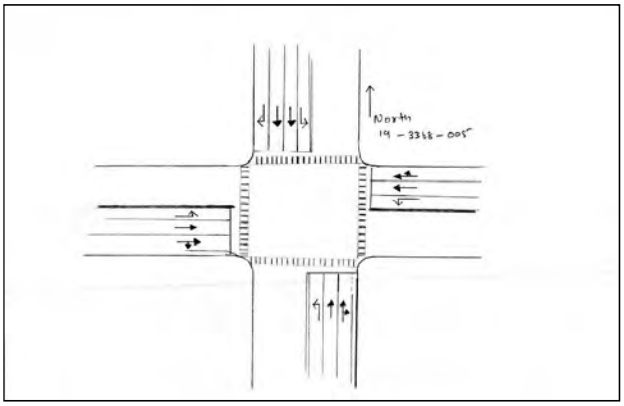
LOCATION: US 17/92/ Orlando Ave & Fairbanks Ave
CITY/STATE: Winter Park, FL

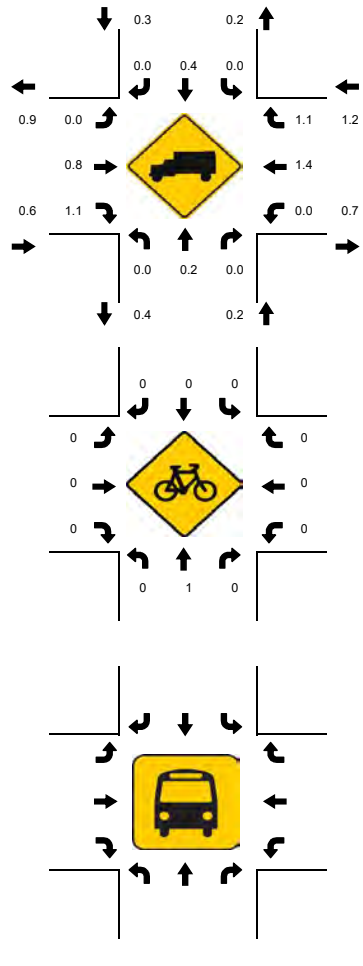
PROJECT ID: 19-03368-005
DATE: 05/21/2019

Peak-Hour: 05:00 PM - 06:00 PM
Peak 15-Minute: 05:30 PM - 05:45 PM



National Data & Surveying Services





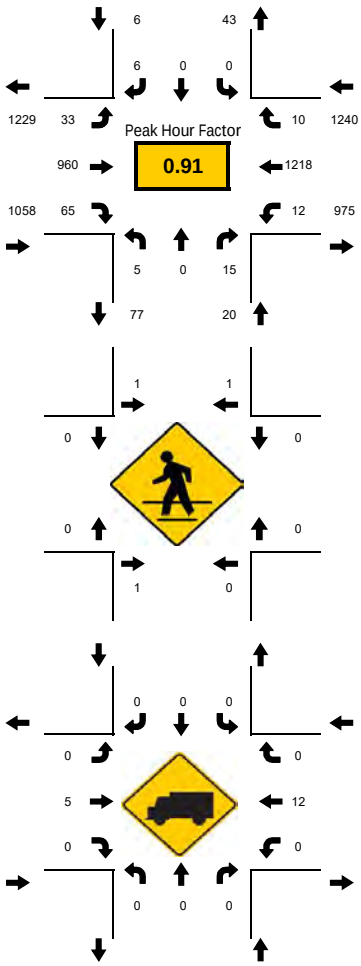
15-Min Count Period Beginning At	US 17/92/ Orlando Ave Northbound					US 17/92/ Orlando Ave Southbound					Fairbanks Ave Eastbound					Fairbanks Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	32	282	17	0	2	27	228	77	0	26	61	133	25	1	2	12	160	19	0	1	1074	4278
04:15 PM	17	244	13	0	0	34	213	65	0	6	48	119	27	0	0	30	183	39	0	1	1032	4352
04:30 PM	36	295	19	0	0	21	225	79	0	23	63	152	22	0	3	32	141	24	0	1	1109	4457
04:45 PM	22	262	14	0	1	27	214	65	0	11	55	113	21	0	2	31	216	23	0	3	1063	4528
05:00 PM	29	316	11	0	0	28	231	82	0	22	62	152	26	1	2	29	166	15	0	1	1148	4572
05:15 PM	27	266	12	0	0	30	242	68	0	21	51	149	24	0	4	25	208	35	0	2	1137	3424
05:30 PM	31	307	15	0	1	22	244	89	0	29	63	159	26	1	3	20	185	18	0	1	1180	2287
05:45 PM	20	254	11	0	0	27	258	54	0	12	50	152	18	0	4	35	203	25	0	3	1107	1107

Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	
All Vehicles	124	1264	60	0	4	120	1032	356	0	116	252	636	104	4	16	140	832	140	0	12	**5064**
Heavy Trucks	0	4	0			0	8	0			0	12	4			0	24	4			**56**
Pedestrians		4						4				8					8				**24**
Bicycles	0	4	0			0	0	0			0	0	0			0	0	0			**4**
Railroad																					
Stopped Buses																					

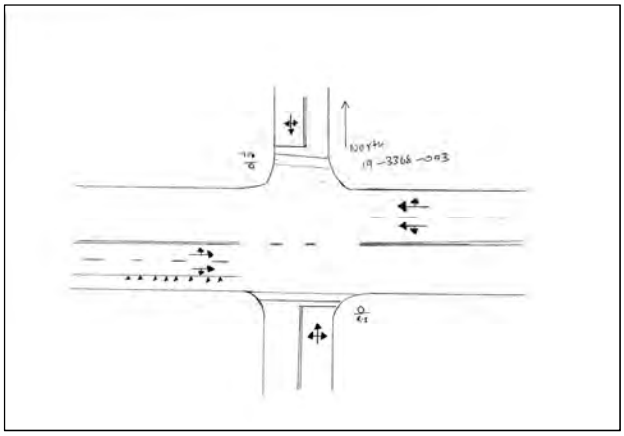
LOCATION: Shoreview Ave & Fairbanks Ave
CITY/STATE: Winter Park, FL

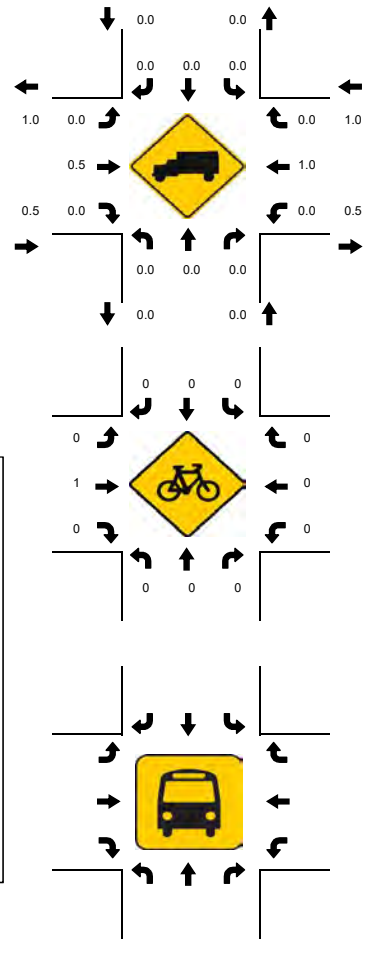
PROJECT ID: 19-03368-003
DATE: 05/21/2019

Peak-Hour: 05:00 PM - 06:00 PM
Peak 15-Minute: 05:30 PM - 05:45 PM



National Data & Surveying Services



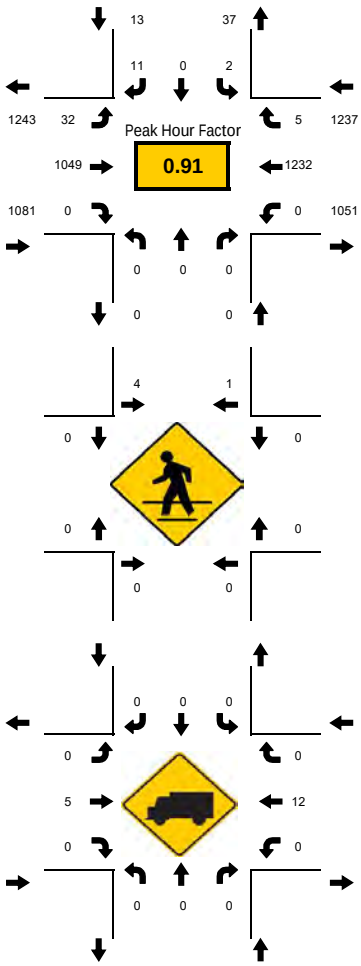


15-Min Count Period Beginning At	Shoreview Ave Northbound					Shoreview Ave Southbound					Fairbanks Ave Eastbound					Fairbanks Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	2	0	3	0		0	0	0	0		13	178	11	1		3	301	1	0		513	2111
04:15 PM	1	0	5	0		0	0	2	0		10	239	13	0		2	264	1	0		537	2178
04:30 PM	3	0	8	0		0	0	2	0		8	220	15	0		0	287	3	1		547	2221
04:45 PM	3	0	6	0		1	0	2	0		11	199	12	0		1	276	3	0		514	2314
05:00 PM	3	0	9	0		0	0	1	0		11	232	20	0		2	299	3	0		580	2324
05:15 PM	0	0	2	0		0	0	2	0		7	241	14	1		4	304	4	1		580	1744
05:30 PM	1	0	2	0		0	0	1	0		9	257	14	0		2	352	2	0		640	1164
05:45 PM	1	0	2	0		0	0	2	0		5	230	17	0		3	263	1	0		524	524
Peak 15-Min Flowrates	**Northbound**					**Southbound**					**Eastbound**					**Westbound**					**Total**	
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
All Vehicles	12	0	36	0		0	0	8	0		44	1028	80	4		16	1408	16	4		**2656**	
Heavy Trucks	0	0	0			0	0	0			0	8	0			0	16	0			**24**	
Pedestrians		4					4					0					0				**8**	
Bicycles	0	0	0			0	0	0			0	4	0			0	0	0			**4**	
Railroad																						
Stopped Buses																						

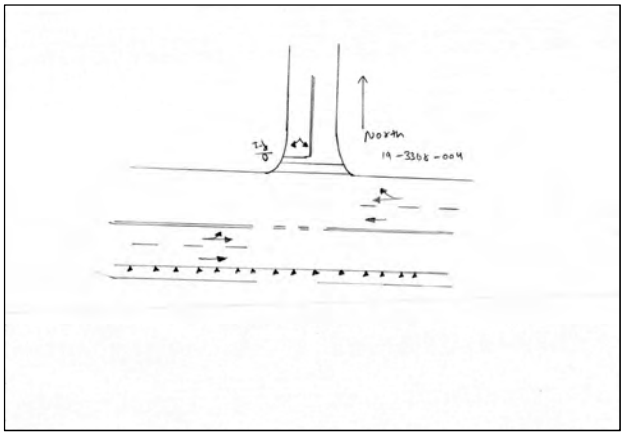
LOCATION: Lakeview Ave & Fairbanks Ave
CITY/STATE: Winter Park, FL

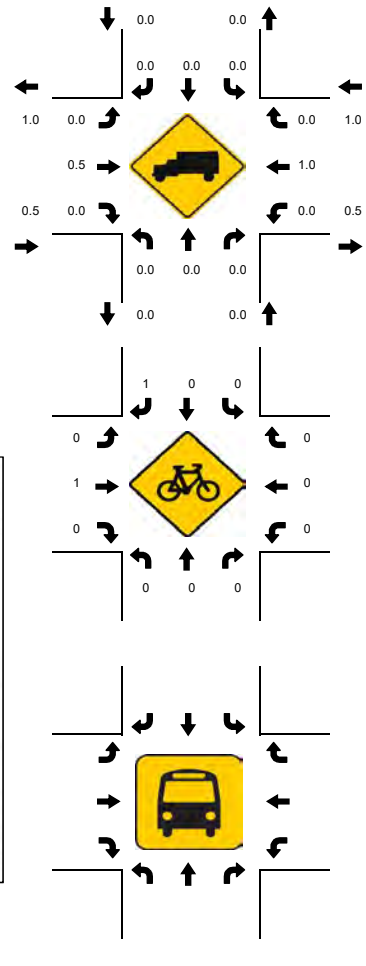
PROJECT ID: 19-03368-004
DATE: 05/21/2019

Peak-Hour: 05:00 PM - 06:00 PM
Peak 15-Minute: 05:30 PM - 05:45 PM



National Data & Surveying Services





15-Min Count Period Beginning At	Lakeview Ave Northbound					Lakeview Ave Southbound					Fairbanks Ave Eastbound					Fairbanks Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	0	0	0	0		0	0	0	0		0	212	0	0		0	311	2	1		526	2116
04:15 PM	0	0	0	0		0	0	2	0		4	252	0	1		0	247	2	0		508	2165
04:30 PM	0	0	0	0		0	0	1	0		13	253	0	0		0	304	0	0		571	2243
04:45 PM	0	0	0	0		0	0	2	0		9	226	0	0		0	274	0	0		511	2310
05:00 PM	0	0	0	0		1	0	2	0		7	251	0	0		0	314	0	0		575	2331
05:15 PM	0	0	0	0		0	0	2	0		7	278	0	0		0	298	1	0		586	1756
05:30 PM	0	0	0	0		1	0	3	0		4	263	0	0		0	363	4	0		638	1170
05:45 PM	0	0	0	0		0	0	4	0		14	257	0	0		0	257	0	0		532	532
Peak 15-Min Flowrates	**Northbound**					**Southbound**					**Eastbound**					**Westbound**					**Total**	
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
All Vehicles	0	0	0	0		4	0	16	0		56	1112	0	0		0	1452	16	0		**2656**	
Heavy Trucks	0	0	0			0	0	0			0	12	0			0	24	0			**36**	
Pedestrians		0					8					0					0				**8**	
Bicycles	0	0	0			0	0	4			0	4	0			0	0	0			**8**	
Railroad																						
Stopped Buses																						

APPENDIX C
FDOT's Florida Traffic Online (FTO) Data

2017 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
CATEGORY: 7500 ORANGE COUNTYWIDE

MOCF: 0.98

WEEK	DATES	SF	PSCF
1	01/01/2017 - 01/07/2017	1.01	1.03
2	01/08/2017 - 01/14/2017	1.03	1.05
3	01/15/2017 - 01/21/2017	1.04	1.06
4	01/22/2017 - 01/28/2017	1.03	1.05
5	01/29/2017 - 02/04/2017	1.02	1.04
6	02/05/2017 - 02/11/2017	1.00	1.02
7	02/12/2017 - 02/18/2017	0.99	1.01
8	02/19/2017 - 02/25/2017	0.99	1.01
* 9	02/26/2017 - 03/04/2017	0.98	1.00
*10	03/05/2017 - 03/11/2017	0.98	1.00
*11	03/12/2017 - 03/18/2017	0.97	0.99
*12	03/19/2017 - 03/25/2017	0.97	0.99
*13	03/26/2017 - 04/01/2017	0.97	0.99
*14	04/02/2017 - 04/08/2017	0.97	0.99
*15	04/09/2017 - 04/15/2017	0.97	0.99
*16	04/16/2017 - 04/22/2017	0.97	0.99
*17	04/23/2017 - 04/29/2017	0.97	0.99
*18	04/30/2017 - 05/06/2017	0.98	1.00
*19	05/07/2017 - 05/13/2017	0.98	1.00
*20	05/14/2017 - 05/20/2017	0.98	1.00
*21	05/21/2017 - 05/27/2017	0.99	1.01
22	05/28/2017 - 06/03/2017	1.00	1.02
23	06/04/2017 - 06/10/2017	1.00	1.02
24	06/11/2017 - 06/17/2017	1.01	1.03
25	06/18/2017 - 06/24/2017	1.01	1.03
26	06/25/2017 - 07/01/2017	1.01	1.03
27	07/02/2017 - 07/08/2017	1.01	1.03
28	07/09/2017 - 07/15/2017	1.02	1.04
29	07/16/2017 - 07/22/2017	1.01	1.03
30	07/23/2017 - 07/29/2017	1.00	1.02
31	07/30/2017 - 08/05/2017	1.00	1.02
32	08/06/2017 - 08/12/2017	0.99	1.01
33	08/13/2017 - 08/19/2017	0.99	1.01
34	08/20/2017 - 08/26/2017	1.01	1.03
35	08/27/2017 - 09/02/2017	1.04	1.06
36	09/03/2017 - 09/09/2017	1.06	1.08
37	09/10/2017 - 09/16/2017	1.09	1.11
38	09/17/2017 - 09/23/2017	1.07	1.09
39	09/24/2017 - 09/30/2017	1.05	1.07
40	10/01/2017 - 10/07/2017	1.03	1.05
41	10/08/2017 - 10/14/2017	1.01	1.03
42	10/15/2017 - 10/21/2017	0.99	1.01
43	10/22/2017 - 10/28/2017	0.99	1.01
44	10/29/2017 - 11/04/2017	0.99	1.01
45	11/05/2017 - 11/11/2017	1.00	1.02
46	11/12/2017 - 11/18/2017	1.00	1.02
47	11/19/2017 - 11/25/2017	1.00	1.02
48	11/26/2017 - 12/02/2017	1.01	1.03
49	12/03/2017 - 12/09/2017	1.01	1.03
50	12/10/2017 - 12/16/2017	1.01	1.03
51	12/17/2017 - 12/23/2017	1.02	1.04
52	12/24/2017 - 12/30/2017	1.03	1.05
53	12/31/2017 - 12/31/2017	1.04	1.06

* PEAK SEASON

APPENDIX D
Turning Movement Volume Worksheet

INTERSECTION VOLUME SHEET

US 17-92 / S Orlando Avenue & **Beachview Avenue**

Weekday PM Peak Hour	**US 17-92 / S Orlando Avenue**						**Beachview Avenue**					
	Northbound			Southbound			Eastbound			Westbound		
04:45 PM - 05:45 PM	L	T	R	L	T	R	L	T	R	L	T	R
TMC	6	1,478	33	9	1,387	18	26	2	15	11	0	12
Seasonal Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Heavy Vehicle	0.00%	0.14%	0.00%	0.00%	0.22%	0.00%	0.00%	0.00%	0.00%	0.00%	-	0.00%
Peak Hour Factor		0.97			0.97			0.83			0.72	
Existing Volume (2019)	**6**	**1,463**	**33**	**9**	**1,373**	**18**	**26**	**2**	**15**	**11**	**0**	**12**
Growth Factor	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Numbers of Years	2	2	2	2	2	2	2	2	2	2	2	2
Background (2021)	**6**	**1,522**	**34**	**9**	**1,428**	**19**	**27**	**2**	**16**	**11**	**0**	**12**
Project Assignment	23%	11%	0%	0%	11%	21%	21%	0%	23%	0%	0%	0%
Direction	**IN**	**OUT**	N/A	N/A	**IN**	**IN**	**OUT**	N/A	**OUT**	N/A	N/A	N/A
Project Trips	7	3	0	0	3	7	6	0	6	0	0	0
Project Buildout (2021)	**13**	**1,525**	**34**	**9**	**1,431**	**26**	**33**	**2**	**22**	**11**	**0**	**12**

INTERSECTION VOLUME SHEET

US 17-92 / S Orlando Avenue & **Fairview Avenue**

Weekday PM Peak Hour	**US 17-92 / S Orlando Avenue**						**Fairview Avenue**					
	Northbound			Southbound			Eastbound			Westbound		
05:00 PM - 06:00 PM	L	T	R	L	T	R	L	T	R	L	T	R
TMC	8	1,450	0	1	1,388	22	45	0	18	0	0	0
Seasonal Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Heavy Vehicle	0.00%	0.21%	-	0.00%	0.29%	0.00%	0.00%	-	0.00%	-	-	-
Peak Hour Factor		0.94			0.95			0.79				
Existing Volume (2019)	**8**	**1,436**	**0**	**1**	**1,374**	**22**	**45**	**0**	**18**	**0**	**0**	**0**
Growth Factor	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Numbers of Years	2	2	2	2	2	2	2	2	2	2	2	2
Background (2021)	**8**	**1,494**	**0**	**1**	**1,430**	**23**	**47**	**0**	**19**	**0**	**0**	**0**
Project Assignment	45%	23%	0%	0%	23%	11%	11%	0%	45%	0%	0%	0%
Direction	**IN**	**IN**	N/A	N/A	**OUT**	**IN**	**OUT**	N/A	**OUT**	N/A	N/A	N/A
Project Trips	14	7	0	0	6	3	3	0	13	0	0	0
Project Buildout (2021)	**22**	**1,501**	**0**	**1**	**1,436**	**26**	**50**	**0**	**32**	**0**	**0**	**0**

INTERSECTION VOLUME SHEET

US 17-92 / S Orlando Avenue & **W Fairbanks Avenue**

Weekday PM Peak Hour	**US 17-92 / S Orlando Avenue**						**W Fairbanks Avenue**					
	Northbound			Southbound			Eastbound			Westbound		
05:00 PM - 06:00 PM	L	T	R	L	T	R	L	T	R	L	T	R
TMC	107	1,143	49	107	975	293	228	612	94	109	762	93
Seasonal Factor	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99	0.99
Heavy Vehicle	0.00%	0.17%	0.00%	0.00%	0.41%	0.00%	0.00%	0.82%	1.06%	0.00%	1.44%	1.08%
Peak Hour Factor		0.91			0.97			0.94			0.90	
Existing Volume (2019)	**106**	**1,132**	**49**	**106**	**965**	**290**	**226**	**606**	**93**	**108**	**754**	**92**
Growth Factor	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Numbers of Years	2	2	2	2	2	2	2	2	2	2	2	2
Background (2021)	**110**	**1,178**	**51**	**110**	**1,004**	**302**	**235**	**630**	**97**	**112**	**784**	**96**
Project Assignment	0%	21%	0%	14%	21%	31%	31%	0%	0%	0%	0%	14%
Direction	N/A	**IN**	N/A	**OUT**	**OUT**	**OUT**	**IN**	N/A	N/A	N/A	N/A	**IN**
Project Trips	0	7	0	4	6	9	10	0	0	0	0	4
Project Buildout (2021)	**110**	**1,185**	**51**	**114**	**1,010**	**311**	**245**	**630**	**97**	**112**	**784**	**100**

APPENDIX E
Synchro Outputs

Intersection	
Int Delay, s/veh	1.3

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	26	2	15	11	0	12	6	1463	33	9	1373	18
Future Vol, veh/h	26	2	15	11	0	12	6	1463	33	9	1373	18
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	1	-	-	1	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	83	83	83	72	72	72	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	31	2	18	15	0	17	6	1508	34	9	1415	19

Major/Minor	Minor2			Minor1			Major1			Major2		
Conflicting Flow All	2209	2997	717	2264	2989	771	1434	0	0	1542	0	0
Stage 1	1443	1443	-	1537	1537	-	-	-	-	-	-	-
Stage 2	766	1554	-	727	1452	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	~ 25	14	377	23	14	347	480	-	-	436	-	-
Stage 1	141	199	-	124	179	-	-	-	-	-	-	-
Stage 2	366	176	-	386	197	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	~ 23	14	377	21	14	347	480	-	-	436	-	-
Mov Cap-2 Maneuver	98	88	-	90	89	-	-	-	-	-	-	-
Stage 1	139	195	-	123	177	-	-	-	-	-	-	-
Stage 2	344	174	-	355	193	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	49.4	36.2	0.1	0.1
HCM LOS	E	E		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	480	-	-	131	147	436	-	-
HCM Lane V/C Ratio	0.013	-	-	0.395	0.217	0.021	-	-
HCM Control Delay (s)	12.6	-	-	49.4	36.2	13.4	-	-
HCM Lane LOS	B	-	-	E	E	B	-	-
HCM 95th %tile Q(veh)	0	-	-	1.7	0.8	0.1	-	-

Notes

~: Volume exceeds capacity $: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection	
Int Delay, s/veh	1.3

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	45	18	8	1436	1374	22
Future Vol, veh/h	45	18	8	1436	1374	22
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	100	-	-	-
Veh in Median Storage, #	1	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	94	94	95	95
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	57	23	9	1528	1446	23

Major/Minor	Minor2		Major1		Major2	
Conflicting Flow All	2240	735	1469	0	-	0
Stage 1	1458	-	-	-	-	-
Stage 2	782	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	~ 37	367	465	-	-	-
Stage 1	184	-	-	-	-	-
Stage 2	417	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 36	367	465	-	-	-
Mov Cap-2 Maneuver	129	-	-	-	-	-
Stage 1	181	-	-	-	-	-
Stage 2	417	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	49	0.1	0
HCM LOS	E		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	465	-	158	-	-
HCM Lane V/C Ratio	0.018	-	0.505	-	-
HCM Control Delay (s)	12.9	-	49	-	-
HCM Lane LOS	B	-	E	-	-
HCM 95th %tile Q(veh)	0.1	-	2.4	-	-

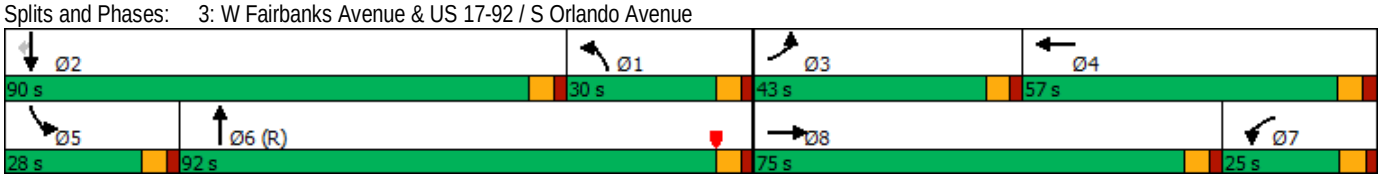
Notes			
~: Volume exceeds capacity	$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	226	606	93	108	754	92	106	1132	49	106	965	290
Future Volume (vph)	226	606	93	108	754	92	106	1132	49	106	965	290
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	500		0	240		0	170		0	190		60
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	50			100			25			25		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	1.00
Frt		0.980			0.984			0.994				0.850
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1805	3503	0	1805	3517	0	1805	3588	0	1805	3610	1615
Flt Permitted	0.950			0.950			0.950			0.950		
Satd. Flow (perm)	1805	3503	0	1805	3517	0	1805	3588	0	1805	3610	1615
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			6			2				107
Link Speed (mph)		35			35			35			35	
Link Distance (ft)		996			708			620			950	
Travel Time (s)		19.4			13.8			12.1			18.5	
Peak Hour Factor	0.94	0.94	0.94	0.90	0.90	0.90	0.91	0.91	0.91	0.97	0.97	0.97
Heavy Vehicles (%)	0%	1%	1%	0%	1%	1%	0%	0%	0%	0%	0%	0%
Adj. Flow (vph)	240	645	99	120	838	102	116	1244	54	109	995	299
Shared Lane Traffic (%)												
Lane Group Flow (vph)	240	744	0	120	940	0	116	1298	0	109	995	299
Turn Type	Prot	NA		Prot	NA		Prot	NA		Prot	NA	Perm
Protected Phases	3	8		7	4		1	6		5	2	
Permitted Phases												2
Detector Phase	3	8		7	4		1	6		5	2	2
Switch Phase												
Minimum Initial (s)	5.0	7.0		5.0	7.0		5.0	15.0		5.0	15.0	15.0
Minimum Split (s)	11.1	42.3		11.2	39.6		11.3	36.2		11.0	38.1	38.1
Total Split (s)	43.0	75.0		25.0	57.0		30.0	92.0		28.0	90.0	90.0
Total Split (%)	19.5%	34.1%		11.4%	25.9%		13.6%	41.8%		12.7%	40.9%	40.9%
Maximum Green (s)	37.0	69.0		18.8	50.4		24.0	86.0		22.0	84.0	84.0
Yellow Time (s)	4.0	4.0		4.1	4.1		4.0	4.0		4.0	4.0	4.0
All-Red Time (s)	2.0	2.0		2.1	2.5		2.0	2.0		2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0		6.2	6.6		6.0	6.0		6.0	6.0	6.0
Lead/Lag	Lead	Lead		Lag	Lag		Lag	Lag		Lead	Lead	Lead
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	Yes
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	4.0		3.0	4.0	4.0
Recall Mode	None	None		None	None		None	C-Max		None	Max	Max
Walk Time (s)		7.0			7.0			7.0			7.0	7.0
Flash Dont Walk (s)		29.0			26.0			23.0			25.0	25.0
Pedestrian Calls (#/hr)		0			0			0			0	0
Act Effct Green (s)	33.0	53.6		34.2	54.4		24.0	90.0		18.0	84.0	84.0
Actuated g/C Ratio	0.15	0.24		0.16	0.25		0.11	0.41		0.08	0.38	0.38
v/c Ratio	0.89	0.87		0.43	1.08		0.59	0.88		0.74	0.72	0.44
Control Delay	122.7	89.8		91.6	125.9		106.5	68.4		126.7	61.7	33.5
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	0.0
Total Delay	122.7	89.8		91.6	125.9		106.5	68.4		126.7	61.7	33.5
LOS	F	F		F	F		F	E		F	E	C
Approach Delay		97.8			122.0			71.6			60.7	
Approach LOS		F			F			E			E	
Queue Length 50th (ft)	344	546		161	~815		164	924		157	652	207
Queue Length 95th (ft)	#472	597		253	#986		246	1053		234	737	307
Internal Link Dist (ft)		916			628			540			870	

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Turn Bay Length (ft)	500			240			170			190		60
Base Capacity (vph)	303	1104		280	873		196	1469		180	1378	682
Starvation Cap Reductn	0	0		0	0		0	0		0	0	0
Spillback Cap Reductn	0	0		0	0		0	0		0	0	0
Storage Cap Reductn	0	0		0	0		0	0		0	0	0
Reduced v/c Ratio	0.79	0.67		0.43	1.08		0.59	0.88		0.61	0.72	0.44

Intersection Summary			
Area Type:	Other		
Cycle Length: 220			
Actuated Cycle Length: 220			
Offset: 29 (13%), Referenced to phase 6:NBT, Start of Yellow			
Natural Cycle: 125			
Control Type: Actuated-Coordinated			
Maximum v/c Ratio: 1.08			
Intersection Signal Delay: 84.8		Intersection LOS: F	
Intersection Capacity Utilization 95.5%		ICU Level of Service F	
Analysis Period (min) 15			
~ Volume exceeds capacity, queue is theoretically infinite.			
Queue shown is maximum after two cycles.			
# 95th percentile volume exceeds capacity, queue may be longer.			
Queue shown is maximum after two cycles.			

Splits and Phases: 3: W Fairbanks Avenue & US 17-92 / S Orlando Avenue



Intersection	
Int Delay, s/veh	1.4

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	27	2	16	11	0	12	6	1522	34	9	1428	19
Future Vol, veh/h	27	2	16	11	0	12	6	1522	34	9	1428	19
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	1	-	-	1	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	83	83	83	72	72	72	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	33	2	19	15	0	17	6	1569	35	9	1472	20

Major/Minor	Minor2			Minor1			Major1			Major2		
Conflicting Flow All	2297	3116	746	2354	3109	802	1492	0	0	1604	0	0
Stage 1	1500	1500	-	1599	1599	-	-	-	-	-	-	-
Stage 2	797	1616	-	755	1510	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	~ 21	12	360	19	12	331	456	-	-	413	-	-
Stage 1	130	187	-	113	167	-	-	-	-	-	-	-
Stage 2	351	164	-	371	185	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	~ 19	12	360	17	12	331	456	-	-	413	-	-
Mov Cap-2 Maneuver	90	81	-	82	83	-	-	-	-	-	-	-
Stage 1	128	183	-	112	165	-	-	-	-	-	-	-
Stage 2	329	162	-	339	181	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	56.2	39.7	0	0.1
HCM LOS	F	E		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	456	-	-	122	135	413	-	-
HCM Lane V/C Ratio	0.014	-	-	0.444	0.237	0.022	-	-
HCM Control Delay (s)	13	-	-	56.2	39.7	13.9	-	-
HCM Lane LOS	B	-	-	F	E	B	-	-
HCM 95th %tile Q(veh)	0	-	-	2	0.9	0.1	-	-

Notes
~: Volume exceeds capacity $: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection	
Int Delay, s/veh	1.5

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	47	19	8	1494	1430	23
Future Vol, veh/h	47	19	8	1494	1430	23
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	100	-	-	-
Veh in Median Storage, #	1	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	94	94	95	95
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	59	24	9	1589	1505	24

Major/Minor	Minor2		Major1		Major2	
Conflicting Flow All	2330	765	1529	0	-	0
Stage 1	1517	-	-	-	-	-
Stage 2	813	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	~ 32	350	441	-	-	-
Stage 1	171	-	-	-	-	-
Stage 2	402	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 31	350	441	-	-	-
Mov Cap-2 Maneuver	120	-	-	-	-	-
Stage 1	168	-	-	-	-	-
Stage 2	402	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	57	0.1	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	441	-	148	-	-
HCM Lane V/C Ratio	0.019	-	0.564	-	-
HCM Control Delay (s)	13.3	-	57	-	-
HCM Lane LOS	B	-	F	-	-
HCM 95th %tile Q(veh)	0.1	-	2.9	-	-

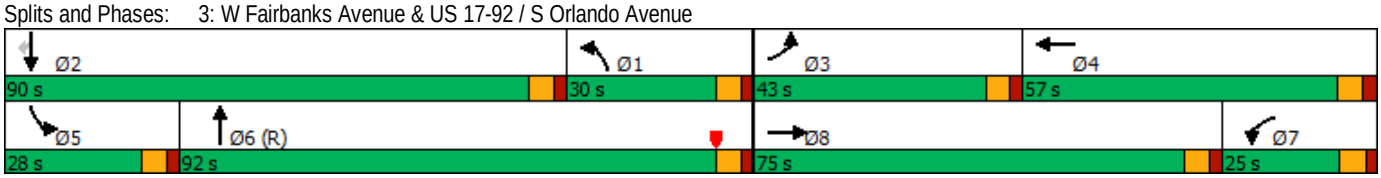
Notes			
~: Volume exceeds capacity	$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	235	630	97	112	784	96	110	1178	51	110	1004	302
Future Volume (vph)	235	630	97	112	784	96	110	1178	51	110	1004	302
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	500		0	240		0	170		0	190		60
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	50			100			25			25		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	1.00
Frt		0.980			0.984			0.994				0.850
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1805	3503	0	1805	3517	0	1805	3588	0	1805	3610	1615
Flt Permitted	0.950			0.950			0.950			0.950		
Satd. Flow (perm)	1805	3503	0	1805	3517	0	1805	3588	0	1805	3610	1615
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			6			2				107
Link Speed (mph)		35			35			35			35	
Link Distance (ft)		996			708			620			950	
Travel Time (s)		19.4			13.8			12.1			18.5	
Peak Hour Factor	0.94	0.94	0.94	0.90	0.90	0.90	0.91	0.91	0.91	0.97	0.97	0.97
Heavy Vehicles (%)	0%	1%	1%	0%	1%	1%	0%	0%	0%	0%	0%	0%
Adj. Flow (vph)	250	670	103	124	871	107	121	1295	56	113	1035	311
Shared Lane Traffic (%)												
Lane Group Flow (vph)	250	773	0	124	978	0	121	1351	0	113	1035	311
Turn Type	Prot	NA		Prot	NA		Prot	NA		Prot	NA	Perm
Protected Phases	3	8		7	4		1	6		5	2	
Permitted Phases												2
Detector Phase	3	8		7	4		1	6		5	2	2
Switch Phase												
Minimum Initial (s)	5.0	7.0		5.0	7.0		5.0	15.0		5.0	15.0	15.0
Minimum Split (s)	11.1	42.3		11.2	39.6		11.3	36.2		11.0	38.1	38.1
Total Split (s)	43.0	75.0		25.0	57.0		30.0	92.0		28.0	90.0	90.0
Total Split (%)	19.5%	34.1%		11.4%	25.9%		13.6%	41.8%		12.7%	40.9%	40.9%
Maximum Green (s)	37.0	69.0		18.8	50.4		24.0	86.0		22.0	84.0	84.0
Yellow Time (s)	4.0	4.0		4.1	4.1		4.0	4.0		4.0	4.0	4.0
All-Red Time (s)	2.0	2.0		2.1	2.5		2.0	2.0		2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0		6.2	6.6		6.0	6.0		6.0	6.0	6.0
Lead/Lag	Lead	Lead		Lag	Lag		Lag	Lag		Lead	Lead	Lead
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	Yes
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	4.0		3.0	4.0	4.0
Recall Mode	None	None		None	None		None	C-Max		None	Max	Max
Walk Time (s)		7.0			7.0			7.0			7.0	7.0
Flash Dont Walk (s)		29.0			26.0			23.0			25.0	25.0
Pedestrian Calls (#/hr)		0			0			0			0	0
Act Effct Green (s)	33.9	55.5		32.3	53.5		24.0	89.7		18.3	84.0	84.0
Actuated g/C Ratio	0.15	0.25		0.15	0.24		0.11	0.41		0.08	0.38	0.38
v/c Ratio	0.90	0.87		0.47	1.14		0.62	0.92		0.75	0.75	0.46
Control Delay	123.8	88.8		94.6	144.9		108.0	72.7		127.5	63.1	34.6
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	0.0
Total Delay	123.8	88.8		94.6	144.9		108.0	72.7		127.5	63.1	34.6
LOS	F	F		F	F		F	E		F	E	C
Approach Delay		97.3			139.3			75.6			62.0	
Approach LOS		F			F			E			E	
Queue Length 50th (ft)	358	567		168	~895		171	989		163	689	223
Queue Length 95th (ft)	#505	618		262	#1045		256	#1158		244	775	324
Internal Link Dist (ft)		916			628			540			870	

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Turn Bay Length (ft)	500			240			170			190		60
Base Capacity (vph)	303	1104		264	860		196	1463		180	1378	682
Starvation Cap Reductn	0	0		0	0		0	0		0	0	0
Spillback Cap Reductn	0	0		0	0		0	0		0	0	0
Storage Cap Reductn	0	0		0	0		0	0		0	0	0
Reduced v/c Ratio	0.83	0.70		0.47	1.14		0.62	0.92		0.63	0.75	0.46

Intersection Summary			
Area Type:	Other		
Cycle Length: 220			
Actuated Cycle Length: 220			
Offset: 29 (13%), Referenced to phase 6:NBT, Start of Yellow			
Natural Cycle: 135			
Control Type: Actuated-Coordinated			
Maximum v/c Ratio: 1.14			
Intersection Signal Delay: 90.0		Intersection LOS: F	
Intersection Capacity Utilization 98.5%		ICU Level of Service F	
Analysis Period (min) 15			
~ Volume exceeds capacity, queue is theoretically infinite.			
Queue shown is maximum after two cycles.			
# 95th percentile volume exceeds capacity, queue may be longer.			
Queue shown is maximum after two cycles.			

Splits and Phases: 3: W Fairbanks Avenue & US 17-92 / S Orlando Avenue



Intersection	
Int Delay, s/veh	1.9

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	33	2	22	11	0	12	13	1525	34	9	1431	26
Future Vol, veh/h	33	2	22	11	0	12	13	1525	34	9	1431	26
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	100	-	-	100	-	-
Veh in Median Storage, #	-	1	-	-	1	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	83	83	83	72	72	72	97	97	97	97	97	97
Heavy Vehicles, %	0	0	0	0	0	0	0	0	0	0	0	0
Mvmt Flow	40	2	27	15	0	17	13	1572	35	9	1475	27

Major/Minor	Minor2			Minor1			Major1			Major2		
Conflicting Flow All	2319	3140	751	2373	3136	804	1502	0	0	1607	0	0
Stage 1	1507	1507	-	1616	1616	-	-	-	-	-	-	-
Stage 2	812	1633	-	757	1520	-	-	-	-	-	-	-
Critical Hdwy	7.5	6.5	6.9	7.5	6.5	6.9	4.1	-	-	4.1	-	-
Critical Hdwy Stg 1	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.5	5.5	-	6.5	5.5	-	-	-	-	-	-	-
Follow-up Hdwy	3.5	4	3.3	3.5	4	3.3	2.2	-	-	2.2	-	-
Pot Cap-1 Maneuver	~ 21	11	358	19	11	330	452	-	-	412	-	-
Stage 1	129	185	-	110	164	-	-	-	-	-	-	-
Stage 2	343	161	-	370	183	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	~ 19	10	358	17	10	330	452	-	-	412	-	-
Mov Cap-2 Maneuver	88	79	-	79	79	-	-	-	-	-	-	-
Stage 1	125	181	-	107	159	-	-	-	-	-	-	-
Stage 2	316	156	-	331	179	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	66.2	41.1	0.1	0.1
HCM LOS	F	E		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	452	-	-	123	131	412	-	-
HCM Lane V/C Ratio	0.03	-	-	0.558	0.244	0.023	-	-
HCM Control Delay (s)	13.2	-	-	66.2	41.1	13.9	-	-
HCM Lane LOS	B	-	-	F	E	B	-	-
HCM 95th %tile Q(veh)	0.1	-	-	2.7	0.9	0.1	-	-

Notes
~: Volume exceeds capacity $: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection	
Int Delay, s/veh	2.2

Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	50	32	22	1501	1436	26
Future Vol, veh/h	50	32	22	1501	1436	26
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	100	-	-	-
Veh in Median Storage, #	1	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	94	94	95	95
Heavy Vehicles, %	0	0	0	0	0	0
Mvmt Flow	63	41	23	1597	1512	27

Major/Minor	Minor2		Major1		Major2	
Conflicting Flow All	2371	770	1539	0	-	0
Stage 1	1526	-	-	-	-	-
Stage 2	845	-	-	-	-	-
Critical Hdwy	6.8	6.9	4.1	-	-	-
Critical Hdwy Stg 1	5.8	-	-	-	-	-
Critical Hdwy Stg 2	5.8	-	-	-	-	-
Follow-up Hdwy	3.5	3.3	2.2	-	-	-
Pot Cap-1 Maneuver	~ 30	348	438	-	-	-
Stage 1	169	-	-	-	-	-
Stage 2	387	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	~ 28	348	438	-	-	-
Mov Cap-2 Maneuver	114	-	-	-	-	-
Stage 1	160	-	-	-	-	-
Stage 2	387	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	65.8	0.2	0
HCM LOS	F		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	438	-	155	-	-
HCM Lane V/C Ratio	0.053	-	0.67	-	-
HCM Control Delay (s)	13.7	-	65.8	-	-
HCM Lane LOS	B	-	F	-	-
HCM 95th %tile Q(veh)	0.2	-	3.8	-	-

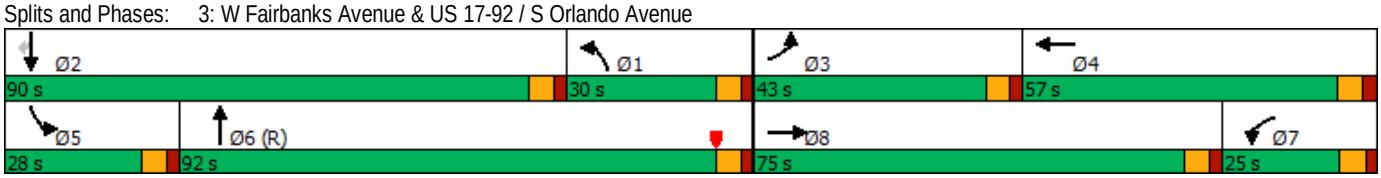
Notes			
~: Volume exceeds capacity	$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	245	630	97	112	784	100	110	1185	51	114	1010	311
Future Volume (vph)	245	630	97	112	784	100	110	1185	51	114	1010	311
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	500		0	240		0	170		0	190		60
Storage Lanes	1		0	1		0	1		0	1		1
Taper Length (ft)	50			100			25			25		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	1.00
Frt		0.980			0.983			0.994				0.850
Flt Protected	0.950			0.950			0.950			0.950		
Satd. Flow (prot)	1805	3503	0	1805	3513	0	1805	3588	0	1805	3610	1615
Flt Permitted	0.950			0.950			0.950			0.950		
Satd. Flow (perm)	1805	3503	0	1805	3513	0	1805	3588	0	1805	3610	1615
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		8			6			2				107
Link Speed (mph)		35			35			35			35	
Link Distance (ft)		996			1275			620			950	
Travel Time (s)		19.4			24.8			12.1			18.5	
Peak Hour Factor	0.94	0.94	0.94	0.90	0.90	0.90	0.91	0.91	0.91	0.97	0.97	0.97
Heavy Vehicles (%)	0%	1%	1%	0%	1%	1%	0%	0%	0%	0%	0%	0%
Adj. Flow (vph)	261	670	103	124	871	111	121	1302	56	118	1041	321
Shared Lane Traffic (%)												
Lane Group Flow (vph)	261	773	0	124	982	0	121	1358	0	118	1041	321
Turn Type	Prot	NA		Prot	NA		Prot	NA		Prot	NA	Perm
Protected Phases	3	8		7	4		1	6		5	2	
Permitted Phases												2
Detector Phase	3	8		7	4		1	6		5	2	2
Switch Phase												
Minimum Initial (s)	5.0	7.0		5.0	7.0		5.0	15.0		5.0	15.0	15.0
Minimum Split (s)	11.1	42.3		11.2	39.6		11.3	36.2		11.0	38.1	38.1
Total Split (s)	43.0	75.0		25.0	57.0		30.0	92.0		28.0	90.0	90.0
Total Split (%)	19.5%	34.1%		11.4%	25.9%		13.6%	41.8%		12.7%	40.9%	40.9%
Maximum Green (s)	37.0	69.0		18.8	50.4		24.0	86.0		22.0	84.0	84.0
Yellow Time (s)	4.0	4.0		4.1	4.1		4.0	4.0		4.0	4.0	4.0
All-Red Time (s)	2.0	2.0		2.1	2.5		2.0	2.0		2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	0.0
Total Lost Time (s)	6.0	6.0		6.2	6.6		6.0	6.0		6.0	6.0	6.0
Lead/Lag	Lead	Lead		Lag	Lag		Lag	Lag		Lead	Lead	Lead
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes	Yes	Yes
Vehicle Extension (s)	3.0	3.0		3.0	3.0		3.0	4.0		3.0	4.0	4.0
Recall Mode	None	None		None	None		None	C-Max		None	Max	Max
Walk Time (s)		7.0			7.0			7.0			7.0	7.0
Flash Dont Walk (s)		29.0			26.0			23.0			25.0	25.0
Pedestrian Calls (#/hr)		0			0			0			0	0
Act Effct Green (s)	34.7	55.5		32.3	52.7		24.0	89.3		18.7	84.0	84.0
Actuated g/C Ratio	0.16	0.25		0.15	0.24		0.11	0.41		0.08	0.38	0.38
v/c Ratio	0.92	0.87		0.47	1.16		0.62	0.93		0.77	0.76	0.47
Control Delay	125.8	88.8		94.6	153.0		108.0	74.0		129.1	63.4	35.5
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	0.0
Total Delay	125.8	88.8		94.6	153.0		108.0	74.0		129.1	63.4	35.5
LOS	F	F		F	F		F	E		F	E	D
Approach Delay		98.1			146.5			76.8			62.6	
Approach LOS		F			F			E			E	
Queue Length 50th (ft)	375	567		168	~910		171	1002		171	695	235
Queue Length 95th (ft)	#541	618		262	#1053		256	#1170		252	781	340
Internal Link Dist (ft)		916			1195			540			870	

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Turn Bay Length (ft)	500			240			170			190		60
Base Capacity (vph)	303	1104		264	846		196	1457		180	1378	682
Starvation Cap Reductn	0	0		0	0		0	0		0	0	0
Spillback Cap Reductn	0	0		0	0		0	0		0	0	0
Storage Cap Reductn	0	0		0	0		0	0		0	0	0
Reduced v/c Ratio	0.86	0.70		0.47	1.16		0.62	0.93		0.66	0.76	0.47

Intersection Summary			
Area Type:	Other		
Cycle Length: 220			
Actuated Cycle Length: 220			
Offset: 29 (13%), Referenced to phase 6:NBT, Start of Yellow			
Natural Cycle: 125			
Control Type: Actuated-Coordinated			
Maximum v/c Ratio: 1.16			
Intersection Signal Delay: 92.1		Intersection LOS: F	
Intersection Capacity Utilization 99.6%		ICU Level of Service F	
Analysis Period (min) 15			
~ Volume exceeds capacity, queue is theoretically infinite.			
Queue shown is maximum after two cycles.			
# 95th percentile volume exceeds capacity, queue may be longer.			
Queue shown is maximum after two cycles.			

Splits and Phases: 3: W Fairbanks Avenue & US 17-92 / S Orlando Avenue



APPENDIX F
Killarney Estates Neighborhood Traffic Study

Kimley»Horn

MEMORANDUM

To: Adam Wonus
Atrium Management Company

From: James M. Taylor, P.E.
Kimley-Horn and Associates, Inc.

Date: June 6, 2019

Subject: Killarney Estates Neighborhood Cut-through Traffic Study
City of Winter Park

Purpose

The following study was conducted to determine the amount of cut-through traffic occurring through the Killarney Estates neighborhood in Winter Park, FL during a typical weekday PM peak hour. Additionally, impacts of proposed solutions being considered to reduce the cut-through behavior were analyzed. The study was performed at the request of the developer of the proposed Henderson Hotel, located adjacent to the neighborhood, to facilitate discussions with the residents of Killarney Estates and the City of Winter Park regarding implementation of solutions to reduce cut-through traffic through the neighborhood.

Project Description

The Killarney Estates Subdivision is generally defined by the Orange County Property Appraiser as mixed-used parcels located in the northwest quadrant of the intersection at Fairbanks Avenue & US 17-92 in Winter Park, Florida. The neighborhood is bound in the west by lots along Lakeview Avenue and in the north by lots along Beachview Avenue. Retail and office uses are generally located along Fairbanks Avenue and US 17-92, while residential uses are located along internal local roads.

To avoid congestion on W Fairbanks Avenue and US 17-92, vehicles regularly use the local roads within the residential portion of Killarney Estates as a cut-through route. As shown in **Figure 1**, traffic generally accesses the residential portion of Killarney Estates from US 17-92 at Beachview Avenue (#1) and Fairview Avenue (#2). Traffic accesses the residential portion of Killarney Estates from W Fairbanks Avenue at Shoreview Avenue (#3) and Lakeview Avenue (#4). Several portions of the local roads within Killarney Estates are currently designated as one-way facilities.

Kimley»Horn

Figure 1: Project Location Map & Study Area Intersections

Kimley»Horn

Data Collection Methodology

To quantify the existing amount of cut-through traffic that occurs through Killarney Estates between W Fairbanks Avenue to US 17-92 in the PM peak hour, intersection count data was collected at the following four (4) primary access points during a typical weekday PM peak period (4 PM to 6 PM) as identified in **Figure 1**:

1. US 17-92 & Beachview Avenue
2. US 17-92 & Fairview Avenue
3. W Fairbanks Avenue & Shoreview Avenue
4. W Fairbanks Avenue & Lakeview Avenue

License plates were inventoried at the four (4) locations to isolate cut-through traffic from local traffic. The raw origin-destination data was collected on May 21, 2019 during the peak period (4 PM to 6 PM) and is provided in **Attachment A.**

Existing Cut-through Volumes

PM peak period (4 PM – 6 PM) origin-destination data at the four (4) access points was reviewed to determine PM peak hour cut-through behavior. A summary of the PM peak hour cut-through volumes is summarized by origin and destination in **Table 1** below.

Table 1: Summary of PM Peak Hour Cut-through Traffic Volumes

FROM (Origin) \ TO (Destination)	1. Beachview Ave	2. Fairview Ave	3. Shoreview Ave	4. Lakeview Ave
1. Beachview Ave		-	1	0
2. Fairview Ave	-		2	2
3. Shoreview Ave	5	9		-
4. Lakeview Ave	13	9	-	

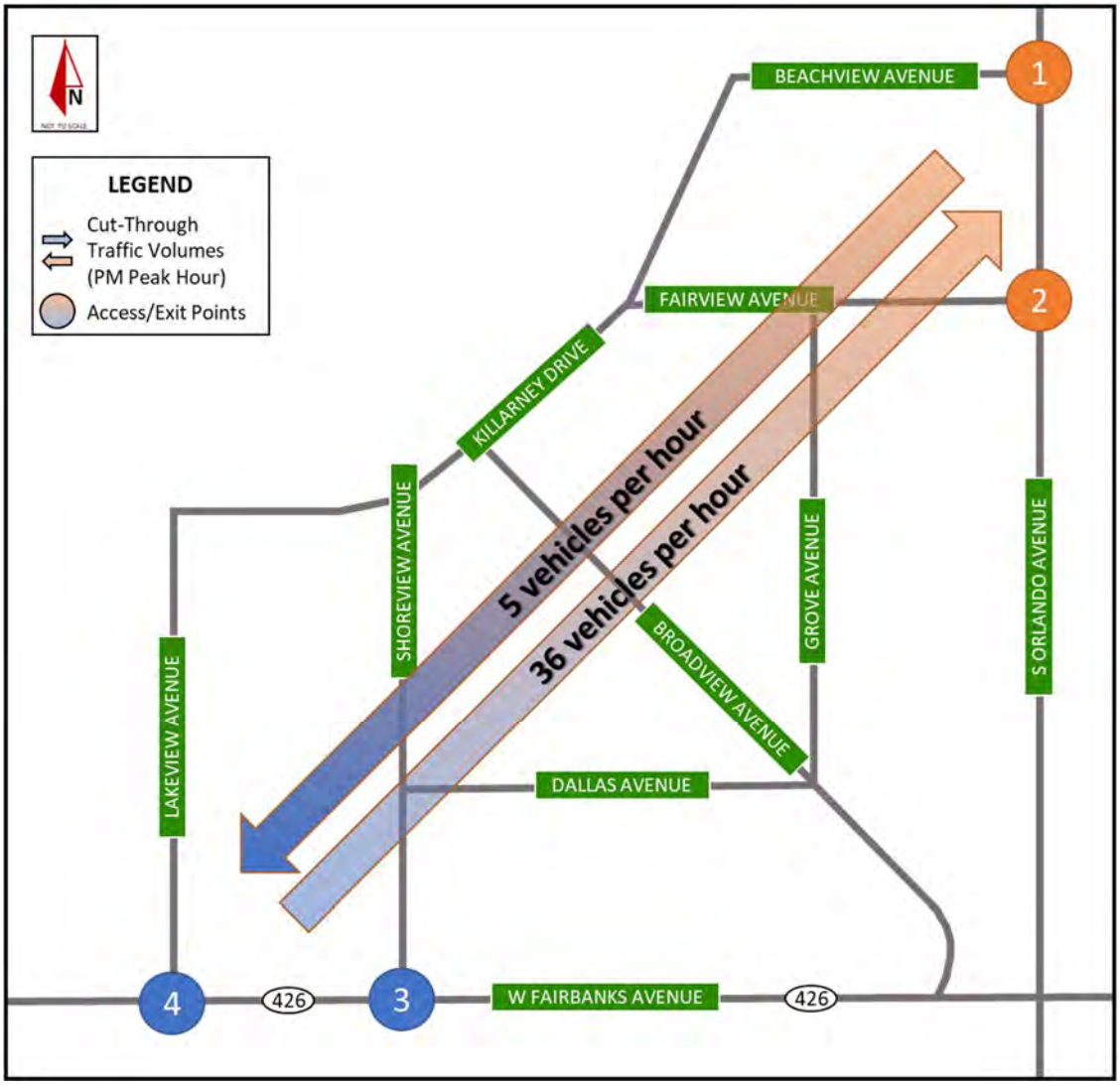
Notes: Values are cumulative number of vehicles per hour by direction.

Kimley»Horn

As shown in the table, 5 vehicles were observed cutting through Killarney Estates during the PM peak hour in the westbound direction (from US 17-92 to W Fairbanks Avenue) using the study area intersections. 36 vehicles were observed cutting through Killarney Estates during the PM peak hour in the eastbound direction (from W Fairbanks Avenue to US 17-92) using the study area intersections.

The primary direction of cut-through traffic through Killarney Estates is eastbound for, presumably, two major reasons. First, eastbound cut-through traffic avoids excessive delay at the eastbound approach at the intersection of W Fairbanks & US 17-92. Second, eastbound cut-through traffic is not restricted by one-way facilities in Killarney Estates in the same manner westbound cut-through traffic is. Compared to eastbound cut-through activity, westbound cut-through traffic observed was minimal. Overall cut-through behavior is shown in **Figure 2.**

Figure 2: PM Peak Hour Cut-Through Traffic Volumes



Kimley»Horn

Proposed Solutions

As shown in **Figure 3**, the following solutions are proposed to reduce the amount of cut-through traffic through the Killarney Estates neighborhood:

1. **Fairview Avenue Cul-de-sac** – As part of site development at the Henderson Hotel, it is proposed that Fairview Avenue be improved with a cul-de-sac design at the existing intersection of Fairview Avenue & Killarney Drive. Implementation of the cul-de-sac would remove existing connectivity at intersection, thus eliminating the cut-through route from W Fairbanks Avenue to US 17-92 via Killarney Drive. Killarney Drive (Broadview Avenue to the proposed dead end at Fairview Avenue) would be returned to two-way operation to allow for local access. The Fairbanks Avenue Cul-de-sac would be considered a permanent solution to reducing the Killarney Estates neighborhood cut-through route concerns.

2. **Killarney Drive Temporary Closure** – Prior to the more permanent Fairview Avenue Cul-de-sac solution, it is proposed that Killarney Drive be closed as a cut-through route by installing temporary roadway barriers on Killarney Drive between Shoreview Avenue and Broadview Avenue. As with the Fairview Avenue Cul-de-sac, implementation of the barriers would eliminate the cut-through route from W Fairbanks Avenue to US 17-92 via Killarney Drive.

Kimley»Horn

Figure 3: Proposed Solutions

Kimley»Horn

Solutions Analysis

As shown in the **Figure 3**, the proposed solutions would not completely shut off opportunities for cut-through traffic to occur through the Killarney Estates neighborhood. However, it would generally increase length and complexity of cut-through routes, which is anticipated to reduce the amount of traffic willing to divert through the neighborhood, as follows:

- Today, 61% of eastbound cut-through traffic in the peak hour access the neighborhood at Lakeview Avenue (22 eastbound vehicles per hour at Lakeview of 36 eastbound vehicles per hour total) to make a +/-0.4-mile trip to US 17-92 presumably via Killarney Drive. Without Killarney Drive connectivity, the same vehicle would increase the complexity of its cut-through route by adding several turns and add 50% to route distance.
- Today, 39% of eastbound cut-through traffic in the peak hour access the neighborhood at Shoreview Avenue (14 eastbound vehicles per hour at Lakeview of 36 eastbound vehicles per hour total) to make a +/-0.3-mile trip to US 17-92 presumably via Killarney Drive. Without Killarney Drive connectivity, the same vehicle would increase the complexity of its cut-through route by adding several turns and add 25% to route distance.

In addition to the anticipated reduction in eastbound cut-through traffic, the proposed improvements would also reduce safety concerns in the neighborhood. Eliminating the existing one-way control at Killarney Drive from Broadview Avenue and Fairview Avenue (in conjunction with the Fairview Avenue Cul-de-sac) would reduce the potential for head-on collisions with wrong-way drivers. As reported by residents of Killarney Estates, traffic is regularly observed traveling westbound on the eastbound-only segment. Additionally, reducing cut-through traffic on Killarney Drive will reduce the amount of traffic conflicts at the many all-way stop controlled intersections where cut-through vehicles are regularly observed not coming to a complete stop (or in some cases not slowing at all) at stop signs along the route.

Kimley»Horn

Conclusion

Data collected at the primary access points of the residential portions of the Killarney Estates neighborhood show that a significant amount of cut-through traffic occurs during a typical PM peak hour, primarily in the eastbound direction (from W Fairbanks Avenue to US 17-92). To reduce the amount of cut-through traffic through the neighborhood, it is proposed that Fairview Avenue be permanently improved with a cul-de-sac design at the existing intersection of Fairview Avenue & Killarney Drive in conjunction with the development of the Henderson Hotel site. In the interim, it is proposed that Killarney Drive be temporarily closed by installing roadway barriers on Killarney Drive between Shoreview Avenue and Broadview Avenue. These improvements are anticipated to reduce cut-through traffic volumes by increasing the length and complexity of cut-through routes through the neighborhood. The improvements would also reduce safety concerns associated with existing one-way control along a segment of Killarney Drive and cut-through drivers not obeying stop-control at intersections along Killarney Drive.

kimley-horn.com | 189 South Orange Avenue, Suite 1000, Orlando, FL, 32801 | 407 898 1511

ATTACHMENT A
Origin-Destination Data

Prepared by National Data & Surveing Services

Origin and Destination Study

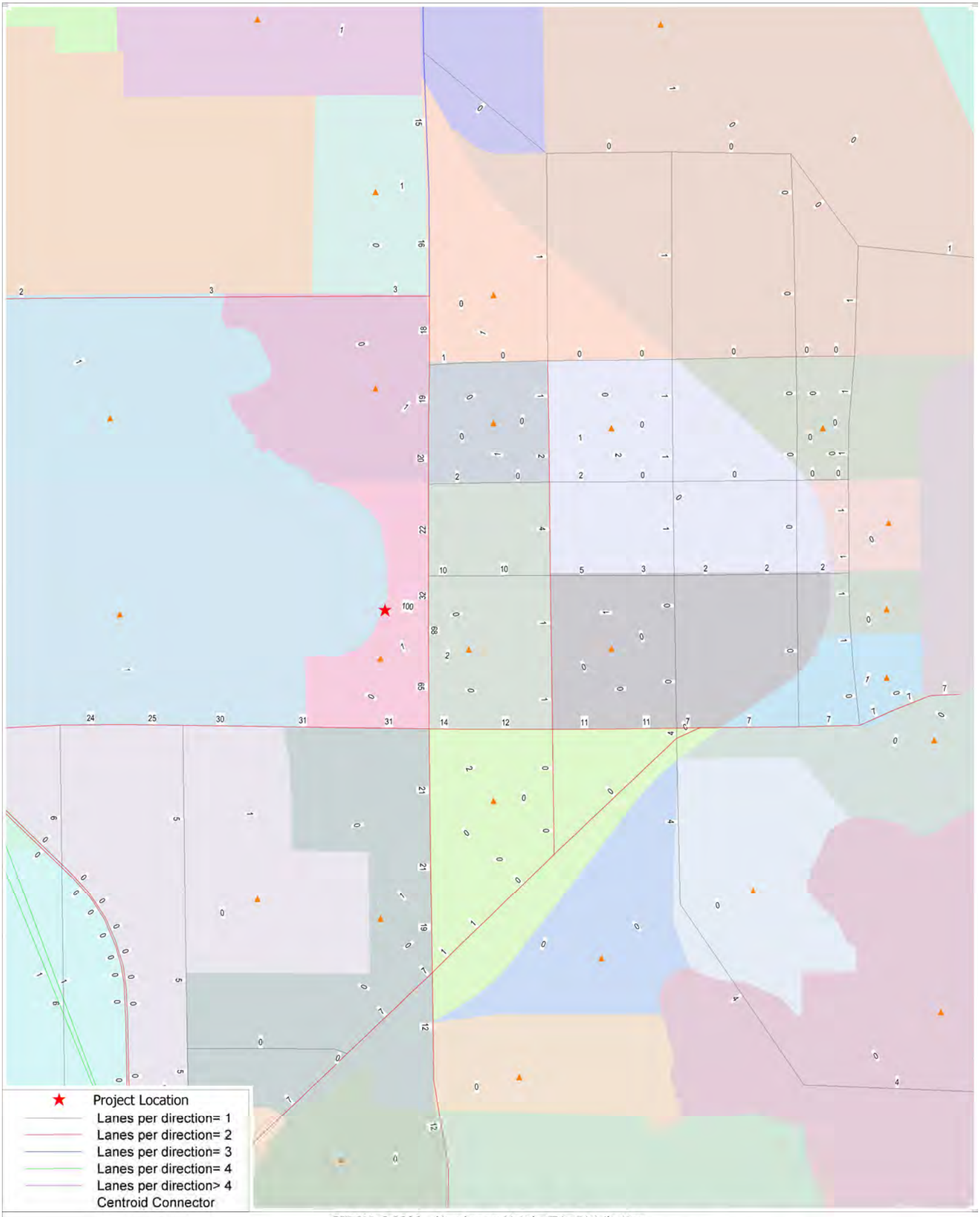
Project: 19-3369
Location: Winter Park, FL

Date: 5/21/2019
Day: Tuesday

Time	1->2	1->3	2->1	2->3	2->4	3->1	3->2	4->1	4->2	4->3	Grand Total
4:00:00 PM										1	1
4:15:00 PM	1	1			1	3		4	1		11
4:30:00 PM							2	5	5	1	13
4:45:00 PM			3	2	1	2	5	1	2	1	17
5:00:00 PM							2	3	1		6
5:15:00 PM						4	4		1		9
5:30:00 PM		1	1	2		3	2	2			11
5:45:00 PM						1	5	4	2	1	13
Grand Total	1	2	4	4	2	13	20	19	12	4	81

ID	Location
1	Vehicles entering & Exiting on Beachview Ave from/to S Orlando Ave
2	Vehicles entering & Exiting on Fairview Ave from/to S Orlando Ave
3	Vehicles Entering & exiting on Shoreview Ave from/to Fairbanks Ave
4	Vehicles Entering & exiting on Lakeview Ave from/to Fairbanks Ave

APPENDIX G
CFRPM Model Plot



CFRPMv6 2020 - Henderson Hotel - Trip Distribution

C:\FSUTMS\D5\CFRPMV61_Daily\Base\CF_2020\Henderson_20\Output\HWYLOAD_C20.NET 5/31/2019

cube

(Licensed to Kimley Horn and Associates Inc)

APPENDIX H
Signal Timings

Phase Parameters

Phases	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20
Walk Time	0	7	0	7	0	7	0	7	0	0	0	0	0	0	0	0	0	0	0	0
Clear Time	0	24	0	26	0	24	0	29	0	0	0	0	0	0	0	0	0	0	0	0
Don't Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	5	15	5	7	5	15	5	7	0	0	0	0	0	0	0	0	0	0	0	0
Passage	3	4	3	3	3	4	3	3	0	0	0	0	0	0	0	0	0	0	0	0
Max-1	30	60	34	45	30	60	34	45	0	0	0	0	0	0	0	0	0	0	0	0
Max-2	25	40	25	30	25	40	25	30	0	0	0	0	0	0	0	0	0	0	0	0
Max-3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Yel Change	4	4	4	4.1	4	4	4.1	4	3	3	3	3	3	3	3	3	3	3	3	3
Red Clear	2	2	2	2.5	2	2	2.1	2	0	0	0	0	0	0	0	0	0	0	0	0
Red Revert	4	4	4	4	4	4	4	4	0	0	0	0	0	0	0	0	0	0	0	0
Added Initial	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Max Initial	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Time B4 Reduce	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Cars B4 Reduce	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Time To Reduce	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Reduce By	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Gap	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dyn Max Limit	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dyn Max Step	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Delayed Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Delay Ped	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Alt Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Alt Ped Clr																				
Pre Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Pre Clearance	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Add Red Clear	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Phases	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40
Walk Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Clear Time	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Don't Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Passage	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Max-1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Max-2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Max-3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Yel Change	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3	3
Red Clear	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Red Revert	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Added Initial	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Max Initial	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Time B4 Reduce	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Cars B4 Reduce	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Time To Reduce	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Reduce By	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Min Gap	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dyn Max Limit	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Dyn Max Step	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Delayed Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Delay Ped	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Alt Walk	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Alt Ped Clr																				

Pre Green	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Pre Clearance	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Add Red Clear	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Phase Options

Phases	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20
Enable	X	X	X	X	X	X	X	X	.	.	.	.	.	.	.	.	.	.	.	.
Auto Flash Ent.	.	.	.	X	.	.	.	X	.	.	.	.	.	.	.	.	.	.	.	.
Auto Flash Exit	.	X	.	.	.	X	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Non Actuated I	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Non Actuated II	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Non Lock Mem	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Min Veh Recall	.	X	.	.	.	X	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Max Veh Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Ped Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Soft Veh Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Dual Entry	.	.	.	X	.	.	.	X	.	.	.	.	.	.	.	.	.	.	.	.
Sim Gap Dis	.	X	.	.	.	X	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Act Rest Walk	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Cond Service	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Phases	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40
Enable	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Auto Flash Ent.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Auto Flash Exit	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Non Actuated I	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Non Actuated II	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Non Lock Mem	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Min Veh Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Max Veh Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Ped Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Soft Veh Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Dual Entry	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Sim Gap Dis	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Act Rest Walk	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Cond Service	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

Additional Phase Options

Phases	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20
Ped Clr During Yel	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Ped Clr During Red	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Cond Reservice	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Yel Min Override	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
No Startup Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Adv. Warn Flasher	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
No Ped Str Up Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Ped Clr OVTG	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Ped Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Min2 Veh Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Min3 Vehicle Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Ped2 Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

Phases	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40
Yel Ped Clr	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Red Clr Ped Clr	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Cond Reservice	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Yel Min Override	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
No Start Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Adv. Warn Flasher	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
No Ped Startup Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
ed Clr During OVTG	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Ped Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Flash Exit Call	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Min2 Veh Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Min3 Vehicle Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.
Ped2 Recall	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.	.

Phase Configuration

Ph.	Startup	Ring	Concurrent	No Served Phases	Phs Min	Description
1	Phase Not On	1	5,6		0	NBL
2	Green No Walk	1	5,6		0	SB
3	Phase Not On	1	8,7		0	EBL
4	Phase Not On	1	8,7		0	WB
5	Phase Not On	2	2,1		0	SBL
6	Green No Walk	2	2,1		0	NB
7	Phase Not On	2	3,4		0	WBL
8	Phase Not On	2	3,4		0	EB
9	None	0			0	
10	None	0			0	
11	None	0			0	
12	None	0			0	
13	None	0			0	
14	None	0			0	
15	None	0			0	
16	None	0			0	
17	None	0			0	
18	None	0			0	
19	None	0			0	
20	None	0			0	
21	None	0			0	
22	None	0			0	
23	None	0			0	
24	None	0			0	
25	None	0			0	
26	None	0			0	
27	None	0			0	
28	None	0			0	
29	None	0			0	
30	None	0			0	
31	None	0			0	
32	None	0			0	
33	None	0			0	
34	None	0			0	
35	None	0			0	
36	None	0			0	

37	None	0			0	
38	None	0			0	
39	None	0			0	
40	None	0			0	

Overlaps

OLP	Type	Included Phases	Modifier Phases	Trail GRN	Trail YEL	Trail RED	Walk 1	Ped Clr 1	Walk 2	Ped Clr 2	Delay	Flash	Descriptions
1	Off			0	0	0	0	0	0	0	0	Off	
2	Off			0	0	0	0	0	0	0	0	Off	
3	Off			0	0	0	0	0	0	0	0	Off	
4	Off			0	0	0	0	0	0	0	0	Off	
5	Off			0	0	0	0	0	0	0	0	Off	
6	Off			0	0	0	0	0	0	0	0	Off	
7	Off			0	0	0	0	0	0	0	0	Off	
8	Off			0	0	0	0	0	0	0	0	Off	
9	Off			0	0	0	0	0	0	0	0	Off	
10	Off			0	0	0	0	0	0	0	0	Off	
11	Off			0	0	0	0	0	0	0	0	Off	
12	Off			0	0	0	0	0	0	0	0	Off	
13	Off			0	0	0	0	0	0	0	0	Off	
14	Off			0	0	0	0	0	0	0	0	Off	
15	Off			0	0	0	0	0	0	0	0	Off	
16	Off			0	0	0	0	0	0	0	0	Off	
17	Off			0	0	0	0	0	0	0	0	Off	
18	Off			0	0	0	0	0	0	0	0	Off	
19	Off			0	0	0	0	0	0	0	0	Off	
20	Off			0	0	0	0	0	0	0	0	Off	
21	Off			0	0	0	0	0	0	0	0	Off	
22	Off			0	0	0	0	0	0	0	0	Off	
23	Off			0	0	0	0	0	0	0	0	Off	
24	Off			0	0	0	0	0	0	0	0	Off	
25	Off			0	0	0	0	0	0	0	0	Off	
26	Off			0	0	0	0	0	0	0	0	Off	
27	Off			0	0	0	0	0	0	0	0	Off	
28	Off			0	0	0	0	0	0	0	0	Off	
29	Off			0	0	0	0	0	0	0	0	Off	
30	Off			0	0	0	0	0	0	0	0	Off	
31	Off			0	0	0	0	0	0	0	0	Off	
32	Off			0	0	0	0	0	0	0	0	Off	

Sequence Configuration

Sequence 1

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 2

Ring	Phases
1	2,1,a,3,4,b
2	5,6,a,8,7,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 3

Ring	Phases
1	1,2,a,4,3,b
2	6,5,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 4

Ring	Phases
1	2,1,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 5

Ring	Phases
1	1,2,a,4,3,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 6

Ring	Phases
1	1,2,a,4,3,b
2	6,5,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 7

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 8

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 9

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,8,7,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	

Sequence 10

Ring	Phases
1	2,1,a,4,3,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	

Sequence 11

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	

Sequence 12

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	

15	
16	

15	
16	

15	
16	

15	
16	

Sequence 13

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 14

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 15

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 16

Ring	Phases
1	1,2,a,3,4,b
2	5,6,a,7,8,b
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 17

Ring	Phases
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 18

Ring	Phases
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 19

Ring	Phases
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Sequence 20

Ring	Phases
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	
11	
12	
13	
14	
15	
16	

Coordination Parameters

Operational Mode	Correction Mode	Maximum Mode	Force Mode	Max Cyc Limit %	Min Cyc Limit %
Automatic	Shortway (Auto)	Max Inhibit	Floating	22	12

Patterns

Patt.	Cycle	Offset 1	Offset 2	Offset 3	Split	Sequence	Ref. Color	Max Mode	Phs Pln	Det Pln	Ped Pln
1	180	149	0	0	1	2	Yellow	Max Inhibit	1	1	1
2	0	0	0	0	2	1	Yellow	Max Inhibit	1	1	1
3	200	28	0	0	3	2	Yellow	Max Inhibit	1	1	1
4	220	29	0	0	4	2	Yellow	Max Inhibit	1	1	1
5	140	18	0	0	5	4	Yellow	Max Inhibit	1	1	1
6	180	67	0	0	6	2	Yellow	Max Inhibit	1	1	1
7	0	0	0	0	7	1	Yellow	Max Inhibit	1	1	1
8	0	0	0	0	8	1	Yellow	Max Inhibit	1	1	1
9	0	0	0	0	9	1	Yellow	Max Inhibit	1	1	1
10	0	0	0	0	10	1	Yellow	Max Inhibit	1	1	1
11	0	0	0	0	11	1	Yellow	Max Inhibit	1	1	1
12	0	0	0	0	12	1	Yellow	Max Inhibit	1	1	1
13	0	0	0	0	13	1	Yellow	Max Inhibit	1	1	1
14	0	0	0	0	14	1	Yellow	Max Inhibit	1	1	1
15	0	0	0	0	15	1	Yellow	Max Inhibit	1	1	1
16	0	0	0	0	16	1	Yellow	Max Inhibit	1	1	1
17	0	0	0	0	17	1	Yellow	Max Inhibit	1	1	1
18	0	0	0	0	18	1	Yellow	Max Inhibit	1	1	1
19	0	0	0	0	19	1	Yellow	Max Inhibit	1	1	1
20	0	0	0	0	20	1	Yellow	Max Inhibit	1	1	1
21	190	136	0	0	21	10	Yellow	Max Inhibit	1	1	1
22	0	0	0	0	22	1	Yellow	Max Inhibit	1	1	1
23	195	190	0	0	23	2	Yellow	Max Inhibit	1	1	1
24	220	127	0	0	24	2	Yellow	Max Inhibit	1	1	1
25	0	0	0	0	25	1	Yellow	Max Inhibit	1	1	1
26	0	0	0	0	26	1	Yellow	Max Inhibit	1	1	1
27	0	0	0	0	27	1	Yellow	Max Inhibit	1	1	1
28	0	0	0	0	28	1	Yellow	Max Inhibit	1	1	1
29	0	0	0	0	29	1	Yellow	Max Inhibit	1	1	1
30	0	0	0	0	30	1	Yellow	Max Inhibit	1	1	1
31	0	0	0	0	31	1	Yellow	Max Inhibit	1	1	1
32	0	0	0	0	32	1	Yellow	Max Inhibit	1	1	1
33	0	0	0	0	33	1	Yellow	Max Inhibit	1	1	1
34	0	0	0	0	34	1	Yellow	Max Inhibit	1	1	1
35	0	0	0	0	35	1	Yellow	Max Inhibit	1	1	1
36	0	0	0	0	36	1	Yellow	Max Inhibit	1	1	1
37	0	0	0	0	37	1	Yellow	Max Inhibit	1	1	1
38	0	0	0	0	28	1	Yellow	Max Inhibit	1	1	1
39	0	0	0	0	39	1	Yellow	Max Inhibit	1	1	1
40	0	0	0	0	40	1	Yellow	Max Inhibit	1	1	1
41	0	0	0	0	41	1	Yellow	Max Inhibit	1	1	1
42	0	0	0	0	42	1	Yellow	Max Inhibit	1	1	1
43	0	0	0	0	43	1	Yellow	Max Inhibit	1	1	1
44	0	0	0	0	44	1	Yellow	Max Inhibit	1	1	1
45	0	0	0	0	45	1	Yellow	Max Inhibit	1	1	1
46	0	0	0	0	46	1	Yellow	Max Inhibit	1	1	1
47	0	0	0	0	47	1	Yellow	Max Inhibit	1	1	1
48	0	0	0	0	48	1	Yellow	Max Inhibit	1	1	1

49	0	0	0	0	49	1	Yellow	Max Inhibit	1	1	1
50	0	0	0	0	50	1	Yellow	Max Inhibit	1	1	1
51	0	0	0	0	51	1	Yellow	Max Inhibit	1	1	1
52	0	0	0	0	52	1	Yellow	Max Inhibit	1	1	1
53	0	0	0	0	53	1	Yellow	Max Inhibit	1	1	1
54	0	0	0	0	54	1	Yellow	Max Inhibit	1	1	1
55	0	0	0	0	55	1	Yellow	Max Inhibit	1	1	1
56	0	0	0	0	56	1	Yellow	Max Inhibit	1	1	1
57	0	0	0	0	57	1	Yellow	Max Inhibit	1	1	1
58	0	0	0	0	58	1	Yellow	Max Inhibit	1	1	1
59	0	0	0	0	59	1	Yellow	Max Inhibit	1	1	1
60	0	0	0	0	60	1	Yellow	Max Inhibit	1	1	1
61	0	0	0	0	61	1	Yellow	Max Inhibit	1	1	1
62	0	0	0	0	62	1	Yellow	Max Inhibit	1	1	1
63	0	0	0	0	63	1	Yellow	Max Inhibit	1	1	1
64	0	0	0	0	64	1	Yellow	Max Inhibit	1	1	1
65	0	0	0	0	65	1	Yellow	Max Inhibit	1	1	1
66	0	0	0	0	66	1	Yellow	Max Inhibit	1	1	1
67	0	0	0	0	67	1	Yellow	Max Inhibit	1	1	1
68	0	0	0	0	68	1	Yellow	Max Inhibit	1	1	1
69	0	0	0	0	69	1	Yellow	Max Inhibit	1	1	1
70	0	0	0	0	70	1	Yellow	Max Inhibit	1	1	1
71	0	0	0	0	71	1	Yellow	Max Inhibit	1	1	1
72	0	0	0	0	72	1	Yellow	Max Inhibit	1	1	1
73	0	0	0	0	73	1	Yellow	Max Inhibit	1	1	1
74	0	0	0	0	74	1	Yellow	Max Inhibit	1	1	1
75	0	0	0	0	75	1	Yellow	Max Inhibit	1	1	1
76	0	0	0	0	76	1	Yellow	Max Inhibit	1	1	1
77	0	0	0	0	77	1	Yellow	Max Inhibit	1	1	1
78	0	0	0	0	78	1	Yellow	Max Inhibit	1	1	1
79	0	0	0	0	79	1	Yellow	Max Inhibit	1	1	1
80	0	0	0	0	80	1	Yellow	Max Inhibit	1	1	1
81	0	0	0	0	81	1	Yellow	Max Inhibit	1	1	1
82	0	0	0	0	82	1	Yellow	Max Inhibit	1	1	1
83	0	0	0	0	83	1	Yellow	Max Inhibit	1	1	1
84	0	0	0	0	84	1	Yellow	Max Inhibit	1	1	1
85	0	0	0	0	85	1	Yellow	Max Inhibit	1	1	1
86	0	0	0	0	86	1	Yellow	Max Inhibit	1	1	1
87	0	0	0	0	87	1	Yellow	Max Inhibit	1	1	1
88	0	0	0	0	88	1	Yellow	Max Inhibit	1	1	1
89	0	0	0	0	89	1	Yellow	Max Inhibit	1	1	1
90	0	0	0	0	90	1	Yellow	Max Inhibit	1	1	1
91	0	0	0	0	91	1	Yellow	Max Inhibit	1	1	1
92	0	0	0	0	92	1	Yellow	Max Inhibit	1	1	1
93	0	0	0	0	93	1	Yellow	Max Inhibit	1	1	1
94	0	0	0	0	94	1	Yellow	Max Inhibit	1	1	1
95	0	0	0	0	95	1	Yellow	Max Inhibit	1	1	1
96	0	0	0	0	96	1	Yellow	Max Inhibit	1	1	1
97	0	0	0	0	97	1	Yellow	Max Inhibit	1	1	1
98	0	0	0	0	98	1	Yellow	Max Inhibit	1	1	1
99	0	0	0	0	99	1	Yellow	Max Inhibit	1	1	1
100	220	128	0	0	100	3	Yellow	Max Inhibit	1	1	1
101	220	128	0	0	101	3	Yellow	Max Inhibit	1	1	1
102	220	128	0	0	102	5	Yellow	Max Inhibit	1	1	1

103	0	0	0	0	103	1	Yellow	Max Inhibit	1	1	1
104	0	0	0	0	104	1	Yellow	Max Inhibit	1	1	1
105	0	0	0	0	105	1	Yellow	Max Inhibit	1	1	1
106	0	0	0	0	106	1	Yellow	Max Inhibit	1	1	1
107	0	0	0	0	107	1	Yellow	Max Inhibit	1	1	1
108	0	0	0	0	108	1	Yellow	Max Inhibit	1	1	1
109	0	0	0	0	109	1	Yellow	Max Inhibit	1	1	1
110	0	0	0	0	110	1	Yellow	Max Inhibit	1	1	1
111	0	0	0	0	111	1	Yellow	Max Inhibit	1	1	1
112	0	0	0	0	112	1	Yellow	Max Inhibit	1	1	1
113	0	0	0	0	113	1	Yellow	Max Inhibit	1	1	1
114	0	0	0	0	114	1	Yellow	Max Inhibit	1	1	1
115	0	0	0	0	115	1	Yellow	Max Inhibit	1	1	1
116	0	0	0	0	116	1	Yellow	Max Inhibit	1	1	1
117	0	0	0	0	117	1	Yellow	Max Inhibit	1	1	1
118	0	0	0	0	118	1	Yellow	Max Inhibit	1	1	1
119	0	0	0	0	119	1	Yellow	Max Inhibit	1	1	1
120	0	0	0	0	120	1	Yellow	Max Inhibit	1	1	1
121	0	0	0	0	121	1	Yellow	Max Inhibit	1	1	1
122	0	0	0	0	122	1	Yellow	Max Inhibit	1	1	1
123	0	0	0	0	123	1	Yellow	Max Inhibit	1	1	1
124	0	0	0	0	124	1	Yellow	Max Inhibit	1	1	1
125	0	0	0	0	125	1	Yellow	Max Inhibit	1	1	1
126	0	0	0	0	126	1	Yellow	Max Inhibit	1	1	1
127	0	0	0	0	127	1	Yellow	Max Inhibit	1	1	1
128	0	0	0	0	128	1	Yellow	Max Inhibit	1	1	1

Split Parameters

Split 1		Coord	Ref	
PH.	Time	PH	PH	Mode
1	25	.	.	None
2	65	X	.	None
3	38	.	.	None
4	52	.	.	Max Veh Recall
5	20	.	.	None
6	70	X	X	None
7	20	.	.	None
8	70	.	.	Max Veh Recall
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 2		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 3		Coord	Ref	
PH.	Time	PH	PH	Mode
1	32	.	.	None
2	73	X	.	None
3	35	.	.	None
4	60	.	.	None
5	22	.	.	None

Split 4		Coord	Ref	
PH.	Time	PH	PH	Mode
1	30	.	.	None
2	90	X	.	None
3	43	.	.	None
4	57	.	.	None
5	28	.	.	None

6	83	X	X	None
7	25	.	.	None
8	70	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

6	92	X	X	None
7	25	.	.	None
8	75	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 5		Coord	Ref	
PH.	Time	PH	PH	Mode
1	18	.	.	None
2	55	X	.	None
3	27	.	.	None
4	40	.	.	None
5	18	.	.	None
6	55	X	X	None
7	24	.	.	None
8	43	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 6		Coord	Ref	
PH.	Time	PH	PH	Mode
1	30	.	.	None
2	70	X	.	None
3	35	.	.	None
4	45	.	.	None
5	30	.	.	None
6	70	X	X	None
7	35	.	.	None
8	45	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 7		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 8		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 9		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None

Split 10		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None

3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None

3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None

Split 9		Coord	Ref	
PH.	Time	PH	PH	Mode
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 10		Coord	Ref	
PH.	Time	PH	PH	Mode
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 11		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 12		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 13		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None

Split 14		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0		.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None

16	0	.	.	None

16	0	.	.	None

Split 15		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 16		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 17		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 18		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None
10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Split 19		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None

Split 20		Coord	Ref	
PH.	Time	PH	PH	Mode
1	0	.	.	None
2	0	.	.	None
3	0	.	.	None
4	0	.	.	None
5	0	.	.	None
6	0	.	.	None

Split 19		Coord	Ref	
PH.	Time	PH	PH	Mode
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None

Split 20		Coord	Ref	
PH.	Time	PH	PH	Mode
7	0	.	.	None
8	0	.	.	None
9	0	.	.	None

10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

10	0	.	.	None
11	0	.	.	None
12	0	.	.	None
13	0	.	.	None
14	0	.	.	None
15	0	.	.	None
16	0	.	.	None

Ring	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16
Offset																

Prioritor Settings

Prioritor	Priority Ph	Output Dly
1		0
2		20
3		0
4		15
5		0
6		0
7		0
8		0

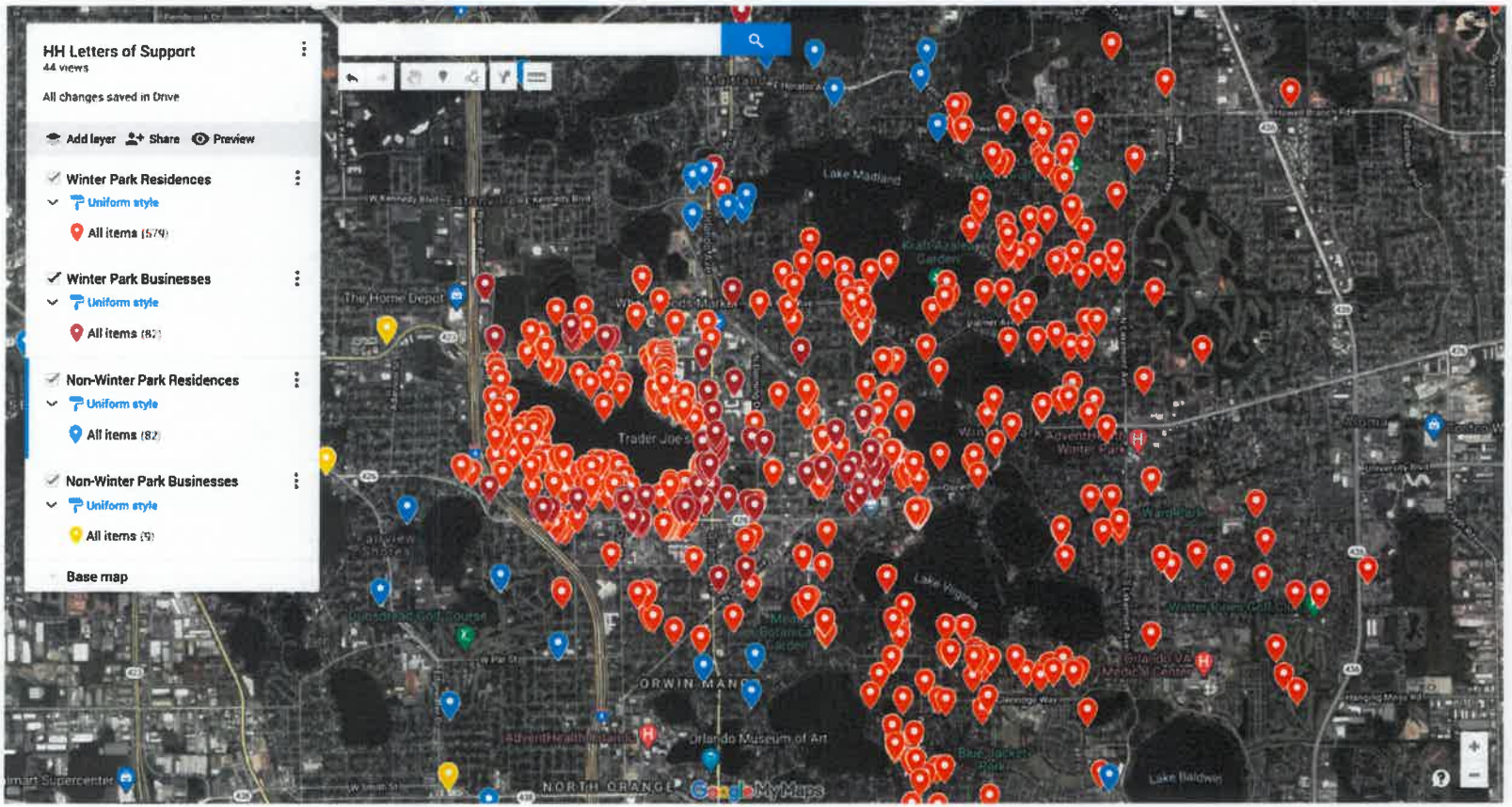
Enabled	Lock Out Time
	0

Henderson Hotel at Lake Killarney – Signed Letters of Support

Enclosed are three sets of signed letters of support:

1. Winter Park residents and residential property owners (more than 575 signed letters)
2. Winter Park businesses and commercial property owners (more than 80 signed letters)
3. Other supporters outside of Winter Park (more than 90 signed letters)

The Map of our Supporters (as of 12/22/2020):



Planning & TRANSPORTATION

401 South Park Avenue • Winter Park, Florida 32789

407-599-3324 • planning@cityofwinterpark.org
cityofwinterpark.org

Planning & Zoning Board
Staff Report for January 5, 2021 Meeting

ANNEX #21-01; CPA 21-01; RZ 21-01 and CU #21-01 Request of the Magruder Eye Institute: to Annex the Properties at 279/283 Orange Terrace Drive; 450/460/470 Cambridge Blvd. and 2310 Devon Court and establish Parking Lot (PL) Future Land Use and Zoning and for Conditional Use approval to build a two-story, 32,000 sq. ft. medical office building on the combined properties including 2245 W. Fairbanks Avenue.

Description of the Requests:

This public hearing is to consider four requests made by the Magruder Eye Institute for 1) annexation of 279/283 Orange Terrace Drive; 450/460/470 Cambridge Blvd. and 2310 Devon Court; 2) establishing Parking Lot future land use and 3) Parking Lot (PL) zoning on those properties and 4) Conditional Use approval to build a two-story, 32,000 sq. ft. medical office building on the combined properties including the former Skycraft business at 2245 W. Fairbanks Avenue.

Background:

Skycraft Surplus has been at 2245 W. Fairbanks Avenue since 1974. Over the years, the Skycraft owners purchased the four adjacent rental homes with the idea that someday they might want to rebuild a larger store and would need more land for the new building and parking. However, Skycraft has decided to transition to more of an on-line platform and about a year ago listed their combined properties for sale. The applicant, the Magruder Eye Institute has those collective properties under contract with the desire to build a new 32,000 square foot medical building in order to move from their current leased facilities at 1911 Mills Avenue to ownership at this new location. To achieve that result, they have also added two other adjacent residential properties to their redevelopment proposal.

Request for Annexation and to Establish Parking Lot Zoning

In order to utilize this combined redevelopment site, the applicants are seeking annexation of 279/283 Orange Terrace Drive; 450/460/470 Cambridge Blvd. and 2310 Devon Court and for the City to establish, a Parking Lot future land use designation and Parking Lot (PL) zoning on those annexed properties. This would allow for the proposed redevelopment of a new two-story, 32,000 square foot medical office building on the commercially zoned frontage at 2245 West Fairbanks Avenue, where the Skycraft building now is located with the associated required patient parking to the rear and employee parking across the street. In order for the entire project area to be annexed into the City, there is also included the small portion of the right-of-way of Cambridge Blvd. within the area to be annexed.

Comprehensive Plan/Zoning Code Conformance

Redevelopment of this West Fairbanks gateway corridor has been a long-term project pursued by the City Commissions over the past 15 years. The first step was the annexation of this corridor 15 years ago and then over time, major investments in the public infrastructure. The City has invested over $20 million to install a sanitary sewer system along this West Fairbanks corridor in order to allow for redevelopment. The City has done repaving, installed new mast arm traffic signals and street lights. The City has just recently completed the project to underground the electric lines (both the major transmission and service lines).

All of this financial investment along with the removal of several billboards has been done to help encourage the redevelopment of this gateway corridor. Like it or not, the City's image is defined by this major gateway entrance and the transformation of that entry experience to a quality experience is the long-term goal of the City.

To that end, redevelopment must occur along this corridor. That redevelopment will not occur along the north side of the corridor unless owners can increase their values with new buildings. In this particular instance, the Skycraft building sits on a parcel that is only 131 feet deep. Redevelopment cannot happen without parking for the new development which must occur on the residentially zoned properties to the rear.

Protections for the Adjacent Residential Neighborhood

The Comprehensive Plan contains the policy (below) that specifically encourages redevelopment on the North side of West Fairbanks Avenue in the manner as dictated by the City and in full compliance by the applicant. This Comprehensive Plan policy mandates the protections and screening necessary to protect the adjacent residential homes. This includes a six-foot brick perimeter wall with column/cap and a landscape program outside the wall to soften that appearance including street trees to buffer the view of the building. Dumpsters must be up by the buildings. Lighting is low profile poles with shielding for downlighting only. There can be no driveway curb cuts near the neighborhood with the driveways as close to Fairbanks avenue as possible. All traffic then is intended to come and gos via the side street of Cambridge Blvd.

> **Policy 1-L-11: Provide for & Encourage the Redevelopment of the North Side of Fairbanks Avenue** Given the shallow lot depths on the north side of Fairbanks Avenue, the City shall consider the annexation of properties to the rear/north and land use designations for parking usage provided that the parking is screened from view by a brick wall, landscape, and ligustrum tree buffer per the template provided by the City; that no driveways are permitted that allow access onto the rear streets and that any lighting has no negative impact on nearby homes.

Medical Office Building Conditional Use Request

The specific request by the Magruder Eye Institute anticipates a two-story, 32,000 medical office building. Parking (one per 200 sf for medical use) and setbacks are provided to meet code. No variances are requested. The building itself is a 40% FAR, which does not count the land area of the parking lot across the street. This FAR reflects the land needed to meet the medical parking requirements of one space per 200 sf versus the general business office requirement of one space for each 250 sf. The building height, other than for rooftop elevator, mechanical and architectural elements is 30 feet. The building is contemporary or modern architectural style which is typical for the medical industry. There is ample articulation to break up the mass of the walls except for the east facing wall where staff would ask for some added architectural styling.

The plans submitted include the commitments, as described above, for the brick perimeter wall, landscape buffering and the other design requirements for the parking lot. If you are driving in or out via Orange Terrace Drive, you will not see the parking lot at all. You will see a brick wall with landscaping and street trees on the exterior of the wall. If you are driving in or out via Cambridge Blvd. you will not see the parking lots unless you look down the driveways which are within 150 feet of Fairbanks Avenue where the existing driveway to the Skycraft parking lot now exists.

It is important to note that several driveways are being removed along the Fairbanks Avenue frontage. In addition, the applicant is partnering with the City to improve the pedestrian sidewalk along Fairbanks to add a wider sidewalk, oak trees and a landscape pedestrian safety zone by pulling the sidewalk back from the edge of the roadway.

Traffic Impacts

This redevelopment from a 6,000 square foot retail store and seven single family homes to a 32,000 square foot medical office will generate some additional traffic over the volumes experienced today, but not as much as one would think. A traffic study has been provided by the applicant which indicates that 116 more cars per day will visit this site. The daily traffic generation increase is not that large because retail stores, like Skycraft have a higher volume of customers than most other types of uses.

The applicant indicates that the patient base for the Eye Institute is drawn from across the Orlando metro area. As such, the predominate and overwhelming volume of daily traffic will be coming from and going to Interstate-4 or going to/from the east on Fairbanks/Aloma or going to/from the west on Fairbanks Avenue. The driving direction websites will direct traffic to the I-4/Fairbanks intersection as the site is addressed at 2245 W. Fairbanks Avenue. It is true that some patients may discover an alternative route through the neighborhood to Wymore Road just as some percentage of Skycraft customers have also discovered that route. This is just a fraction of the 116 cars per weekday and spread over the length of the day. The City can to some degree discourage the exiting of traffic toward Wymore Road through the design of the driveway exit.

There is however, a significant bottleneck on Cambridge Blvd. for those vehicles wanting to exit turning left onto Fairbanks Avenue due to the backup que of cars from the I-4 traffic lights. The solution is to open up a driveway onto Orange Terrace Drive so that vehicles can exit out to Fairbanks and safely make those left turns.

Alternatives to Consider

When there is opposition to a project, it is often helpful to think about the alternatives that are achievable. Skycraft is a 6,000 sq. ft building that is easily converted to a full-service restaurant. If only one or two of these residential lots were rezoned for parking then the site would have parking for a 250+ plus seat restaurant. The upside for the neighborhood is that the Eye Institute is almost exclusively weekday and daytime operations. At nights and on weekends there is no traffic. A restaurant however, is lunch and dinner and after-hours lounge activity. It is seven days a week and would generate traffic for greater number of hours**.** Skycraft being redeveloped into another retail store is not a likely option. The options tend to be medical office or restaurant in today's economic marketplace.

Summary and Recommendation

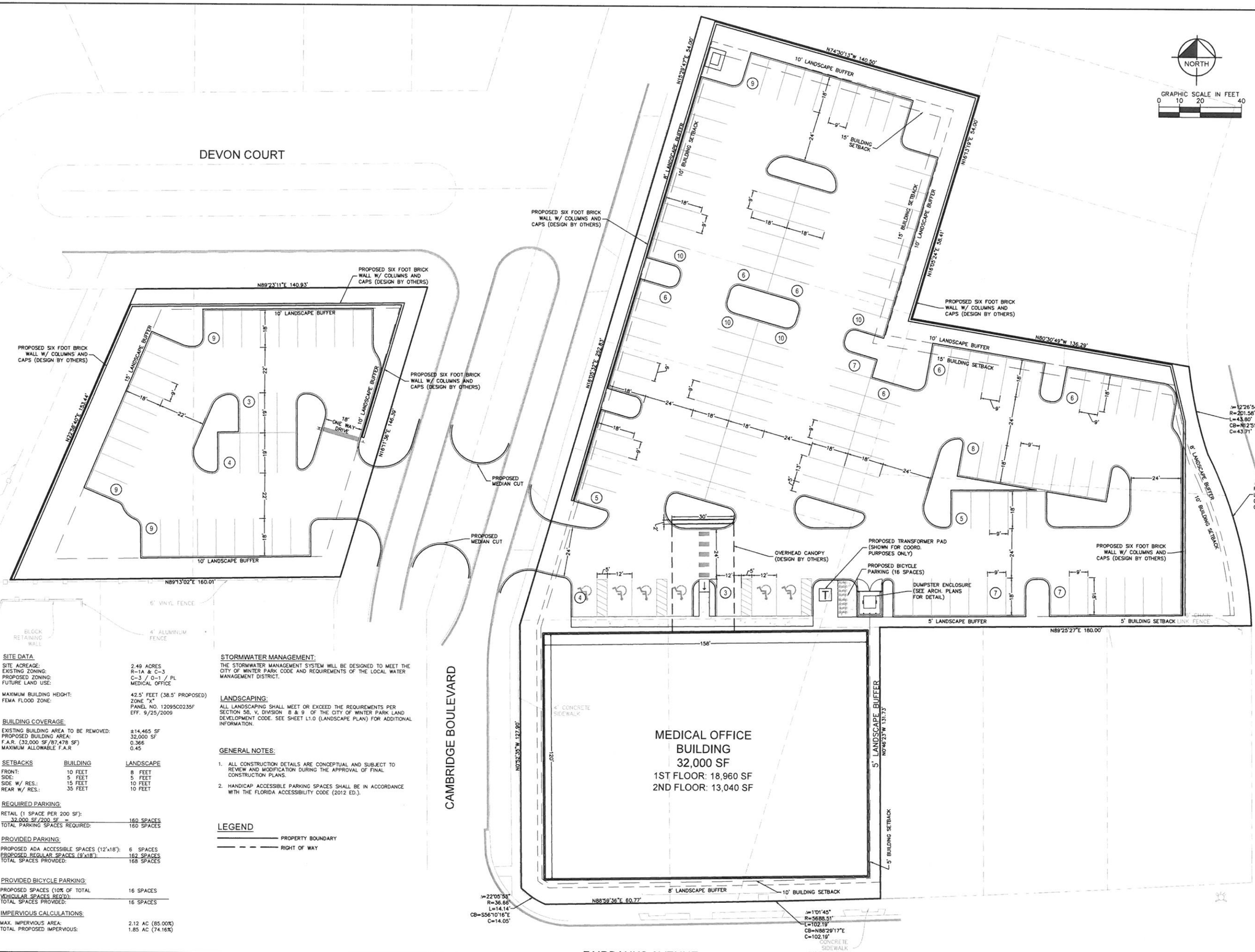
This project will continue upon the template set by the Paco's redevelopment for the redevelopment desired by the City along the North side of the West Fairbanks Avenue corridor. The planning staff is very pleased to have this project pave the way for others at the very entrance to this gateway corridor adjacent to interstate Four.

All of the design elements required by the City for the rear parking lot to be harmonious with the adjacent residential properties have been implemented. The building will be a great addition to the street image and again set the tone for redevelopment along the corridor.

Staff recommendation is for approval of all of the requests with the following conditions related to the conditional use:

1.That the applicant partner with the City to implement a landscape pedestrian safety zone along the Fairbanks Avenue frontage, provide a pedestrian easement for the sidewalk improvements to be implemented along the Fairbanks Avenue frontage and add some architectural styling to the east facing wall.

2. That modifications be made to the driveway design to discourage traffic exiting onto Cambridge Blvd. heading south toward Wymore Road and open a driveway onto Orange Terrace Drive to facilitate exiting onto W. Fairbanks Avenue.



SITE DATA

SITE ACREAGE:	2.49 ACRES
EXISTING ZONING:	R-1A & C-3
PROPOSED ZONING:	C-3 / O-1 / PL
FUTURE LAND USE:	MEDICAL OFFICE
MAXIMUM BUILDING HEIGHT:	42.5' FEET (38.5' PROPOSED)
FEMA FLOOD ZONE:	ZONE "X" PANEL NO. 12095C0235F EFF. 9/25/2009

BUILDING COVERAGE:

EXISTING BUILDING AREA TO BE REMOVED:	±14,465 SF
PROPOSED BUILDING AREA:	32,000 SF
F.A.R. (32,000 SF/87,478 SF)	0.366
MAXIMUM ALLOWABLE F.A.R	0.45

SETBACKS	BUILDING	LANDSCAPE
FRONT:	10 FEET	8 FEET
SIDE:	5 FEET	5 FEET
SIDE W/ RES.:	15 FEET	10 FEET
REAR W/ RES.:	35 FEET	10 FEET

REQUIRED PARKING:

RETAIL (1 SPACE PER 200 SF):	
32,000 SF/200 SF =	160 SPACES
TOTAL PARKING SPACES REQUIRED:	160 SPACES

PROVIDED PARKING:

PROPOSED ADA ACCESSIBLE SPACES (12'x18'):	6 SPACES
PROPOSED REGULAR SPACES (9'x18'):	162 SPACES
TOTAL SPACES PROVIDED:	168 SPACES

PROVIDED BICYCLE PARKING:

PROPOSED SPACES (10% OF TOTAL VEHICULAR SPACES REQ'D):	16 SPACES
TOTAL SPACES PROVIDED:	16 SPACES

IMPERVIOUS CALCULATIONS:

MAX. IMPERVIOUS AREA:	2.12 AC (85.00%)
TOTAL PROPOSED IMPERVIOUS:	1.85 AC (74.16%)

STORMWATER MANAGEMENT:

THE STORMWATER MANAGEMENT SYSTEM WILL BE DESIGNED TO MEET THE CITY OF WINTER PARK CODE AND REQUIREMENTS OF THE LOCAL WATER MANAGEMENT DISTRICT.

LANDSCAPING:

ALL LANDSCAPING SHALL MEET OR EXCEED THE REQUIREMENTS PER SECTION 58, V, DIVISION 8 & 9 OF THE CITY OF WINTER PARK LAND DEVELOPMENT CODE. SEE SHEET L1.0 (LANDSCAPE PLAN) FOR ADDITIONAL INFORMATION.

GENERAL NOTES:

1. ALL CONSTRUCTION DETAILS ARE CONCEPTUAL AND SUBJECT TO REVIEW AND MODIFICATION DURING THE APPROVAL OF FINAL CONSTRUCTION PLANS.
2. HANDICAP ACCESSIBLE PARKING SPACES SHALL BE IN ACCORDANCE WITH THE FLORIDA ACCESSIBILITY CODE (2012 ED.).

LEGEND

PROPERTY BOUNDARY

RIGHT OF WAY

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2020 KIMLEY-HORN AND ASSOCIATES, INC.

189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER
76118
DATE: 10/28/2020

KHA PROJECT 049610000
DATE 11/02/2020
SCALE AS SHOWN
DESIGNED BY JJS
DRAWN BY EGS
CHECKED BY RMG

SITE PLAN

MAGRUDER EYE INSTITUTE WP

WINTER PARK FLORIDA

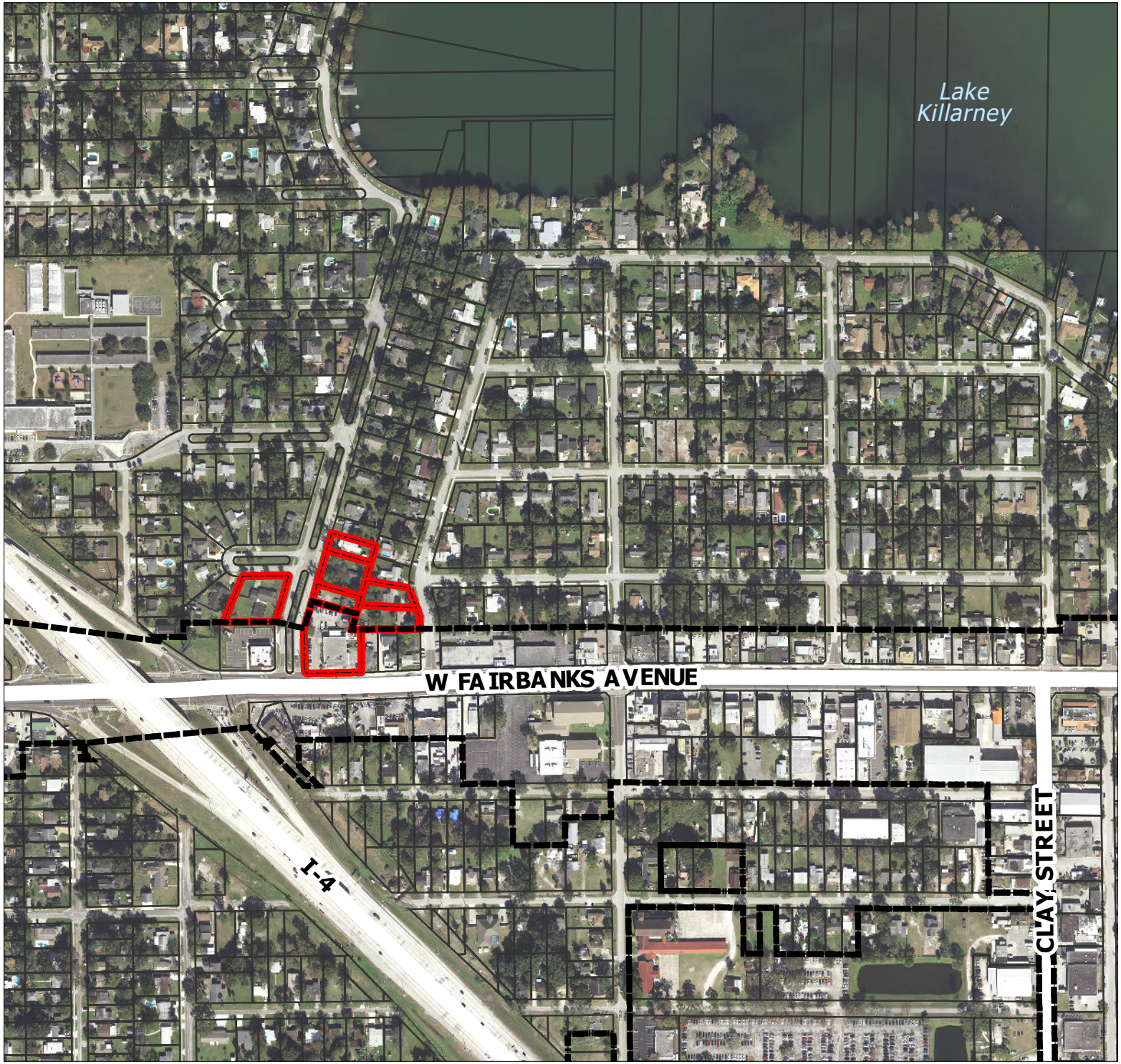
SHEET NUMBER
DP1.0

Plotted By:Gonzalez, Randy Sheet Set:Magruder Eye Institute WP-DP Layout:DP1.0 November 09, 2020 04:24:18pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CADD\DEVELOPMENT\PlanSheets\DP1.0-SITE.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

LOCATION MAP

Proposed Magruder Eye Institute

City of Winter Park
Florida



N
W E
S

LOCATION MAP

Proposed Magruder Eye Institute

City of Winter Park
Florida

ANNEXATION MAP

Proposed Magruder Eye Institute

City of Winter Park
Florida

Corner Street View

© ECKERT WORDELL LLC

Parking Lot View

Magruder Eye Institute

ECKERT WORDELL

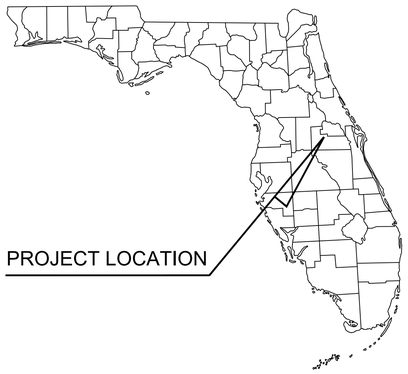
© ECKERT WORDELL LLC

Inside Parking Lot View

Magruder Eye Institute

ECKERT WORDELL

© ECKERT WORDELL LLC

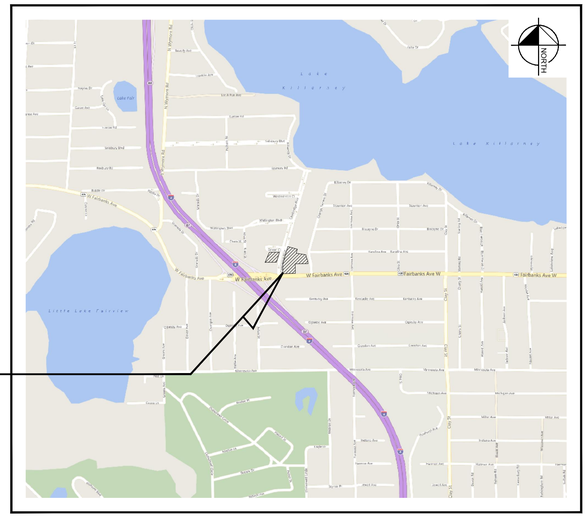


MAGRUDER EYE INSTITUTE OF WINTER PARK

CONDITIONAL USE

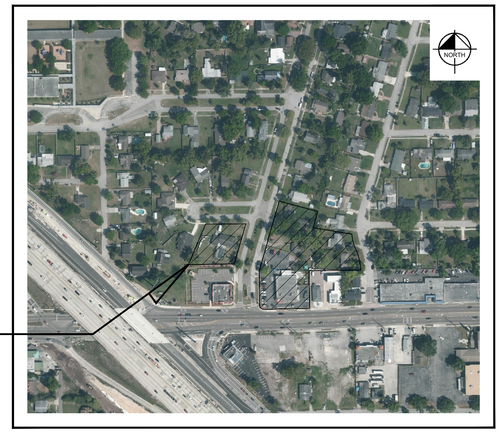
PARCEL ID #'S: 02-22-29-2996-01-010, 02-22-29-2996-01-040,
02-22-29-2996-01-050, 02-22-29-2996-02-060, 02-22-29-2996-02-069
11-22-29-6260-05-020 & 11-22-29-6260-05-010

2245 W. FAIRBANKS AVENUE
WINTER PARK, FLORIDA
NOVEMBER 2, 2020



SECTION 02, TOWNSHIP 22S, RANGE 29E
SECTION 11, TOWNSHIP 22S, RANGE 29E

VICINITY MAP



AERIAL PHOTOGRAPH
N.T.S.

PROJECT TEAM

OWNER/DEVELOPER
MAGRUDER EYE INSTITUTE
1911 NORTH MILLS AVENUE
ORLANDO, FL 32803
CONTACT: CHAD TOMLINSON
PHONE: (407) 893-8235
EMAIL: Chad.Tomlinson@MagruderEye.com

ENGINEER
KIMLEY HORN AND ASSOCIATES
189 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: JENNIFER J. STICKLER P.E.
PHONE: (407) 898-1511
EMAIL: Jennifer.Stickler@kimley-horn.com

SURVEYOR
L&S DIVERSIFIED
489 STATE ROAD 436, SUITE 117
CASSELBERRY, FL 32707
CONTACT: JUSTIN MANOR
PHONE: (407) 681-3836
EMAIL: justin.manor@LSsurveyor.com

PREPARED BY
Kimley»Horn

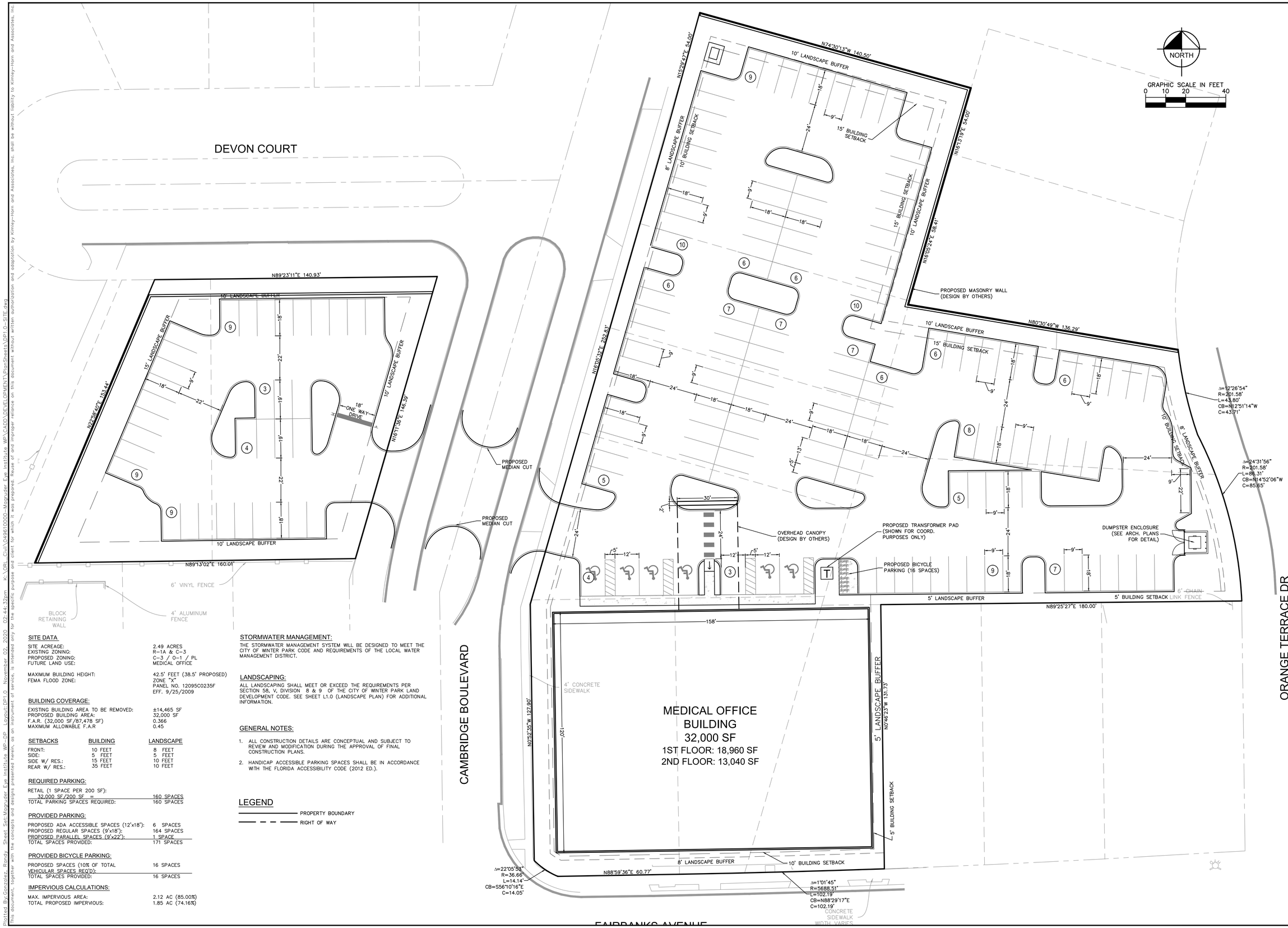
© 2020 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. Orange Ave., Suite 1000, Orlando, FL 32801
Phone: (407) 898-1511
WWW.KIMLEY-HORN.COM CA 00000696

SHEET INDEX

DP0.0	COVER
DP1.0	SITE PLAN
DP2.0	PAVING, GRADING AND DRAINAGE PLAN
DP3.0	UTILITY PLAN
L0.50	TREE MITIGATION PLAN
L0.51	TREE MITIGATION SPECIFICATIONS
L1.00	LANDSCAPE PLAN
L1.50	LANDSCAPE DETAILS
L1.51	LANDSCAPE SPECIFICATIONS
S-1	SURVEY

Drawing name: K:\ORL_Civil\049610000-Magruder Eye Institute WP\CADD\DEVELOPMENT\PlanSheets\DP0.0-COVER.dwg DP0.0 Nov 02, 2020 4:03pm by: Randy.Gonzalez
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

Plotted By: Gonzalez, Randy Sheet Set: Magruder Eye Institute WP_DP Layout: DP1.0 November 02, 2020 02:44:32pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CADD\DEVELOPMENT\PlanSheets\DP1.0-SITE.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



SITE DATA

SITE ACREAGE:	2.49 ACRES
EXISTING ZONING:	R-1A & C-3
PROPOSED ZONING:	C-3 / O-1 / PL
FUTURE LAND USE:	MEDICAL OFFICE
MAXIMUM BUILDING HEIGHT:	42.5' FEET (38.5' PROPOSED)
FEMA FLOOD ZONE:	ZONE "X" PANEL NO. 12095C0235F EFF. 9/25/2009

BUILDING COVERAGE:

EXISTING BUILDING AREA TO BE REMOVED:	±14,465 SF
PROPOSED BUILDING AREA:	32,000 SF
F.A.R. (32,000 SF/87,478 SF)	0.366
MAXIMUM ALLOWABLE F.A.R	0.45

SETBACKS

	BUILDING	LANDSCAPE
FRONT:	10 FEET	8 FEET
SIDE:	5 FEET	5 FEET
SIDE W/ RES.:	15 FEET	10 FEET
REAR W/ RES.:	35 FEET	10 FEET

REQUIRED PARKING:

RETAIL (1 SPACE PER 200 SF):	
32,000 SF/200 SF =	160 SPACES
TOTAL PARKING SPACES REQUIRED:	160 SPACES

PROVIDED PARKING:

PROPOSED ADA ACCESSIBLE SPACES (12'x18'):	6 SPACES
PROPOSED REGULAR SPACES (9'x18'):	164 SPACES
PROPOSED PARALLEL SPACES (9'x22'):	1 SPACE
TOTAL SPACES PROVIDED:	171 SPACES

PROVIDED BICYCLE PARKING:

PROPOSED SPACES (10% OF TOTAL VEHICULAR SPACES REQ'D):	16 SPACES
TOTAL SPACES PROVIDED:	16 SPACES

IMPERVIOUS CALCULATIONS:

MAX. IMPERVIOUS AREA:	2.12 AC (85.00%)
TOTAL PROPOSED IMPERVIOUS:	1.85 AC (74.16%)

STORMWATER MANAGEMENT:

THE STORMWATER MANAGEMENT SYSTEM WILL BE DESIGNED TO MEET THE CITY OF WINTER PARK CODE AND REQUIREMENTS OF THE LOCAL WATER MANAGEMENT DISTRICT.

LANDSCAPING:

ALL LANDSCAPING SHALL MEET OR EXCEED THE REQUIREMENTS PER SECTION 58, V, DIVISION 8 & 9 OF THE CITY OF WINTER PARK LAND DEVELOPMENT CODE. SEE SHEET L1.0 (LANDSCAPE PLAN) FOR ADDITIONAL INFORMATION.

GENERAL NOTES:

1. ALL CONSTRUCTION DETAILS ARE CONCEPTUAL AND SUBJECT TO REVIEW AND MODIFICATION DURING THE APPROVAL OF FINAL CONSTRUCTION PLANS.
2. HANDICAP ACCESSIBLE PARKING SPACES SHALL BE IN ACCORDANCE WITH THE FLORIDA ACCESSIBILITY CODE (2012 ED.).

LEGEND

PROPERTY BOUNDARY

RIGHT OF WAY

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2020 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER
76118
DATE: 10/28/2020

KHA PROJECT 049610000
DATE 11/02/2020
SCALE AS SHOWN
DESIGNED BY JJS
DRAWN BY EGS
CHECKED BY RMG

SITE PLAN

MAGRUDER EYE INSTITUTE WP

WINTER PARK, FLORIDA

SHEET NUMBER
DP1.0

DEVON COURT

EXFILTRATION TRENCH

EXFILTRATION TRENCH

EXFILTRATION TRENCH

±25LF TRENCH OUTFALL

OUTFALL POP OFF

MEDICAL OFFICE BUILDING
32,000 SF
1ST FLOOR: 18,960 SF
2ND FLOOR: 13,040 SF
FFE = 99.00

CAMBRIDGE BOULEVARD

ORANGE TERRACE DR

FAIRBANKS AVENUE

4' ALUMINUM FENCE

6' VINYL FENCE

BLOCK RETAINING WALL

4' CONCRETE SIDEWALK

6' CHAIN LINK FENCE

CONCRETE SIDEWALK WITH VARIES

DRAINAGE NOTES:

THE SIZE LOCATION AND POINTS OF CONNECTION FOR STORM SEWER AS WELL AS GRADING OF THE SITE IS SUBJECT TO CHANGE DURING FINAL ENGINEERING REVIEW.

NOTES

1. ALL CONSTRUCTION DETAILS ARE CONCEPTUAL AND SUBJECT TO REVIEW AND MODIFICATION DURING THE APPROVAL OF FINAL CONSTRUCTION PLANS.
2. STORMWATER DESIGN WILL MEET THE CITY AND SJRWMD STANDARDS.

LEGEND

- PROPERTY BOUNDARY
- RIGHT OF WAY
- DIRECTION OF SURFACE WATER RUNOFF
- XX.XX PROPOSED SPOT ELEVATION
- PROPOSED STORM PIPE
- PROPOSED STORM STRUCTURE
- 61 PROPOSED CONTOUR
- PROPOSED STORM MANHOLE
- X 60.00 EXISTING SPOT ELEVATION
- 61 EXISTING CONTOUR

NORTH

GRAPHIC SCALE IN FEET
0 10 20 40

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2020 KIMLEY-HORN AND ASSOCIATES, INC.

189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER
76118
DATE: 10/28/2020

KHA PROJECT
049610000
DATE
11/02/2020
SCALE AS SHOWN
DESIGNED BY JJS
DRAWN BY EGS
CHECKED BY RMG

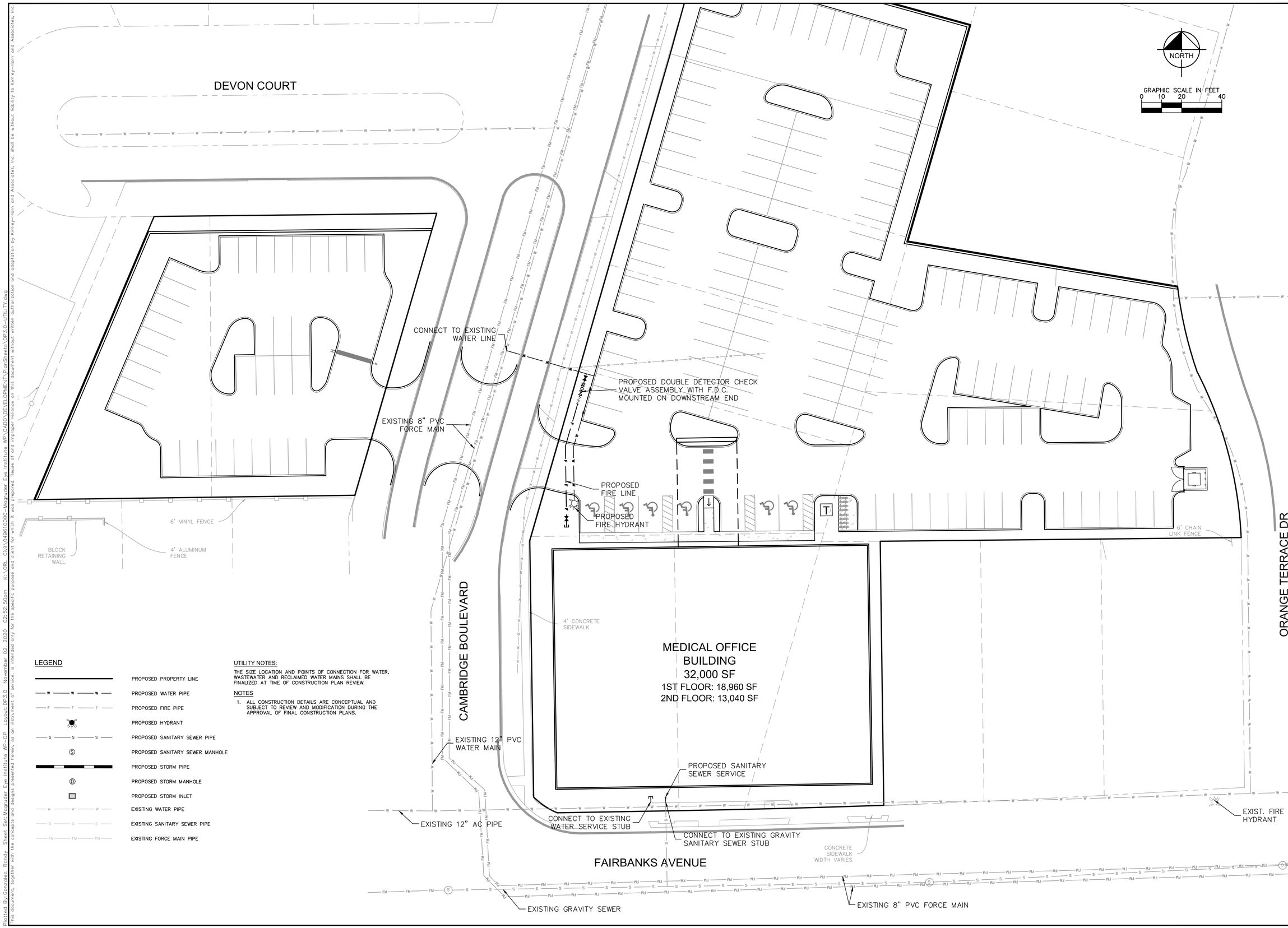
PAVING, GRADING AND DRAINAGE PLAN

MAGRUDER EYE INSTITUTE WP

ORANGE PARK FLORIDA

SHEET NUMBER
DP2.0

Plotted By: Gonzalez, Randy Sheet Set: Magruder Eye Institute WP - DP Layout: DP2.0 November 02, 2020 02:57:20pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CAD\DEVELOPMENT\PlanSheets\DP2.0-PSD.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



LEGEND

- PROPOSED PROPERTY LINE
- PROPOSED WATER PIPE
- PROPOSED FIRE PIPE
- PROPOSED HYDRANT
- PROPOSED SANITARY SEWER PIPE
- PROPOSED SANITARY SEWER MANHOLE
- PROPOSED STORM PIPE
- PROPOSED STORM MANHOLE
- PROPOSED STORM INLET
- EXISTING WATER PIPE
- EXISTING SANITARY SEWER PIPE
- EXISTING FORCE MAIN PIPE

UTILITY NOTES:

THE SIZE LOCATION AND POINTS OF CONNECTION FOR WATER, WASTEWATER AND RECLAIMED WATER MAINS SHALL BE FINALIZED AT TIME OF CONSTRUCTION PLAN REVIEW.

NOTES

1. ALL CONSTRUCTION DETAILS ARE CONCEPTUAL AND SUBJECT TO REVIEW AND MODIFICATION DURING THE APPROVAL OF FINAL CONSTRUCTION PLANS.

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2020 KIMLEY-HORN AND ASSOCIATES, INC.

189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER
76118
DATE 10/28/2020

KHA PROJECT	049610000
DATE	11/02/2020
SCALE	AS SHOWN
DESIGNED BY	JJS
DRAWN BY	EGS
CHECKED BY	RMG

UTILITY PLAN

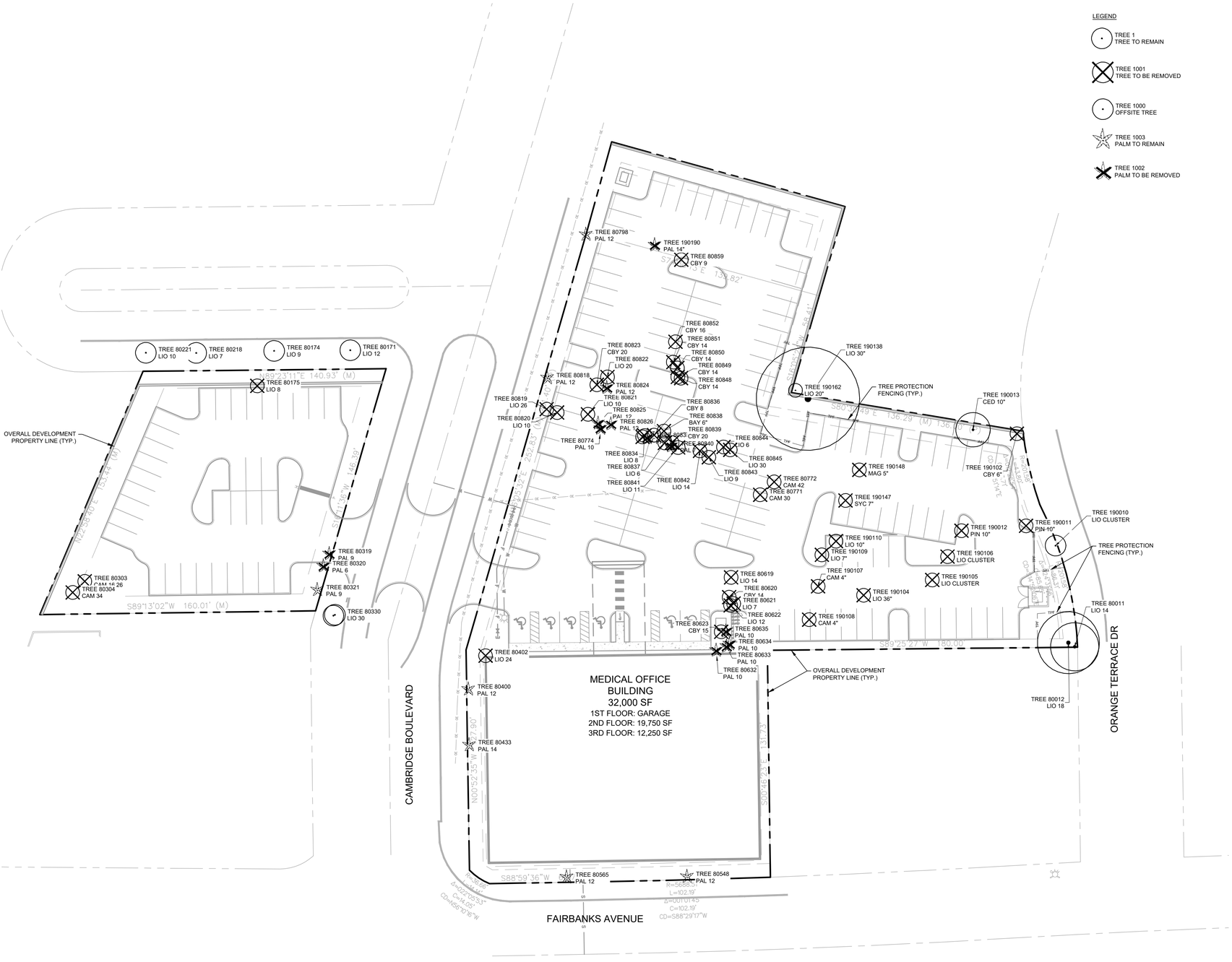
MAGRUDER EYE INSTITUTE WP

FLORIDA

CITY OF WINTER PARK

SHEET NUMBER
DP3.0

Plotted By:Gonzalez, Randy Sheet Set:Magruder Eye Institute WP-DP Layout:DP3.0 November 02, 2020 02:52:50pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CAD\DEVELOPMENT\PlanSheets\DP3.0-UTILITY.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



TREE NUMBER	TREE SPECIES	DBH	MITIGATION INCHES	REQUIRED REPLACEMENT TREES	STATUS
80011	LIVE OAK	14			REMAIN
80012	LIVE OAK	18			REMAIN
80175	LIVE OAK	8			REMOVE
80303	CAMPHOR	42			REMOVE
80304	CAMPHOR	24			REMOVE
80319	PALM	9			REMOVE
80320	PALM	6			REMOVE
80321	PALM	9			REMAIN
80400	PALM	12			REMAIN
80402	LIVE OAK	24	24	2	REMOVE
80433	PALM	14			REMAIN
80548	PALM	12			REMAIN
80565	PALM	12			REMAIN
80619	LIVE OAK	14	14	1	REMOVE
80620	CHINABERRY	14			REMOVE
80621	LIVE OAK	7			REMOVE
80622	LIVE OAK	12	12	1	REMOVE
80623	CHINABERRY	15			REMOVE
80632	PALM	10			REMOVE
80633	PALM	10			REMOVE
80634	PALM	10			REMOVE
80635	PALM	10			REMOVE
80771	CAMPHOR	30			REMOVE
80772	CAMPHOR	42			REMOVE
80774	PALM	10			REMOVE
80798	PALM	12			REMAIN
80818	PALM	12			REMAIN
80819	LIVE OAK	26	26	2	REMOVE
80820	LIVE OAK	10	10	1	REMOVE
80821	LIVE OAK	10	10	1	REMOVE
80822	LIVE OAK	20	20	2	REMOVE
80823	CHINABERRY	20			REMOVE
80824	PALM	12			REMOVE
80825	PALM	12			REMOVE
80826	PALM	12			REMOVE
80833	CHINABERRY	11			REMOVE
80834	LIVE OAK	8			REMOVE
80835	PALM	14			REMOVE
80836	CHINABERRY	8			REMOVE
80837	LIVE OAK	6			REMOVE
80838	BAY	6			REMOVE
80839	CHINABERRY	20			REMOVE
80840	PALM	7			REMOVE
80841	LIVE OAK	11	11	1	REMOVE
80842	LIVE OAK	14	14	1	REMOVE
80843	LIVE OAK	9	9	1	REMOVE
80844	LIVE OAK	6			REMOVE
80845	LIVE OAK	30	30	2	REMOVE
80848	CHINABERRY	14			REMOVE
80849	CHINABERRY	14			REMOVE
80850	CHINABERRY	14			REMOVE
80851	CHINABERRY	14			REMOVE
80852	CHINABERRY	16			REMOVE
80859	CHINABERRY	9			REMOVE
190010	LIVE OAK CLUSTER				REMAIN
190011	PINE	10	10	1	REMOVE
190012	PINE	10	10	1	REMOVE
190013	CEDAR	10			REMAIN
190102	CHINABERRY	6			REMOVE
190104	LIVE OAK	36	36	1	REMOVE
190105	LIVE OAK CLUSTER				REMOVE
190106	LIVE OAK CLUSTER				REMOVE
190107	CAMPHOR	4			REMOVE
190108	CAMPHOR	4			REMOVE
190109	LIVE OAK	7			REMOVE
190110	LIVE OAK	10	10	1	REMOVE
190138	LIVE OAK	30			REMAIN
190147	SYCAMORE	7			REMOVE
190148	MAGNOLIA	5			REMOVE
190190	PALM	14			REMOVE
TOTAL NUMBER OF REPLACEMENT TREES				**19**	
TOTAL NUMBER OF TREES TO BE PLANTED				**19 - 3" CAL. TREES MIN.**	

NOTES:
1. Replacement of protected trees shall require replacement by one or two trees withа dbh equal to or greater than three inches based on the caliper inch measurementof the tree to be removed. If the dbh of the tree to be removed is less than 19 inches,then one replacement tree is required to be replanted. If the dbh of the tree to beremoved is 19 inches, then two replacement trees are required to be replanted.
2. Replacement tree(s) shall be maintained and warranted to survive for a period of oneyear from installation. Trees not deemed to have satisfactorily survived shall bereplaced with new tree(s) of the same size. Replacement tree(s) shall comply withthe same maintenance and replacement warranty as the original replacement tree(s)and the warranty period will restart at the date of replanting. In the event thata tree planting is approved on public property or city rights-of-way, maintenancefees as established by the city commission may be assessed as part of the permit process.

No.	REVISIONS	DATE	BY

Kimley»Horn
© 2020 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER 76118
DATE: 11/25/2020

KHA PROJECT 049610000
DATE: 11/02/2020
SCALE: AS SHOWN
DESIGNED BY: JJS
DRAWN BY: ECS
CHECKED BY: RMC

TREE MITIGATION PLAN

MAGRUDER EYE INSTITUTE WP

WINTER PARK FLORIDA

SHEET NUMBER
L0.50

Plotted By:McKenna, Katelyn Sheet Set:Magruder Eye Institute WP-DP Layout:L0.50 November 02, 2020 01:05:43pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CADD\DEVELOPMENT\PlanSheet\L0.50 – TREE MITIGATION.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

TREE MITIGATION SPECIFICATIONS

A. GENERAL

1. CONTRACTOR SHALL ADHERE TO ALL TREE PROTECTION REQUIREMENTS LISTED IN THESE SPECIFICATIONS AND/OR THOSE LISTED IN THE CITY OR COUNTY ZONING CODE, TREE PROTECTION (LATEST EDITION). WHICHEVER IS MORE STRINGENT SHALL APPLY.
2. CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION PROCEDURES WITH THE PROJECT ARBORIST PRIOR TO BEGINNING WORK.
3. ANY AREAS SUBJECT TO EROSION MUST BE ADEQUATELY STABILIZED WITH VEGETATION MATERIAL THAT WILL, WITHIN A REASONABLE TIME FRAME, DETER SOIL DISTURBANCE.
4. NO SIGNS, BUILDING PERMITS, WIRES OR OTHER ATTACHMENTS OF ANY KIND SHALL BE ATTACHED TO ANY TREE OR PALM. GUY WIRES DESIGNED TO PROTECT TREES ARE EXCLUDED FROM THIS PROHIBITION.
5. EXISTING TREE LOCATIONS AND SIZES ARE ESTIMATES AND ARE BASED ON A SURVEY PROVIDED BY THE OWNER SELECTED SURVEYOR.
6. CONTRACTOR SHALL COORDINATE TREE REMOVAL WITH PERMITTING AGENCY AND PROJECT ARBORIST PRIOR TO CONSTRUCTION. NO PERSON MAY REMOVE OR CAUSE TO BE REMOVED ANY PROTECTED TREE OR PALM WITHOUT FIRST HAVING PROCURED A PERMIT AS PROVIDED BY THE APPROPRIATE PERMITTING AGENCY.
7. FOR PROTECTED TREES OR PALMS BEING REMOVED, THE CONTRACTOR MUST GIVE THE PERMITTING AGENCY REASONABLE OPPORTUNITY TO RELOCATE TREES DESIGNATED FOR REMOVAL TO ANOTHER SITE AT THE PERMITTING AGENCY'S EXPENSE.
8. CONTRACTOR IS RESPONSIBLE FOR POSSESSING ALL REQUIRED APPLICATOR LICENSES, BUSINESS REGISTRATIONS AND INSURANCE, PESTICIDE LABELS, AND MATERIAL DATA SAFETY SHEETS. THE CONTRACTOR IS ALSO RESPONSIBLE FOR HAVING ALL SPILL CONTAINMENT MATERIALS AND REQUIRED PERSONAL PROTECTIVE EQUIPMENT FOR PESTICIDE APPLICATIONS AND ACCIDENTAL SPILLS ON SITE AT ALL TIMES. THE OWNER RESERVES THE RIGHT TO INSPECT EACH APPLICATOR AND HAVE THESE MATERIALS PRESENTED BEFORE AND DURING ANY PESTICIDE TREATMENT.
9. WHERE TRAFFIC AREAS ARE PROPOSED WITHIN THE DRIP LINE OF PROTECTED TREES AND LESS THAN FOUR (4) INCHES OF GRADE CHANGE ARE PROPOSED, PERMEABLE SURFACES THAT ALLOW AIR AND WATER INTO THE SOIL SHOULD BE USED IN LIEU OF ASPHALT OR OTHER SUCH IMPERVIOUS SURFACES.
10. TREE WELLS OF AN APPROVED DESIGN SHALL BE CONSTRUCTED AROUND ALL TREES TO BE PRESERVED WHEN MORE THAN FOUR INCHES OF FILL IS TO BE DEPOSITED WITHIN THE DRIP LINE AREA OF THOSE TREES. COORDINATE WITH PROJECT ARBORIST.
11. THE SEQUENCE OF TREE MITIGATION AND PRESERVATION MEASURES IS IMPERATIVE TO THE HEALTH AND SURVIVABILITY OF THE SUBJECT TREES AND SHALL BE COORDINATED WITH THE OWNER SELECTED PROJECT ARBORIST. THE DESIRED SEQUENCE IS OUTLINED BELOW:
    a. TREE PROTECTION FENCING.
    b. ROOT PRUNING AND ROOT BARRIERS.
    c. CLEARING.
    d. TREE CANOPY PRUNING.
    e. FERTILIZATION.
    f. INSECTICIDE.
    g. IRRIGATION.

B. TREE PROTECTION FENCING

1. PRIOR TO THE ERECTION OF ANY TREE PROTECTION FENCING, ALL FOREIGN SURFACE MATERIAL, TRASH OR DEBRIS SHALL BE REMOVED FROM THE AREA TO BE ENCLOSED BY THE FENCING. AFTER ERECTION OF THE FENCING NO SUCH MATERIAL OR LITTER SHALL BE PERMITTED TO REMAIN WITHIN THE PROTECTED AREA.
2. TREE PROTECTION FENCING SHALL BE PLACED AROUND ALL PROTECTED TREES TO CREATE A PROTECTIVE ROOT ZONE AND SHALL REMAIN IN PLACE UNTIL SITE CLEARING, LAND ALTERATION, AND CONSTRUCTION ACTIVITIES ARE COMPLETE.
3. NATIVE GROUND COVER AND UNDERSTORY VEGETATION EXISTING WITHIN THE PROTECTED AREA SHALL REMAIN THROUGHOUT CONSTRUCTION. OTHER DESIGNATED VEGETATION AND INVASIVE PLANT SPECIES SHALL BE REMOVED ONLY BY MANUAL LABOR UTILIZING HAND TOOLS, OR BY OTHER METHODS APPROVED BY THE PROJECT ARBORIST.
4. TREE PROTECTION FENCING TYPES AND LOCATIONS SHALL BE ERECTED AS SHOWN ON THE TREE MITIGATION PLANS AND DETAILS, OR AS REQUESTED BY LOCAL AGENCY.
5. FINAL LOCATIONS SHALL BE COORDINATED WITH AND APPROVED BY THE PROJECT ARBORIST.
6. NO MATERIALS, EQUIPMENT, SPOIL, WASTE OR WASHOUT WATER MAY BE DEPOSITED, STORED, OR PARKED WITHIN 20 FEET OF THE TREE PROTECTION ZONE.
7. EROSION CONTROL DEVICES SUCH AS SILT FENCING, DEBRIS BASINS, AND WATER DIVERSION STRUCTURES SHALL BE INSTALLED TO PREVENT SILTATION AND/OR EROSION WITHIN THE TREE PROTECTION ZONE.
8. CONSTRUCTION ACTIVITY SHALL NOT DESTROY OR IRREVERSIBLY HARM THE ROOT SYSTEM OF PROTECTED TREES. POST HOLES AND TRENCHES LOCATED CLOSE TO PROTECTED TREES SHALL BE ADJUSTED TO AVOID DAMAGE TO MAJOR ROOTS.
9. DO NOT INSTALL CONDUIT, DRAIN OR IRRIGATION LINES, OR ANY UTILITY LINE WITHIN THE TREE PROTECTION ZONE WITHOUT THE APPROVAL OF THE PROJECT ARBORIST. IF LINES MUST TRAVERSE THE PROTECTION AREA, THEY SHALL BE TUNNELED OR BORED UNDER THE TREE.
10. CONTRACTOR'S ACCESS TO FENCED TREE PROTECTION AREAS WILL BE PERMITTED ONLY WITH APPROVAL OF THE PROJECT ARBORIST.
11. EXCAVATION OR GRADING REQUIRED WITHIN THE PROTECTED AREA SHALL BE LIMITED TO THREE (3) INCHES OF CUT OR FILL. COORDINATE WITH PROJECT ARBORIST.
12. STRUCTURES AND UNDERGROUND FEATURES TO BE REMOVED WITHIN THE TREE PROTECTION ZONE SHALL BE COORDINATED WITH THE PROJECT ARBORIST.
13. TREE PROTECTION FENCING AROUND TREES TO BE RELOCATED SHALL BE ERECTED UNTIL THE TREE IS READY TO BE RELOCATED AND NEW FENCING SHALL BE ERECTED AT THE TREES NEW LOCATION AND WILL REMAIN IN PLACE UNTIL ALL CONSTRUCTION ACTIVITIES ARE COMPLETE.
14. IF ANY DAMAGE TO TREE PROTECTION FENCING SHOULD OCCUR BY ACCIDENT OR NEGLIGENCE, THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMMEDIATE REPAIRS.
15. IF TEMPORARY HAUL OR ACCESS ROADS MUST PASS OVER THE PROTECTED AREA OF TREES TO BE PRESERVED, A ROAD BED OF SIX (6) INCHES OF MULCH OR GRAVEL SHALL BE CREATED TO PROTECT THE SOIL. THE ROAD BED MATERIAL SHALL BE REPLENISHED AS NECESSARY TO MAINTAIN A SIX (6) INCH ROAD BED AT ALL TIMES. CONTRACTOR SHALL REMOVE ALL SUCH MATERIALS FROM THE SITE AS SOON AS TEMPORARY ACCESS IS NO LONGER NECESSARY.
16. CONTRACTOR SHALL COORDINATE WITH THE PROJECT ARBORIST PRIOR TO THE REMOVAL OF ALL TREE PROTECTION FENCING.

C. ROOT PRUNING/TRENCHING

1. TRENCHING LOCATIONS SHALL BE APPROVED IN THE FIELD BY THE PROJECT ARBORIST.
2. TRENCHING EQUIPMENT THAT WILL TURN AT HIGH RPM'S IS PREFERRED, AND SHALL BE APPROVED BY THE PROJECT ARBORIST. APPROVED EQUIPMENT WILL BE USED TO PERFORM ALL ROOT PRUNING OPERATIONS. A MINIMUM DEPTH OF THREE FEET IS REQUIRED.
3. INSTALL ROOT BARRIER WHERE DESIGNATED. SEE TREE MITIGATION PLAN AND DETAIL SHEETS.
4. THE TRENCH SHALL BE BACKFILLED WITH PREVIOUSLY EXCAVATED SOIL AND COMPACTED IMMEDIATELY.
5. TREES TO BE RELOCATED SHALL BE ROOT PRUNED A MINIMUM OF TWELVE (12) WEEKS PRIOR TO TREE RELOCATION.
6. WHEN THE TREE ROOT ZONE WILL BE DISTURBED, AFFECTED ROOTS MUST BE SEVERED BY CLEAN PRUNING CUTS AT THE POINT WHERE CONSTRUCTION IMPACTS THE ROOTS.

D. CLEARING

1. ANY BRUSH CLEARING REQUIRED WITHIN THE TREE PROTECTION ZONE SHALL BE ACCOMPLISHED WITH HAND-OPERATED EQUIPMENT.
2. CONTRACTOR SHALL CLEAR ALL TREE PROTECTION AREAS OF VINES, SHRUBS, GROUND COVERS, WEEDS, SAPLINGS, AND INVASIVES LISTED ON THE LATEST EDITION OF THE FLORIDA EXOTIC PEST PLANT COUNCIL'S LIST OF INVASIVE SPECIES.
3. PROJECT ARBORIST MUST APPROVE METHODS OTHER THAN HAND CLEARING.
4. A TWO (2) INCH LAYER OF MULCH SHALL BE APPLIED OVER THE SURFACE OF EXPOSED ROOTS OF PROTECTED TREES DURING THE SITE CLEARING PHASE.

E. TREE CANOPY PRUNING

1. TREE PRUNING SPECIFICATIONS SHALL BE DEFINED BASED ON SPECIFIC RECOMMENDATIONS OF THE PROJECT ARBORIST. INFORMATION PRESENTED BELOW SHOULD BE USED AS A GUIDELINE.
2. CONTRACTOR SHALL VISIT THE SITE WITH THE PROJECT ARBORIST TO VERIFY THE EXTENT OF REQUIRED PRUNING.
3. ALL PRUNING SHALL BE PERFORMED IN ACCORDANCE WITH THE RECOMMENDATIONS OF A QUALIFIED INTERNATIONAL SOCIETY OF ARBORICULTURE (ISA) CERTIFIED ARBORIST OR AN AMERICAN SOCIETY OF CONSULTING ARBORISTS (ASCA) REGISTERED CONSULTING ARBORIST (RCA).
4. AT LEAST ONE MEMBER OF THE PRUNING CREW SHALL BE AN ISA CERTIFIED ARBORIST.
5. WHILE IN THE TREE, THE ARBORIST SHALL PERFORM AN AERIAL INSPECTION TO IDENTIFY DEFECTS THAT REQUIRE TREATMENT. ANY ADDITIONAL WORK NEEDED SHALL BE REPORTED TO THE OWNER.
6. PRUNING CUTS SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITION OF THE ANSI A300 PRUNING STANDARD (AMERICAN NATIONAL STANDARD FOR TREE CARE OPERATIONS) AND WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST EDITION OF ANSI Z133.1 SAFETY STANDARD; PRUNING SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF ISA'S "BEST MANAGEMENT PRACTICES: TREE PRUNING".
7. WHERE TEMPORARY CLEARANCE IS NEEDED FOR ACCESS, BRANCHES SHALL BE TIED BACK TO HOLD THEM OUT OF THE CLEARANCE ZONE.
8. NO MORE THAN 20 PERCENT OF LIVE FOLIAGE SHALL BE REMOVED WITHIN ANY TREE.
9. ALL TREES WITHIN THE PROJECT AREA SHALL BE PRUNED AS FOLLOWS:
    a. LIVE BRANCH PRUNING SHOULD BE PERFORMED ONLY WHEN THE DANGER OF INSECT OR DISEASE INFESTATION IS NOT PRESENT.
    b. REMOVE STUBS, CUTTING OUTSIDE THE WOUND WOOD TISSUE THAT HAS FORMED AROUND THE BRANCH.
    c. CLEANING, FOR THE SELECTIVE REMOVAL OF DEAD, DISEASED, BROKEN, OR CROSSING BRANCHES DOWN TO ONE INCH IN DIAMETER OR AS DIRECTED BY THE PROJECT ARBORIST.
    d. PRUNING CUTS LARGER THAN 4 INCHES IN DIAMETER, EXCEPT FOR DEAD WOOD, SHALL BE AVOIDED.
    e. PRUNING CUTS THAT EXPOSE HEARTWOOD SHALL BE AVOIDED WHENEVER POSSIBLE.
    f. ALL TREES WITH CROWNS THAT PROJECT INTO PARKING LOT/ROADWAY AREAS SHALL BE RAISED TO 14 FEET ABOVE FINISHED GRADE.
    g. ALL TREES WITH CROWNS THAT PROJECT INTO SIDEWALK AREAS SHALL BE RAISED TO A HEIGHT OF 8 FEET ABOVE FINISHED GRADE.
9. TREES, WHO'S ROOT SYSTEMS WILL BE IMPACTED SHALL RECEIVE THE FOLLOWING PRUNING TO COMPENSATE FOR ROOT LOSS:
    a. THE LOCATION AND SIZE OF BRANCHES FOR REDUCTION SHALL BE DEFINED BY THE PROJECT ARBORIST.
    b. REDUCTION, OR THE SELECTIVE PRUNING TO REDUCE TREE HEIGHT OR SPREAD.
    c. REDUCE END WEIGHT ON HEAVY, HORIZONTAL BRANCHES BY SELECTIVELY REMOVING SMALL DIAMETER BRANCHES, NO GREATER THAN 2 TO 3 INCHES, NEAR THE ENDS OF SCAFFOLD BRANCHES.
    d. RAISING SHALL CONSIST OF SELECTIVE PRUNING TO PROVIDE VERTICAL CLEARANCE.
10. PROPOSED/REPLACEMENT TREES
    a. PRUNING SHALL BE LIMITED TO CLEANING.
11. BRUSH SHALL BE CHIPPED AND SPREAD (ONLY WHEN DISEASE OR INSECT INFESTATION IS NOT PRESENT) UNDERNEATH TREES WITHIN THE TREE PROTECTION ZONE TO A MAXIMUM DEPTH OF THREE (3) INCHES, LEAVING THE TRUNK CLEAR OF MULCH.
12. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ALL EXCESS DEBRIS ON A DAILY BASIS.

F. FERTILIZATION

1. CONTRACTOR SHALL COORDINATE FERTILIZATION PLAN, FOLLOWING BEST MANAGEMENT PRACTICES WITH THE PROJECT ARBORIST PRIOR TO COMMENCEMENT OF WORK.
2. EVERY EFFORT SHALL BE MADE TO UTILIZE CHEMICALS OF AN ORGANIC OR BIODEGRADABLE NATURE IN ORDER TO OFFER THE LEAST IMPACT TO THE NATURAL ENVIRONMENT. CONTRACTOR IS RESPONSIBLE FOR MIXING, APPLYING, AND DISPOSAL OF ALL CHEMICALS IN ACCORDANCE WITH STRICT ADHERENCE TO MANUFACTURER'S SPECIFICATIONS. COORDINATE WITH PROJECT ARBORIST FOR FURTHER INSTRUCTION.
3. ONLY TREES AFFECTED BY CONSTRUCTION OR AS SHOWN ON THE TREE MITIGATION PLAN AND TREE INVENTORY SCHEDULE SHALL BE TREATED.
4. TREES SPECIFIED TO RECEIVE FERTILIZER SHALL BE TREATED AS FOLLOWS.
    a. MIX FERTILIZER ACCORDING TO MANUFACTURER'S SPECIFICATIONS INTO A TANK WITH AGITATION CAPABILITY.
    b. MIX WETTING AGENT ACCORDING TO MANUFACTURER'S SPECIFICATIONS INTO SAME TANK WITH FERTILIZER. AGITATE MIX.
    c. INJECT THE MIXTURE WITH A HYDRAULIC INJECTION SYSTEM INTO THE UPPER 6-12 INCHES OF SOIL WITH A SOIL PROBE. INJECT AT THE RATE OF ONE THIRD (1/3) GALLON AT EACH INJECTION SITE.
    d. THE CRITICAL ROOT ZONE AREA PLUS 2' BEYOND THE CRITICAL ROOT ZONE SHALL BE INJECTED, BUT NOT BEYOND ROOT PRUNING LOCATIONS.
    e. FERTILIZER SHALL BE INSTALLED PRIOR TO THE INSTALLATION OF ANY AERATION SYSTEMS.
    f. EMPTY PRODUCT CONTAINERS SHALL BE STOCKPILED FOR INSPECTION BY THE PROJECT ARBORIST PRIOR TO DISPOSAL.

G. INSECTICIDE

7. NOTIFY PROJECT ARBORIST IF ANY INFESTATION IS NOTICED.
8. FOLLOW PROJECT ARBORIST'S RECOMMENDED PROCEDURES.
9. FOLLOW ALL MANUFACTURERS' RECOMMENDATIONS CONCERNING APPLICATION. READ ALL WARNING LABELS.
10. ANY PETS, AS WELL AS, THE PETS FOOD AND WATER BOWLS SHOULD BE REMOVED FROM THE AREA AND ANY SWIMMING POOLS SHOULD BE COVERED. COORDINATE WITH PROJECT ARBORIST FOR FURTHER INSTRUCTION.
11. ENSURE COMPLETE COVERAGE AND REAPPLY 2-3 MONTHS AFTER INITIAL APPLICATION UTILIZING SAME PROCEDURE.

H. IRRIGATION

1. EVERY EFFORT SHALL BE MADE TO WATER THE PRESERVED TREES AND TRANSPLANTS. CONTRACTOR SHALL IRRIGATE BY HAND OR BY TEMPORARY IRRIGATION.
2. IRRIGATE AS REQUIRED BY PROJECT ARBORIST UNTIL PERMANENT IRRIGATION IS INSTALLED AND OPERATING.
3. UNDERGROUND IRRIGATION SHALL NOT BE INSTALLED WITHIN THE DRIP LINES OF EXISTING TREES UNLESS ROOT PROTECTION MEASURES ARE PROVIDED AND APPROVED BY PROJECT ARBORIST.

I. TREE REMOVALS

1. PRIOR TO AND DURING LAND CLEARING, INCLUDING GRUBBING, ALL TREES TO BE REMOVED SHALL BE CLEARLY MARKED BY PROJECT ARBORIST WITH RED SURVEY RIBBONS AT 36 INCHES MINIMUM ABOVE GRADE.
2. CONTRACTOR SHALL REMOVE ALL TREES AS SHOWN ON THE TREE MITIGATION PLANS AFTER THE TREE PROTECTION FENCING IS INSTALLED.
3. ALL TREES SHOWN TO BE REMOVED SHALL BE FELLED WITH A CHAIN SAW AND STUMP GROUND 6" BELOW SURFACE. ANY TREE SHOWN TO BE REMOVED THAT IS IN AN AREA WHERE COMPACTION IS CRITICAL SHALL BE FELLED WITH A CHAIN SAW AND STUMP REMOVED BY CONTRACTOR.
4. ALL WOOD AND STUMPS FROM REMOVALS SHALL BE HAULED FROM THE SITE THE SAME DAY, EXCEPT FOR TOPS. ALL TOPS ARE TO BE MULCHED AND STOCKPILED OR HAULED DIRECTLY TO MULCHED AREAS FOR RELOCATED TREES IF SCHEDULING PERMITS. TOPS SHALL BE CHIPPED AND PLACED IN THE TREE PROTECTION ZONE TO A DEPTH OF THREE (3) INCHES. ALL EXCESS WOOD CHIPS SHOULD BE HAULED OFF SITE AFTER TRANSPLANTING IS COMPLETE.
5. ALL BURN PITS IF APPLICABLE MUST BE APPROVED BY THE PROJECT ARBORIST AND OWNER.
6. TREES TO BE REMOVED THAT HAVE BRANCHES EXTENDING INTO THE CANOPY OF TREES TO REMAIN MUST BE REMOVED BY A QUALIFIED ISA CERTIFIED ARBORIST AND NOT BY DEMOLITION OR CONSTRUCTION CONTRACTORS. THE QUALIFIED ARBORIST SHALL REMOVE THE TREE IN A MANNER THAT CAUSES NO DAMAGE TO THE TREES AND UNDERSTORY VEGETATION TO REMAIN.
7. TREES TO BE REMOVED LOCATED WITHIN THE TREE PROTECTION ZONE SHALL BE REMOVED BY A QUALIFIED ISA CERTIFIED ARBORIST. THE TREES SHALL BE CUT NEAR GROUND LEVEL AND THE STUMP GROUND OUT.

L. TOPSOIL

10. CONTRACTOR SHALL COORDINATE ALL EARTHWORK OPERATIONS WITHIN TREE PROTECTION AREAS WITH THE PROJECT ARBORIST PRIOR TO BEGINNING WORK.
11. ALL TOPSOIL SHALL BE NATURAL, FRIABLE, FERTILE, FINE LOAMY SOIL POSSESSING CHARACTERISTICS OF REPRESENTATIVE TOPSOIL IN THE VICINITY THAT PRODUCES HEAVY GROWTH.
12. TOPSOIL, PH RANGE OF 5.5 TO 7.0, 3-5 PERCENT ORGANIC MATERIAL MINIMUM, FREE FROM SUBSOIL, OBJECTIONABLE WEEDS, LITTER, SODS, STIFF CLAY, STONES LARGER THAN ONE (1) INCH IN DIAMETER, STUMPS, ROOTS, TRASH, TOXIC SUBSTANCES, OR ANY OTHER MATERIAL WHICH MAY BE HARMFUL TO PLANT GROWTH.
13. VERIFY AMOUNT STOCKPILED IF ANY, AND SUPPLY ADDITIONAL AS NEEDED FROM NATURALLY WELL-DRAINED SITES WHERE TOPSOIL OCCURS AT LEAST FOUR (4) INCHES DEEP. DO NOT OBTAIN TOPSOIL FROM BOGS OR MARSHES.
14. PROJECT ARBORIST SHALL APPROVE ALL TOPSOIL PRIOR TO PLACEMENT.

M. REPAIR OF DAMAGED TREES

1. IF DAMAGE TO ANY TREE SHOULD OCCUR BY ACCIDENT OR NEGLIGENCE DURING THE CONSTRUCTION PERIOD, THE PROJECT ARBORIST SHALL APPRAISE THE DAMAGE AND MAKE RECOMMENDATIONS TO THE OWNER FOR REPAIR BY THE CONTRACTOR.
2. IF ANY TREE DESIGNATED TO BE SAVED IS REMOVED FROM THE SITE WITHOUT PERMISSION OF THE OWNER'S REPRESENTATIVE, THE PROJECT ARBORIST SHALL APPRAISE THE TREE AND MAKE RECOMMENDATIONS TO THE OWNER FOR REPLACEMENT BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACEMENT OF THE TREE AND ANY FEES THAT MAY BE ASSESSED TO THE OWNER BY THE GOVERNING AGENCY.

1 L0.51 TYPICAL TREE MITIGATION NOTES

No.	REVISIONS	DATE	BY

Kimley»Horn
© 2020 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER 76118
DATE: 11/25/2020

KHA PROJECT	049610000
DATE	11/02/2020
SCALE	AS SHOWN
DESIGNED BY	JJS
DRAWN BY	EGS
CHECKED BY	RMG

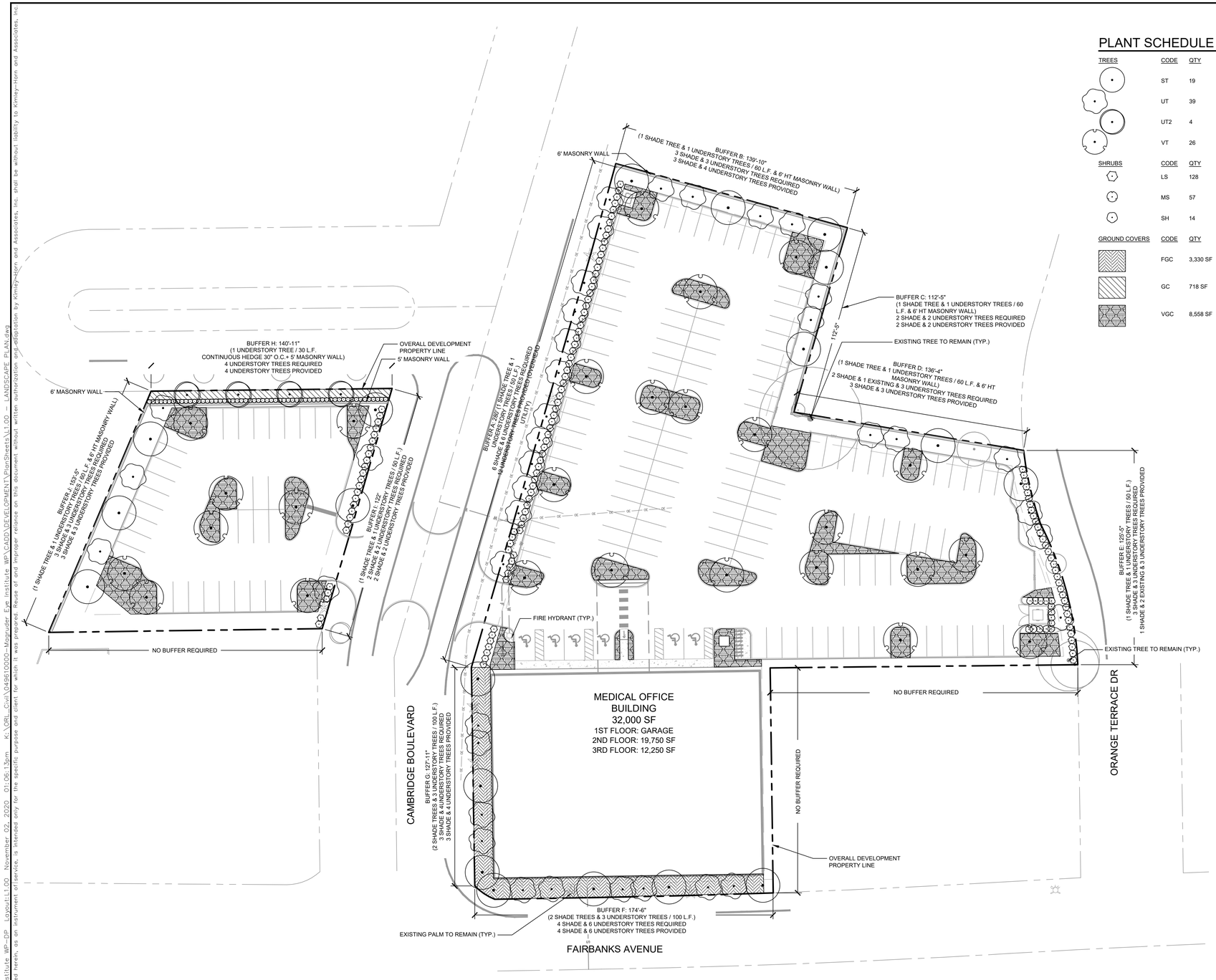
TREE MITIGATION SPECIFICATIONS

MAGRUDER EYE INSTITUTE WP

WINTER PARK FLORIDA

SHEET NUMBER
L0.51

Plotted By:McKenna, Katelyn Sheet Set:Magruder Eye Institute WP-DP Layout:L0.51 November 02, 2020 01:05:54pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CADD\DEVELOPMENT\PlanSheets\L0.50 - TREE MITIGATION.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



PLANT SCHEDULE

TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL	SIZE
	ST	19	SHADE TREE FL #1	SHADE TREE	FG	3" CAL MIN	12` HT MIN
	UT	39	UNDERSTORY TREE FL #1	UNDERSTORY TREE	FG	2" CAL MIN	8` HT., 4` SPR.
	UT2	4	UNDERSTORY TREE FL #1	UNDERSTORY TREE	FG	3" CAL MIN	8` HT., 4` SPR.
	VT	26	VUA SHADE TREE	VUA SHADE TREE	FG	3" CAL MIN	12` HT MIN

SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SPACING	SIZE
	LS	128	LARGE SHRUB	LARGE SHRUB	3 GAL	36" OC	30" HT MIN
	MS	57	MEDIUM SHRUB	MEDIUM SHRUB	7 GAL	30" OC	30" HT MIN
	SH	14	SCREENING HEDGE	SCREENING HEDGE	30 GAL	48" OC	6` HT MIN

GROUND COVERS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	SIZE	SPACING
	FGC	3,330 SF	FACADE LANDSCAPE SHRUB & GROUNDCOVER	FACADE LANDSCAPE SHRUB & GROUNDCOVER	1 GAL	12" FULL	18" OC
	GC	718 SF	GROUNDCOVER	GROUNDCOVER	1 GAL	12" FULL	18" OC
	VGC	8,558 SF	VUA INTERIOR GROUNDCOVER	VUA INTERIOR GROUNDCOVER	1 GAL	12" FULL	18" OC

LANDSCAPE REQUIREMENTS		
CODE	REQUIRED	PROPOSED
SECTION 58-336 (c) BUFFER B/W ROW: 2 SHADE TREES AND 3 UNDERSTORY TREES / 100 LF	BUFFER F: 175 LF 4 SHADE & 6 UNDERSTORY TREES REQUIRED BUFFER G: 127 LF 3 SHADE & 4 UNDERSTORY TREES REQUIRED	BUFFER F: 4 SHADE & 6 UNDERSTORY TREES PROVIDED BUFFER G: 3 SHADE & 4 UNDERSTORY TREES PROVIDED
SECTION 58-336 (E) (1) VUA ADJACENT TO ROW 1 SHADE TREE / 50 LF 1 UNDERSTORY TREE / 50 LF	BUFFER A: 280 LF 6 SHADE & 6 UNDERSTORY TREES BUFFER E: 125 LF 3 SHADE & 3 UNDERSTORY TREES BUFFER I: 122 LF 3 SHADE & 3 UNDERSTORY TREES	BUFFER A: 280 LF 12 UNDERSTORY TREES (UTILITY CONFLICT) BUFFER E: 125 LF 1 SHADE & 2 EXISTING & 3 UNDERSTORY TREES BUFFER I: 122 LF 3 SHADE & 3 UNDERSTORY TREES
SECTION 58-336 (E) (2) (A) VUA ADJACENT TO SINGLE FAMILY 1 SHADE TREE / 60 LF 1 UNDERSTORY TREE / 60 LF 6' HT MASONRY WALL	BUFFER B: 139 LF 3 SHADE & 3 UNDERSTORY TREES BUFFER C: 112 LF 2 SHADE & 2 UNDERSTORY TREES BUFFER D: 136 LF 3 SHADE & 3 UNDERSTORY TREES BUFFER J: 153 LF 3 SHADE & 3 UNDERSTORY TREES	BUFFER B: 139 LF 3 SHADE & 4 UNDERSTORY TREES BUFFER C: 112 LF 2 SHADE & 2 UNDERSTORY TREES BUFFER D: 136 LF 2 SHADE & 1 EXISTING & 3 UNDERSTORY TREES BUFFER J: 153 LF 3 SHADE & 3 UNDERSTORY TREES
SECTION 58-336 (E) (2) (B) VUA ACROSS TO SINGLE FAMILY 1 UNDERSTORY TREE / 30 LF CONTINUOUS HEDGE 30" O.C. 5' MASONRY WALL	BUFFER H: 140 LF 4 UNDERSTORY TREES	BUFFER H: 140 LF 4 UNDERSTORY TREES

NOTES:
1. ALL PROPOSED LANDSCAPE TO BE IRRIGATED PER SEC.58-338
2. ALL PROTECTED TREES TO BE REMOVED SHALL BE REPLACED PER SEC.58-287 CALIPER INCHES NOT PLANTED AS REPLACEMENT TREES ARE TO BE COMPENSATED BY PAYMENT TO THE "TREE REPLACEMENT TRUST FUND"
3. ALL LANDSCAPE TO BE IN COMPLIANCE WITH SEC.58-336 AND 58-337
4. THE LANDSCAPE PLAN WILL HEREAFTER BE MAINTAINED IN COMPLIANCE WITH THIS CODE BY THE PROPERTY OWNER.

CONCEPT PLANT SELECTION

SHADE TREES:
- QUERCUS VIRGINIANA 'HIGH RISE' / SOUTHERN LIVE OAK
- ACER RUBRUM 'FLORIDA FLAME' / FLORIDA FLAME RED MAPLE
- MAGNOLIA GRANDIFLORA 'BRACKEN'S BROWN BEAUTY' / SOUTHERN MAGNOLIA

PALM TREES:
- SABAL PALMETTO / CABBAGE PALMETTO
- PHOENIX SYLVESTRIS / WILD DATE PALM

UNDERSTORY TREE:
- LAGERSTROEMIA INDICA / CREPE MYRTLE
- ILEX CASSINE/ DAHOON HOLLY
- MAGNOLIA GRANDIFLORA 'LITTLE GEM' / LITTLE GEM MAGNOLIA

SCREENING SHRUBS:
- NERIUM OLEANDER / OLEANDER
- VIBURNUM ODORATISSIMUM 'AWABUKI' / AWABUKI VIBURNUM
- PODOCARPUS MACROPHYLLUS / PODOCARPUS

LARGE SHRUBS:
- VIBURNUM ODORATISSIMUM / SWEET VIBURNUM
- PHILODENDRON BIPINNATIFIDUM / SELLOUM
- ZAMIA FURFURACEA / CARDBOARD PALM
- HYDRANGEA MACROPHYLLA / LARGE LEAF HYDRANGEA
- LOROPETULUM CHINENSE RUBRUM / LOROPETULUM

SMALL SHRUBS:
- HAMELIA PATENS 'COMPACTA'/ DWARF SCARLET BUSH
- MUHLENBERGIA CAPILLARIS / PINK MUHLY GRASS
- PLUMBAGO AURICULATA / BLUE PLUMBAGO
- RHAPHIOLEPIS INDICA 'ALBA' / WHITE INDIAN HAWTHORN
- ALLAMANDA CATHARTICA / ALLAMANDA

GROUND COVERS:
- JUNIPERUS HORIZONTALIS / CREEPING JUNIPER
- PANICUM VIRGATUM / PANIC GRASS (SWITCH GRASS)
- TRACHELOSPERMUM JASMINOIDES 'MINIMA' / MINIMA JASMINE
- VARIEGATED BULBINE FRUTESCENS / STALKED BULBINE
- ARACHIS GLABRATA / PERENNIAL PEANUT

GRAPHIC SCALE IN FEET
0 15 30 60

NORTH

No.	REVISIONS	DATE	BY

Kimley»Horn
© 2020 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER 76118
DATE: 11/25/2020

KHA PROJECT 049610000
DATE 11/02/2020
SCALE AS SHOWN
DESIGNED BY JJS
DRAWN BY ECS
CHECKED BY RMG

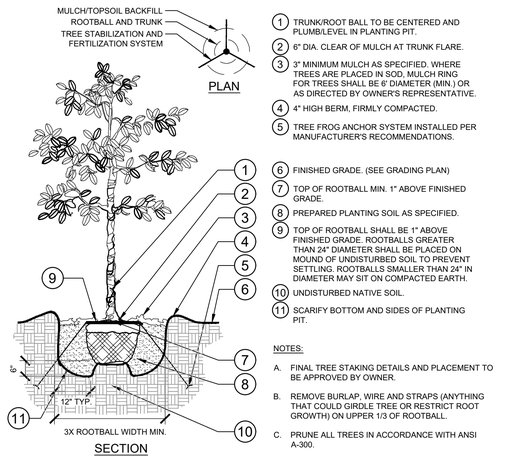
LANDSCAPE PLAN

MAGRUDER EYE INSTITUTE WP

WINTER PARK FLORIDA

SHEET NUMBER
L1.00

Plotted By:McKenna, Katelyn Sheet Set:Magruder Eye Institute WP-DP Layout:L1.00 November 02, 2020 01:06:13pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CAD\DEVELOPMENT\PlanSheets\L1.00 - LANDSCAPE PLAN.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



SECTION

1. TRUNK/ROOT BALL TO BE CENTERED AND PLUMB/LEVEL IN PLANTING PIT.
2. 6" DIA. CLEAR OF MULCH AT TRUNK FLARE.
3. 3" MINIMUM MULCH AS SPECIFIED. WHERE TREES ARE PLACED IN SOD, MULCH RING FOR TREES SHALL BE 6' DIAMETER (MIN.) OR AS DIRECTED BY OWNER'S REPRESENTATIVE.
4. 4" HIGH BERM, FIRMLY COMPACTED.
5. TREE FROG ANCHOR SYSTEM INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
6. FINISHED GRADE. (SEE GRADING PLAN)
7. TOP OF ROOTBALL MIN. 1" ABOVE FINISHED GRADE.
8. PREPARED PLANTING SOIL AS SPECIFIED.
9. TOP OF ROOTBALL SHALL BE 1" ABOVE FINISHED GRADE. ROOTBALLS GREATER THAN 24" DIAMETER SHALL BE PLACED ON MOUND OF UNDISTURBED SOIL TO PREVENT SETTLING. ROOTBALLS SMALLER THAN 24" IN DIAMETER MAY SIT ON COMPACTED EARTH.
10. UNDISTURBED NATIVE SOIL.
11. SCARIFY BOTTOM AND SIDES OF PLANTING PIT.

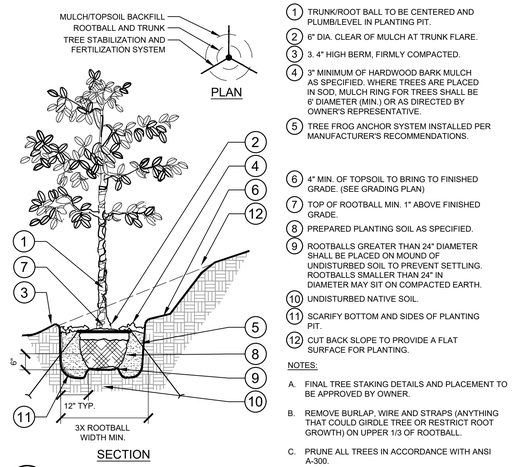
NOTES:

A. FINAL TREE STAKING DETAILS AND PLACEMENT TO BE APPROVED BY OWNER.

B. REMOVE BURLAP, WIRE AND STRAPS (ANYTHING THAT COULD GIRDLE TREE OR RESTRICT ROOT GROWTH) ON UPPER 1/3 OF ROOTBALL.

C. PRUNE ALL TREES IN ACCORDANCE WITH ANSI A-300.

1 L1.50 **TREE PLANTING**
SECTION / PLAN — NTS



SECTION

1. TRUNK/ROOT BALL TO BE CENTERED AND PLUMB/LEVEL IN PLANTING PIT.
2. 6" DIA. CLEAR OF MULCH AT TRUNK FLARE.
3. 3. 4" HIGH BERM, FIRMLY COMPACTED.
4. 3" MINIMUM OF HARDWOOD BARK MULCH AS SPECIFIED. WHERE TREES ARE PLACED IN SOD, MULCH RING FOR TREES SHALL BE 6' DIAMETER (MIN.) OR AS DIRECTED BY OWNER'S REPRESENTATIVE.
5. TREE FROG ANCHOR SYSTEM INSTALLED PER MANUFACTURER'S RECOMMENDATIONS.
6. 4" MIN. OF TOPSOIL TO BRING TO FINISHED GRADE. (SEE GRADING PLAN)
7. TOP OF ROOTBALL MIN. 1" ABOVE FINISHED GRADE.
8. PREPARED PLANTING SOIL AS SPECIFIED.
9. ROOTBALLS GREATER THAN 24" DIAMETER SHALL BE PLACED ON MOUND OF UNDISTURBED SOIL TO PREVENT SETTLING. ROOTBALLS SMALLER THAN 24" IN DIAMETER MAY SIT ON COMPACTED EARTH.
10. UNDISTURBED NATIVE SOIL.
11. SCARIFY BOTTOM AND SIDES OF PLANTING PIT.
12. CUT BACK SLOPE TO PROVIDE A FLAT SURFACE FOR PLANTING.

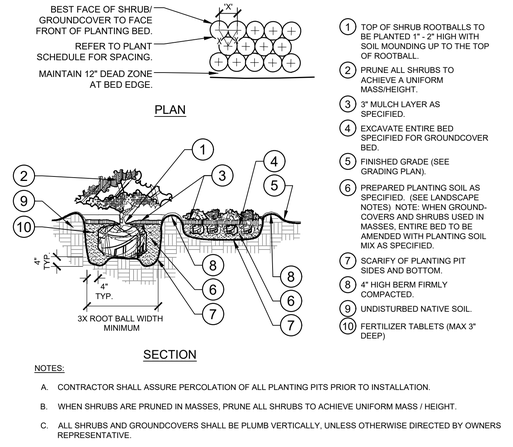
NOTES:

A. FINAL TREE STAKING DETAILS AND PLACEMENT TO BE APPROVED BY OWNER.

B. REMOVE BURLAP, WIRE AND STRAPS (ANYTHING THAT COULD GIRDLE TREE OR RESTRICT ROOT GROWTH) ON UPPER 1/3 OF ROOTBALL.

C. PRUNE ALL TREES IN ACCORDANCE WITH ANSI A-300.

2 L1.50 **TREE PLANTING ON A SLOPE**
SECTION / PLAN — NTS



SECTION

1. TOP OF SHRUB ROOTBALLS TO BE PLANTED 1" - 2" HIGH WITH SOIL MOUNDING UP TO THE TOP OF ROOTBALL.
2. PRUNE ALL SHRUBS TO ACHIEVE A UNIFORM MASS/HEIGHT.
3. 3" MULCH LAYER AS SPECIFIED.
4. EXCAVATE ENTIRE BED SPECIFIED FOR GROUNDCOVER BED.
5. FINISHED GRADE (SEE GRADING PLAN).
6. PREPARED PLANTING SOIL AS SPECIFIED. (SEE LANDSCAPE NOTES) NOTE: WHEN GROUND-COVERS AND SHRUBS USED IN MASSES, ENTIRE BED TO BE AMENDED WITH PLANTING SOIL MIX AS SPECIFIED.
7. SCARIFY OF PLANTING PIT SIDES AND BOTTOM.
8. 4" HIGH BERM FIRMLY COMPACTED.
9. UNDISTURBED NATIVE SOIL.
10. FERTILIZER TABLETS (MAX 3" DEEP)

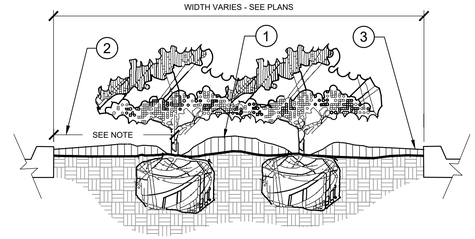
NOTES:

A. CONTRACTOR SHALL ASSURE PERCOLATION OF ALL PLANTING PITS PRIOR TO INSTALLATION.

B. WHEN SHRUBS ARE PRUNED IN MASSES, PRUNE ALL SHRUBS TO ACHIEVE UNIFORM MASS / HEIGHT.

C. ALL SHRUBS AND GROUNDCOVERS SHALL BE PLUMB VERTICALLY, UNLESS OTHERWISE DIRECTED BY OWNERS REPRESENTATIVE.

3 L1.50 **SHRUB/GROUNDCOVER PLANTING**
SECTION / PLAN — NTS



1. CROWN ISLANDS @ 5:1 SLOPES (OR AS SPECIFIED ON THE LANDSCAPE PLANS).
2. CLEAR ZONE: 36" MIN. FROM BACK OF CURB TO CENTER OF NEAREST SHRUB. CLEAR ZONE SHALL CONTAIN 3" CONTINUOUS MULCH OR TURF, SEE PLANS.
3. 2" MIN VERTICAL CLEARANCE, TOP OF CURB TO TOP OF MULCH.

NOTES:

A. EXCAVATE A CONTINUOUS 24" DEEP PIT (FROM TOP OF CURB) FOR ENTIRE LENGTH AND WIDTH OF ISLAND & BACKFILL WITH APPROVED PLANTING MIX.

B. PROTECT AND RETAIN ALL CURBS AND BASE. COMPACTED SUBGRADE TO REMAIN FOR STRUCTURAL SUPPORT OF CURB SYSTEM (TYP).

C. ALL ISLANDS SHALL UTILIZE POOR DRAINAGE DETAIL WHEN PERCOLATION RATES ARE 2" PER HOUR OR LESS.

4 L1.50 **PLANTED PARKING LOT ISLANDS/MEDIANS**
SECTION — NTS

SECTION

1. TOP OF SHRUB ROOTBALLS TO BE PLANTED 1" - 2" HIGH WITH SOIL MOUNDING UP TO THE TOP OF ROOTBALL.
2. PRUNE SHRUBS AS DIRECTED BY OWNER'S REPRESENTATIVE.
3. 3" MINIMUM OF MULCH AS SPECIFIED. WHERE SHRUBS ARE PLACED IN MASSES, MULCH SHALL BE SPREAD IN A CONTINUOUS BED.
4. SOIL BERM TO HOLD WATER. TOP OF PLANTING PIT 'BERM' TO BE LEVEL ACROSS PIT. SLOPE DOWNHILL PORTION OF BERM AS REQUIRED TO MEET EXISTING GRADE. MULCH OVER EXPOSED TOPSOIL.
5. FINISHED GRADE (SEE GRADING PLAN)
6. PREPARED PLANTING SOIL AS SPECIFIED. (SEE LANDSCAPE NOTES).
7. SCARIFY SIDES AND BOTTOM OF PLANTING PIT.
8. FERTILIZER TABLETS (MAX 3" DEEP).
9. UNDISTURBED NATIVE SOIL

NOTES:

A. CONTRACTOR SHALL ASSURE PERCOLATION OF ALL PLANTING PITS PRIOR TO INSTALLATION.

B. WHEN SHRUBS ARE PRUNED IN MASSES, PRUNE ALL SHRUBS TO ACHIEVE UNIFORM MASS / HEIGHT.

C. ALL SHRUBS AND GROUNDCOVERS SHALL BE PLUMB VERTICALLY, UNLESS OTHERWISE DIRECTED BY OWNERS REPRESENTATIVE.

5 L1.50 **SHRUB/GROUNDCOVER PLANTING ON A SLOPE**
SECTION — NTS

NOTES:

1. CLEAR ZONE: 36" MIN. FROM BUILDING TO CENTER OF NEAREST SHRUB.
2. INSTALL SPECIFIED MULCH: 24" MIN. FROM BUILDING. SPECIFIED MULCH TO BE INSTALLED AT A DEPTH OF 3" (MIN.)

6 L1.50 **PLANTINGS ADJACENT TO BUILDINGS**
SECTION — NTS

1. INSTALL CONTINUOUS MULCH BED ADJACENT TO PARKING SPACES AS SHOWN. MULCH SHALL BE MIN. 3" DEEP. NO POP-UP IRRIGATION HEADS SHALL BE LOCATED WITHIN 24" OF A PARKING SPACE ON ANY SIDE.
2. CURB / PARKING LOT EDGE.

7 L1.50 **PARKING SPACE/CURB PLANTING**
SECTION — NTS

SECTION

PLAN

1. FIRE HYDRANT.
2. NO PLANT EXCEEDING 12" MATURE HEIGHT MATERIAL SHALL BE PLACED WITHIN SHOWN RADIUS OF ALL PROPOSED OR EXISTING FIRE HYDRANTS. CONTRACTOR SHALL ADJUST PLANT MATERIAL SO THAT NO CONFLICTS WITH FIRE HYDRANTS OCCUR ON SITE.
3. FRONT OF HYDRANT (TOWARD CURB)

8 L1.50 **SHRUB PLANTING AT FIRE HYDRANT**
SECTION / PLAN — NTS

SECTION

1. FINISH GRADE (SEE GRADING PLANS).
2. BACKFILL WITH PREPARED PLANTING SOIL MIX AS SPECIFIED.
3. FILTER CLOTH, MIRAFI 500X OR BETTER.
4. SLOPE BOTTOM TO DRAIN.
5. AUGURED HOLE Ø ±18" PENETRATE THROUGH OCCLUDING LAYER TO WATER TABLE OR TO A DEPTH OF 7' TO ASSURE PROPER PERCOLATION.
6. BACKFILL WITH 1/2" - 3/4" GRAVEL TO REQUIRED DEPTH THROUGH OCCLUDING LAYER TO ASSURE PROPER PERCOLATION.
7. WATER TABLE. (DEPTH VARIES)
8. UNDISTURBED NATIVE SOIL.
9. SET ROOTBALL ON UNDISTURBED STABLE SUBSOIL SO THAT TOP OF ROOT BALL IS 1" ABOVE FINISHED GRADE.

NOTES:

A. THIS DETAIL SHALL BE IMPLEMENTED WHERE PERCOLATION RATES ARE 2" PER HOUR OR LESS.

B. CONTRACTOR TO PERFORM PERCOLATION TEST AS REQUIRED. AND NOTIFY OWNER/LANDSCAPE ARCHITECT.

C. SEE TYPICAL TREE PLANTING DETAIL THIS SHEET FOR PLANT STAKING.

9 L1.50 **POOR DRAINAGE CONDITION**
SECTION / PLAN — NTS

1. 6'H "PERIMETER PLUS" CONSTRUCTION FENCE BY CONWED PLASTICS OR OWNER'S REPRESENTATIVE APPROVED EQUAL. SUBMIT PRODUCT INFORMATION FOR APPROVAL PRIOR TO INSTALLATION.
2. 8' TALL METAL "T" POSTS ON 2" x 2" X 8' PRESSURE TREATED WOOD POSTS WITH 24" BURIAL BELOW GRADE.

INSTALLATION NOTES:

A. POST SELECTION SHOULD BE BASED ON EXPECTED STRENGTH NEEDS AND THE LENGTH OF TIME FENCE WILL BE IN PLACE. FLEXIBLE FIBERGLASS ROD POSTS ARE RECOMMENDED FOR PARKS, ATHLETIC EVENTS AND CROWD CONTROL INSTALLATIONS. METAL "T" POSTS OR TREATED WOOD POSTS ARE TYPICALLY USED FOR CONSTRUCTION AND OTHER APPLICATIONS.

B. POSTS SHOULD BE DRIVEN INTO THE GROUND TO A DEPTH OF 1/3 OF THE HEIGHT OF THE POST. FOR EXAMPLE, A 6' POST SHOULD BE SET AT LEAST 2' INTO THE GROUND.

C. SPACE POSTS EVERY 6' (MIN.) TO 8' (MAX.).

D. SECURE FENCING TO POST WITH NYLON CABLE TIES (AVAILABLE FROM CONWED PLASTICS). WOOD STRIPS MAY BE ALSO BE USED TO PROVIDE ADDITIONAL SUPPORT AND PROTECTION BETWEEN TIES AND POSTS.

NOTE: IF WIRE TIES ARE USED, AVOID DIRECT CONTACT WITH FENCE. WIRE MAY DAMAGE FENCE OVER TIME.

10 L1.50 **TREE PROTECTION FENCING**
ELEVATION / PLAN — NTS

1. 5 LAYERS OF BURLAP TO PROTECT TRUNK.
2. FIVE 2" X 4" X 18" WOOD BATTENS.
3. SECURE BATTENS WITH 2- 3/4" CARBON STEEL BANDS TO HOLD BATTENS IN PLACE. NO NAILS SHALL BE DRIVEN INTO PALM. HEIGHT OF BATTENS SHALL BE LOCATED IN RELATION TO THE HEIGHT OF THE PALM FOR ADEQUATE BRACING.
4. (3) 2"X4"X8' SUPPORTS. NAIL (DRILL AN NAIL IF NECESSARY) TO BATTENS AND 2" 4" STAKES. STAIN DARK BROWN.
5. 3" SPECIFIED MULCH
6. BERM SOIL TO HOLD WATER.
7. FINISH GRADE
8. 2X4X24" (MIN).P.T. WOOD STAKES (TYP.) NAIL TO SUPPORT POLES.
9. PREPARED PLANTING SOIL AS SPECIFIED. (SEE LANDSCAPE NOTES).

NOTES:

A. FINAL TREE STAKING DETAILS AND PLACEMENT TO BE APPROVED BY OWNER.

B. ALL TREE STAKING IS TO BE WITHIN MULCH BED AREA OUTSIDE TREE PIT.

C. CONTRACTOR SHALL ASSURE PERCOLATION OF ALL PLANTING PITS PRIOR TO INSTALLATION - REFER TO POOR DRAINAGE CONDITION DETAIL.

11 L1.50 **PALM PLANTING**
SECTION / PLAN — NTS

SECTION

1. BASE OF TREE SHALL BE PLANTED SLIGHTLY ABOVE (1" MIN.) ADJACENT FINISH GRADE. REMOVE ALL TWINE & STRAPS & CUT BURLAP FROM TOP 1/3 OF ROOT BALL. NO SYNTHETIC BURLAP WILL BE ACCEPTED.
2. 4" SHREDDED HARDWOOD MULCH OR APPROVED EQUAL.
3. DIAMETER OF TREE PIT TO BE TWICE THE DIAMETER OF ROOT BALL-ROUGHEN SIDES OF TREE PIT.
4. 3" HIGH SOIL BERM TO HOLD WATER.
5. TOPSOIL MIX BACKFILL.
6. TREE WRAP.
7. 4" MIN. OF TOPSOIL TO BRING TO FINISHED GRADE (SEE GRADING PLAN).
8. ROOT BALLS GREATER THAN 24" DIAMETER SHALL BE PLACED ON MOUND OF UNDISTURBED SOIL TO PREVENT SETTLING ROOT BALLS SMALLER THAN 24" IN DIA. MAY SIT ON COMPACTED EARTH.
9. UNDISTURBED SUBSOIL.
10. PREPARE PLANTING SOIL AS SPECIFIED.
11. **Tree Frog® RBK40pt** FOR UP TO 4" TREE CALIPER OR **Tree Frog® RBK60pt** FOR GREATER THAN 4" AND UP TO 6" TREE CALIPER. REFER TO SITEWORK SPECIFICATIONS FOR APPROVED MATERIALS AND INSTALLATION REQUIREMENTS.

NOTES:

A. FINAL TREE STAKING DETAILS AND PLACEMENT TO BE APPROVED BY OWNER.

B. SET TREE AT ORIGINAL DEPTH. REMOVE BURLAP, WIRE AND STRAPS (ANYTHING THAT COULD GIRDLE TREE OR RESTRICT ROOT GROWTH) ON UPPER 1/3 OF ROOTBALL.

C. SEE LANDSCAPE NOTES FOR THE TYPE OF MULCH MATERIAL TO USE.

D. PRUNE TREE AS DIRECTED BY LANDSCAPE ARCHITECT

E. ASSURE PERCOLATION OF ALL PLANTING PITS

12 L1.50 **MULTI-TRUNK TREE PLANTING**
SECTION/PLAN — NTS

No.	REVISIONS	DATE	BY

Kimley»Horn
© 2020 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER 76118
DATE: 11/25/2020

KHA PROJECT 049610000
DATE 11/02/2020
SCALE AS SHOWN
DESIGNED BY JJS
DRAWN BY EGS
CHECKED BY RMG

MAGRUDER EYE INSTITUTE WP

FLORIDA
WINTER PARK

LANDSCAPE DETAILS

SHEET NUMBER
L1.50

Plotted By:McKenna, Kristin Sheet Set:Magruder Eye Institute WP Layout:L1.50 November 02, 2020 01:06:20pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CADD\DEVELOPMENT\PlanSheets\L1.50 - LANDSCAPE DETAILS & SPECIFICATION.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

GENERAL LANDSCAPE SPECIFICATIONS AND NOTES

A. SCOPE OF WORK

1. THE WORK CONSISTS OF: FURNISHING ALL LABOR, MATERIALS, EQUIPMENT, TOOLS, TRANSPORTATION, AND ANY OTHER APPURTENANCES NECESSARY FOR THE COMPLETION OF THIS PROJECT AS SHOWN ON THE DRAWINGS, AS INCLUDED IN THE PLANT LIST, AND AS SPECIFIED HEREIN.

2. WORK SHALL INCLUDE MAINTENANCE AND WATERING OF ALL CONTRACT PLANTING AREAS UNTIL CERTIFICATION OF ACCEPTANCE BY THE OWNER.

B. PROTECTION OF EXISTING STRUCTURES

1. ALL EXISTING BUILDINGS, WALKS, WALLS, PAVING, PIPING, OTHER SITE CONSTRUCTION ITEMS, AND PLANTING ALREADY COMPLETED OR ESTABLISHED AND DESIGNATED TO REMAIN SHALL BE PROTECTED FROM DAMAGE BY THE CONTRACTOR UNLESS OTHERWISE SPECIFIED. ALL DAMAGE RESULTING FROM NEGLIGENCE SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER, AT NO COST TO THE OWNER.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL NECESSARY BMP DEVICES ACCORDING TO ALL REGULATORY AGENCY'S STANDARDS THROUGH THE DURATION OF ALL CONSTRUCTION ACTIVITIES.

3. THE CONTRACTOR SHALL SUBMIT A DETAILED PROJECT SPECIFIC WORK ZONE TRAFFIC CONTROL PLAN UNLESS THE WORK REQUIRES NOTHING MORE THAN A DIRECT APPLICATION OF FDOT DESIGN STANDARDS, INDEX 600. IF A DIRECT APPLICATION OF INDEX 600 IS PROPOSED, THE CONTRACTOR SHALL SUBMIT IN WRITING A STATEMENT INDICATING THE STANDARD INDEX AND PAGE NUMBER NO LESS THAN 10 BUSINESS DAYS PRIOR TO START OF CONSTRUCTION. WHEN A DIRECT APPLICATION OF FDOT STANDARD INDEX 600 IS NOT ACCEPTABLE A PROJECT SPECIFIC WORK ZONE TRAFFIC CONTROL PLAN SHALL BE PREPARED BY A FLORIDA PROFESSIONAL ENGINEER WHO HAS SUCCESSFULLY COMPLETED ADVANCED TRAINING IN MAINTENANCE OF TRAFFIC, AS DEFINED BY FDOT FOR APPROVAL BY THE COUNTY ENGINEER'S REPRESENTATIVE.

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES, WHETHER PUBLIC OR PRIVATE, PRIOR TO EXCAVATION. THE INFORMATION AND DATA SHOWN WITH RESPECT TO EXISTING UNDERGROUND FACILITIES AT OR CONTIGUOUS TO THE SITE IS APPROXIMATE AND BASED ON INFORMATION FURNISHED BY THE OWNER OF SUCH UNDERGROUND FACILITIES OR ON PHYSICAL APPURTENANCES OBSERVED IN THE FIELD. THE OWNER AND DESIGN PROFESSIONAL SHALL NOT BE RESPONSIBLE FOR THE ACCURACY AND COMPLETENESS OF ANY SUCH INFORMATION OR DATA. THE CONTRACTOR SHALL HAVE FULL RESPONSIBILITY FOR; REVIEWING AND CHECKING ALL SUCH INFORMATION AND DATA; LOCATING ALL UNDERGROUND FACILITIES DURING CONSTRUCTION; THE SAFETY AND PROTECTION THEREOF; REPAIRING ANY DAMAGE THERETO RESULTING FROM THE WORK. THE COST OF ALL WILL BE CONSIDERED AS HAVING BEEN INCLUDED IN THE CONTRACT PRICE. THE CONTRACTOR SHALL NOTIFY ANY AFFECTED UTILITY COMPANIES OR AGENCIES IN WRITING AT LEAST 48 HOURS PRIOR TO BEGINNING CONSTRUCTION.

C. PROTECTION OF EXISTING PLANT MATERIALS

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL UNAUTHORIZED CUTTING OR DAMAGE TO TREES AND SHRUBS EXISTING OR OTHERWISE, CAUSED BY CARELESS EQUIPMENT OPERATION, MATERIAL STOCKPILING, ETC... THIS SHALL INCLUDE COMPACTION BY DRIVING OR PARKING INSIDE THE DRIP-LINE AND SPILLING OIL, GASOLINE, OR OTHER DELETERIOUS MATERIALS WITHIN THE DRIP-LINE. NO MATERIALS SHALL BE BURNED ON SITE. EXISTING TREES KILLED OR DAMAGED SO THAT THEY ARE MISSHAPEN AND/OR UNSIGHTLY SHALL BE REPLACED AT THE COST TO THE CONTRACTOR OF THREE HUNDRED DOLLARS ($300) PER CALIPER INCH ON AN ESCALATING SCALE WHICH ADDS AN ADDITIONAL TWENTY (20) PERCENT PER INCH OVER FOUR (4) INCHES CALIPER AS FIXED AND AGREED LIQUIDATED DAMAGES. CALIPER SHALL BE MEASURED SIX (6) INCHES ABOVE GROUND LEVEL FOR TREES UP TO AND INCLUDING FOUR (4) INCHES IN CALIPER AND TWELVE (12) INCHES ABOVE GROUND LEVEL FOR TREES OVER FOUR (4) INCHES IN CALIPER.

2. SEE TREE MITIGATION PLAN AND NOTES, IF APPLICABLE.

D. MATERIALS

1. GENERAL

MATERIAL SAMPLES LISTED BELOW SHALL BE SUBMITTED FOR APPROVAL, ON SITE OR AS DETERMINED BY THE OWNER. UPON APPROVAL, DELIVERY OF MATERIALS MAY COMMENCE.

MATERIAL	SAMPLE SIZE
MULCH	ONE (1) CUBIC FOOT
TOPSOIL MIX	ONE (1) CUBIC FOOT
PLANTS	ONE (1) OF EACH VARIETY (OR TAGGED IN NURSERY)

2. PLANT MATERIALS

a. PLANT SPECIES AND SIZE SHALL CONFORM TO THOSE INDICATED ON THE DRAWINGS. ALL NURSERY STOCK SHALL BE IN ACCORDANCE WITH GRADES AND STANDARDS FOR NURSERY PLANTS, LATEST EDITION, PUBLISHED BY THE FLORIDA DEPARTMENT OF AGRICULTURE AND CONSUMER SERVICES. ALL PLANTS SHALL BE FLORIDA GRADE NO. 1 OR BETTER AS DETERMINED BY THE FLORIDA DIVISION OF PLANT INDUSTRY. ALL PLANTS SHALL BE HEALTHY, VIGOROUS, SOUND, WELL-BRANCHED, AND FREE OF DISEASE AND INSECTS, INSECT EGGS AND LARVAE AND SHALL HAVE ADEQUATE ROOT SYSTEMS. TREES FOR PLANTING IN ROWS SHALL BE UNIFORM IN SIZE AND SHAPE. ALL MATERIALS SHALL BE SUBJECT TO APPROVAL BY THE OWNER. WHERE ANY REQUIREMENTS ARE OMITTED FROM THE PLANT LIST, THE PLANTS FURNISHED SHALL BE NORMAL FOR THE VARIETY. PLANTS SHALL BE PRUNED PRIOR TO DELIVERY ONLY WITH APPROVAL FROM OWNER OR OWNER'S REPRESENTATIVE. NO SUBSTITUTIONS SHALL BE MADE WITHOUT WRITTEN PERMISSION FROM THE OWNER'S REPRESENTATIVE.

b. MEASUREMENTS: THE HEIGHT AND/OR WIDTH OF TREES SHALL BE MEASURED FROM THE GROUND OR ACROSS THE NORMAL SPREAD OF BRANCHES WITH THE PLANTS IN THEIR NORMAL POSITION. THIS MEASUREMENT SHALL NOT INCLUDE THE IMMEDIATE TERMINAL GROWTH. PLANTS LARGER IN SIZE THAN THOSE SPECIFIED IN THE PLANT LIST MAY BE USED IF APPROVED BY THE OWNER. IF THE USE OF LARGER PLANTS IS APPROVED, THE BALL OF EARTH OR SPREAD OF ROOTS SHALL BE INCREASED IN PROPORTION TO THE SIZE OF THE PLANT.

c. INSPECTION: PLANTS SHALL BE SUBJECT TO INSPECTION AND APPROVAL AT THE PLACE OF GROWTH, OR UPON DELIVERY TO THE SITE, AS DETERMINED BY THE OWNER. FOR QUALITY, SIZE, AND VARIETY. SUCH APPROVAL SHALL NOT IMPAIR THE RIGHT OF INSPECTION AND REJECTION AT THE SITE DURING PROGRESS OF THE WORK OR AFTER COMPLETION FOR SIZE AND CONDITION OF ROOT BALLS OR ROOTS, LATENT DEFECTS OR INJURIES. REJECTED PLANTS SHALL BE REMOVED IMMEDIATELY FROM THE SITE. NOTICE REQUESTING INSPECTION SHALL BE SUBMITTED IN WRITING BY THE CONTRACTOR AT LEAST ONE (1) WEEK PRIOR TO ANTICIPATED DATE.

E. SOIL MIXTURE (PLANTING MEDIUM, PLANTING MIX, TOPSOIL MIX)

1. CONTRACTOR SHALL TEST EXISTING SOIL AND AMEND AS NECESSARY IN ACCORDANCE WITH THE GUIDELINES BELOW:

2. SOIL MIXTURE (PLANTING MEDIUM FOR PLANT PITS) SHALL CONSIST OF TWO PARTS OF TOPSOIL AND ONE PART SAND, AS DESCRIBED BELOW. CONTRACTOR TO SUBMIT SAMPLES AND PH TESTING RESULTS OF SOIL MIXTURE FOR OWNER'S REPRESENTATIVE APPROVAL PRIOR TO PLANT INSTALLATION OPERATIONS COMMENCE.

a. TOPSOIL FOR USE IN PREPARING SOIL MIXTURE FOR BACKFILLING PLANT PITS SHALL BE FERTILE, FRIABLE, AND OF A LOAMY CHARACTER; REASONABLY FREE OF SUBSOIL, CLAY LUMPS, BRUSH WEEDS AND OTHER LITTER; FREE OF ROOTS, STUMPS, STONES LARGER THAN 2" IN ANY DIRECTION, AND OTHER EXTRANEOUS OR TOXIC MATTER HARMFUL TO PLANT GROWTH. IT SHALL CONTAIN THREE (3) TO FIVE (5) PERCENT DECOMPOSED ORGANIC MATTER AND HAVE A PH BETWEEN 5.5 AND 7.0.

b. SAND SHALL BE COARSE, CLEAN, WELL-DRAINING, NATIVE SAND.

2. TREES SHALL BE PLANTED IN THE EXISTING NATIVE SOIL ON SITE, UNLESS DETERMINED TO BE UNSUITABLE - AT WHICH POINT THE CONTRACTOR SHALL CONTACT OWNER'S REPRESENTATIVE TO DISCUSS ALTERNATE RECOMMENDATION PRIOR TO PLANTING.

F. WATER

1. WATER NECESSARY FOR PLANTING AND MAINTENANCE SHALL BE OF SATISFACTORY QUALITY TO SUSTAIN ADEQUATE PLANT GROWTH AND SHALL NOT CONTAIN HARMFUL, NATURAL OR MAN-MADE ELEMENTS DETRIMENTAL TO PLANTS. WATER MEETING THE ABOVE STANDARD SHALL BE OBTAINED ON THE SITE FROM THE OWNER, IF AVAILABLE, AND THE CONTRACTOR SHALL BE RESPONSIBLE TO MAKE ARRANGEMENTS FOR ITS USE BY HIS TANKS, HOSES, SPRINKLERS, ETC... IF SUCH WATER IS NOT AVAILABLE AT THE SITE, THE CONTRACTOR SHALL PROVIDE SATISFACTORY WATER FROM SOURCES OFF THE SITE AT NO ADDITIONAL COST TO THE OWNER.

* WATERING/IRRIGATION RESTRICTIONS MAY APPLY - REFER TO PROPERTY'S JURISDICTIONAL AUTHORITY.

G. FERTILIZER

CONTRACTOR SHALL PROVIDE FERTILIZER APPLICATION SCHEDULE TO OWNER, AS APPLICABLE TO SOIL TYPE, PLANT INSTALLATION TYPE, AND SITE'S PROPOSED USE. SUGGESTED FERTILIZER TYPES SHALL BE ORGANIC OR OTHERWISE NATURALLY-DERIVED.

* FERTILIZER RESTRICTIONS MAY APPLY - REFER TO PROPERTY'S JURISDICTIONAL AUTHORITY.

H. MULCH

1. MULCH MATERIAL SHALL BE MOISTENED AT THE TIME OF APPLICATION TO PREVENT WIND DISPLACEMENT, AND APPLIED AT A DEPTH OF THREE (3) INCHES. CLEAR MULCH FROM EACH PLANT'S CROWN (BASE). MULCH SHALL BE "FLORIMULCH," EUCALYPTUS MULCH, OR SIMILAR SUSTAINABLY HARVESTED MULCH UNLESS SPECIFIED OTHERWISE.

2. PROVIDE A THREE (3) INCH MINIMUM LAYER OF SPECIFIED MULCH OVER THE ENTIRE AREA OF EACH SHRUB BED, GROUND COVER, VINE BED, AND TREE PIT (6' MINIMUM) PLANTED UNDER THIS CONTRACT.

I. DIGGING AND HANDLING

1. PROTECT ROOTS OR ROOT BALLS OF PLANTS AT ALL TIMES FROM SUN, DRYING WINDS, WATER AND FREEZING, AS NECESSARY UNTIL PLANTING. PLANT MATERIALS SHALL BE ADEQUATELY PACKED TO PREVENT DAMAGE DURING TRANSIT. TREES TRANSPORTED MORE THAN TEN (10) MILES OR WHICH ARE NOT PLANTED WITHIN THREE (3) DAYS OF DELIVERY TO THE SITE SHALL BE SPRAYED WITH AN ANTITRANSPIRANT PRODUCT ("WILTPRUF" OR EQUAL) TO MINIMIZE TRANSPIRATIONAL WATER LOSS.

2. BALLED AND BURLAPPED (B&B), AND FIELD GROWN (FG) PLANTS SHALL BE DUG WITH FIRM, NATURAL BALLS OF SOIL OF SUFFICIENT SIZE TO ENCOMPASS THE FIBROUS AND FEEDING ROOTS OF THE PLANTS. NO PLANTS MOVED WITH A ROOT BALL SHALL BE PLANTED IF THE BALL IS CRACKED OR BROKEN. PLANTS SHALL NOT BE HANDLED BY STEMS.

3. PLANTS MARKED "BR" IN THE PLANT LIST SHALL BE DUG WITH BARE ROOTS. CARE SHALL BE EXERCISED THAT THE ROOTS DO NOT DRY OUT DURING TRANSPORTATION AND PRIOR TO PLANTING.

4. PROTECTION OF PALMS: ONLY A MINIMUM OF FRONDS SHALL BE REMOVED FROM THE CROWN OF THE PALM TREES TO FACILITATE MOVING AND HANDLING. CLEAR TRUNK (CT) SHALL BE AS SPECIFIED AFTER THE MINIMUM OF FRONDS HAVE BEEN REMOVED. ALL PALMS SHALL BE BRACED PER PALM PLANTING DETAIL.

5. EXCAVATION OF TREE PITS SHALL BE PERFORMED USING EXTREME CARE TO AVOID DAMAGE TO SURFACE AND SUBSURFACE ELEMENTS SUCH AS UTILITIES OR HARDSCAPE ELEMENTS, FOOTERS AND PREPARED SUB-BASES.

J. CONTAINER GROWN STOCK

1. ALL CONTAINER GROWN MATERIAL SHALL BE HEALTHY, VIGOROUS, WELL-ROOTED PLANTS ESTABLISHED IN THE CONTAINER IN WHICH THEY ARE SOLD. THE PLANTS SHALL HAVE TOPS WHICH ARE OF GOOD QUALITY AND ARE IN A HEALTHY GROWING CONDITION.

2. AN ESTABLISHED CONTAINER GROWN PLANT SHALL BE TRANSPLANTED INTO A CONTAINER AND GROWN IN THAT CONTAINER SUFFICIENTLY LONG ENOUGH FOR THE NEW FIBROUS ROOTS TO HAVE DEVELOPED SO THAT THE ROOT MASS WILL RETAIN ITS SHAPE AND HOLD TOGETHER WHEN REMOVED FROM THE CONTAINER. CONTAINER GROWN STOCK SHALL NOT BE HANDLED BY THEIR STEMS.

3. ROOT BOUND PLANTS ARE NOT ACCEPTABLE AND WILL BE REJECTED.

4. RPG= "ROOTS PLUS GROWER" CONTAINER PRODUCTS SHALL BE USED WHERE SPECIFIED.

K. COLLECTED STOCK

WHEN THE USE OF COLLECTED STOCK IS PERMITTED AS INDICATED BY THE OWNER OR OWNER'S REPRESENTATIVE, THE MINIMUM SIZES OF ROOTBALLS SHALL BE EQUAL TO THAT SPECIFIED FOR THE NEXT LARGER SIZE OF NURSERY GROWN STOCK OF THE SAME VARIETY.

L. NATIVE STOCK

PLANTS COLLECTED FROM WILD OR NATIVE STANDS SHALL BE CONSIDERED NURSERY GROWN WHEN THEY HAVE BEEN SUCCESSFULLY RE-ESTABLISHED IN A NURSERY ROW AND GROWN UNDER REGULAR NURSERY CULTURAL PRACTICES FOR A MINIMUM OF TWO (2) GROWING SEASONS AND HAVE ATTAINED ADEQUATE ROOT AND TOP GROWTH TO INDICATE FULL RECOVERY FROM TRANSPLANTING INTO THE NURSERY ROW.

M. MATERIALS LIST

QUANTITIES NECESSARY TO COMPLETE THE WORK ON THE DRAWINGS SHALL BE FURNISHED BY THE CONTRACTOR. QUANTITY ESTIMATES HAVE BEEN MADE CAREFULLY, BUT THE LANDSCAPE ARCHITECT OR OWNER ASSUMES NO LIABILITY FOR OMISSIONS OR ERRORS. SHOULD A DISCREPANCY OCCUR BETWEEN THE PLANS AND THE PLANT LIST QUANTITY, THE OWNER'S REPRESENTATIVE SHALL BE NOTIFIED FOR CLARIFICATION PRIOR TO BIDDING OR INSTALLATION. ALL DIMENSIONS AND/OR SIZES SPECIFIED SHALL BE THE MINIMUM ACCEPTABLE SIZE.

N. FINE GRADING

1. FINE GRADING UNDER THIS CONTRACT SHALL CONSIST OF FINAL FINISHED GRADING OF LAWN AND PLANTING AREAS THAT HAVE BEEN ROUGH GRADED BY OTHERS. BERMING AS SHOWN ON THE DRAWINGS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR, UNLESS OTHERWISE NOTED.

2. THE CONTRACTOR SHALL FINE GRADE THE LAWN AND PLANTING AREAS TO BRING THE ROUGH GRADE UP TO FINAL FINISHED GRADE ALLOWING FOR THICKNESS OF SOD AND/OR MULCH DEPTH. CONTRACTOR SHALL FINE GRADE BY HAND AND/OR WITH ALL EQUIPMENT NECESSARY INCLUDING A GRADING TRACTOR WITH FRONT-END LOADER FOR TRANSPORTING SOIL WITHIN THE SITE.

3. ALL PLANTING AREAS SHALL BE GRADED AND MAINTAINED FOR POSITIVE DRAINAGE TO SURFACE/SUBSURFACE STORM DRAIN SYSTEMS. AREAS ADJACENT TO BUILDINGS SHALL SLOPE AWAY FROM THE BUILDINGS. REFER TO CIVIL ENGINEER'S PLANS FOR FINAL GRADES, IF APPLICABLE.

O. PLANTING PROCEDURES

1. CLEANING UP BEFORE COMMENCING WORK: THE CONTRACTOR SHALL CLEAN WORK AND SURROUNDING AREAS OF ALL RUBBISH OR OBJECTIONABLE MATTER DAILY. ALL MORTAR, CEMENT, AND TOXIC MATERIAL SHALL BE REMOVED FROM THE SURFACE OF ALL PLANT BEDS. THESE MATERIALS SHALL NOT BE MIXED WITH THE SOIL. SHOULD THE CONTRACTOR FIND SUCH SOIL CONDITIONS BENEATH THE SOIL WHICH WILL IN ANY WAY ADVERSELY AFFECT THE PLANT GROWTH, HE SHALL IMMEDIATELY CALL IT TO THE ATTENTION OF THE OWNER'S REPRESENTATIVE. FAILURE TO DO SO BEFORE PLANTING SHALL MAKE THE CORRECTIVE MEASURES THE RESPONSIBILITY OF THE CONTRACTOR.

2. VERIFY LOCATIONS OF ALL UTILITIES, CONDUITS, SUPPLY LINES AND CABLES, INCLUDING BUT NOT LIMITED TO: ELECTRIC, GAS (LINES AND TANKS), WATER, SANITARY SEWER, STORMWATER SYSTEMS, CABLE, AND TELEPHONE. PROPERLY MAINTAIN AND PROTECT EXISTING UTILITIES. CALL SUNSHINE STATE ONE CALL OF FLORIDA, INC. (811) TO LOCATE UTILITIES AT LEAST 48 HOURS PRIOR TO CONSTRUCTION.

3. SUBGRADE EXCAVATION: CONTRACTOR IS RESPONSIBLE TO REMOVE ALL EXISTING AND IMPORTED LIMEROCK AND LIMEROCK SUB-BASE FROM ALL LANDSCAPE PLANTING AREAS TO A MINIMUM DEPTH OF 36" OR TO NATIVE SOIL. CONTRACTOR IS RESPONSIBLE TO BACKFILL THESE PLANTING AREAS TO ROUGH FINISHED GRADE WITH CLEAN TOPSOIL FROM AN ON-SITE SOURCE OR AN IMPORTED SOURCE. IF LIMEROCK OR OTHER ADVERSE CONDITIONS OCCUR IN PLANTED AREAS AFTER 36" DEEP EXCAVATION BY THE CONTRACTOR, AND POSITIVE DRAINAGE CAN NOT BE ACHIEVED, CONTRACTOR SHALL UTILIZE POOR DRAINAGE CONDITION PLANTING DETAIL.

4. FURNISH NURSERY'S CERTIFICATE OF COMPLIANCE WITH ALL REQUIREMENTS AS SPECIFIED HEREIN. INSPECT AND SELECT PLANT MATERIALS BEFORE PLANTS ARE DUG AT NURSERY OR GROWING SITE.

5. COMPLY WITH APPLICABLE FEDERAL, STATE, COUNTY, AND LOCAL REGULATIONS GOVERNING LANDSCAPE MATERIALS AND WORK. CONFORM TO ACCEPTED HORTICULTURAL PRACTICES AS USED IN THE TRADE. UPON ARRIVAL AT THE SITE, PLANTS SHALL BE THOROUGHLY WATERED AND PROPERLY MAINTAINED UNTIL PLANTED. PLANTS STORED ONSITE SHALL NOT REMAIN UNPLANTED OR APPROPRIATELY HEALED IN FOR A PERIOD EXCEEDING TWENTY-FOUR (24) HOURS. AT ALL TIMES WORKMANLIKE METHODS CUSTOMARY IN GOOD HORTICULTURAL PRACTICES SHALL BE EXERCISED.

6. THE WORK SHALL BE COORDINATED WITH OTHER TRADES TO PREVENT CONFLICTS. COORDINATE PLANTING WITH IRRIGATION WORK TO ASSURE AVAILABILITY OF WATER AND PROPER LOCATION OF IRRIGATION APPURTENANCES AND PLANTS.

7. ALL PLANTING PITS SHALL BE EXCAVATED TO SIZE AND DEPTH IN ACCORDANCE WITH THE USA STANDARD FOR NURSERY STOCK 260.1, UNLESS SHOWN OTHERWISE ON THE DRAWINGS, AND BACK FILLED WITH THE PREPARED PLANTING SOIL MIXTURE AS SPECIFIED IN SECTION E. TEST ALL TREE PITS WITH WATER BEFORE PLANTING TO ASSURE PROPER DRAINAGE PERCOLATION IS AVAILABLE. NO ALLOWANCE WILL BE MADE FOR LOST PLANTS DUE TO IMPROPER DRAINAGE. IF POOR DRAINAGE EXISTS, UTILIZE "POOR DRAINAGE CONDITION" PLANTING DETAIL. TREES SHALL BE SET PLUMB AND HELD IN POSITION UNTIL THE PLANTING MIXTURE HAS BEEN FLUSHED INTO PLACE WITH A SLOW, FULL HOSE STREAM. ALL PLANTING SHALL BE PERFORMED BY PERSONNEL FAMILIAR WITH PLANTING PROCEDURES AND UNDER THE SUPERVISION OF A QUALIFIED LANDSCAPE FOREMAN. PROPER "JETTING IN" SHALL BE ASSURED TO ELIMINATE AIR POCKETS AROUND THE ROOTS. "JET STICK" OR EQUAL IS RECOMMENDED.

8. TAKE ALL NECESSARY PRECAUTIONS TO AVOID DAMAGE TO BUILDINGS AND BUILDING STRUCTURES WHILE INSTALLING TREES.

9. SOIL MIXTURE SHALL BE AS SPECIFIED IN SECTION E OF THESE SPECIFICATIONS.

10. TREES AND SHRUBS SHALL BE SET STRAIGHT AT AN ELEVATION THAT, AFTER SETTLEMENT, THE PLANT CROWN WILL STAND ONE (1) TO TWO (2) INCHES ABOVE GRADE. EACH PLANT SHALL BE SET IN THE CENTER OF THE PIT. PLANTING SOIL MIXTURE SHALL BE BACK FILLED, THOROUGHLY TAMPED AROUND THE BALL, AND SETTLED BY WATER (AFTER TAMPING).

11. AMEND PINE AND OAK PLANT PITS WITH ECTOMYCORRHIZAL SOIL APPLICATION PER MANUFACTURER'S RECOMMENDATION. ALL OTHER PLANT PITS SHALL BE AMENDED WITH ENDOMYCORRHIZAL SOIL APPLICATION PER MANUFACTURER'S RECOMMENDATION. PROVIDE PRODUCT INFORMATION SUBMITTAL PRIOR TO INOCULATION.

12. FILL HOLE WITH SOIL MIXTURE, MAKING CERTAIN ALL SOIL IS SATURATED. TO DO THIS, FILL HOLE WITH WATER AND ALLOW TO SOAK MINIMUM TWENTY (20) MINUTES, STIRRING IF NECESSARY TO GET SOIL THOROUGHLY WET. PACK LIGHTLY WITH FEET, ADD MORE WET SOIL MIXTURE. DO NOT COVER TOP OF BALL WITH SOIL MIXTURE. ALL BURLAP, ROPE, WIRES, BASKETS, ETC.,, SHALL BE REMOVED FROM THE SIDES AND TOPS OF BALLS, BUT NO BURLAP SHALL BE PULLED FROM UNDERNEATH.

13. TREES SHALL BE PRUNED, AT THE DIRECTION OF THE OWNER OR OWNER'S REPRESENTATIVE, TO PRESERVE THE NATURAL CHARACTER OF THE PLANT. ALL SOFT WOOD OR SUCKER GROWTH AND ALL BROKEN OR BADLY DAMAGED BRANCHES SHALL BE REMOVED WITH A CLEAN CUT. ALL PRUNING TO BE PERFORMED BY CERTIFIED ARBORIST, IN ACCORDANCE WITH ANSI A-300.

14. SHRUBS AND GROUND COVER PLANTS SHALL BE EVENLY SPACED IN ACCORDANCE WITH THE DRAWINGS AND AS INDICATED ON THE PLANT LIST. MATERIALS INSTALLED SHALL MEET MINIMUM SPECIMEN REQUIREMENTS OR QUANTITIES SHOW ON PLANS, WHICHEVER IS GREATER. CULTIVATE ALL PLANTING AREAS TO A MINIMUM DEPTH OF 6", REMOVE AND DISPOSE ALL DEBRIS. MIX TOP 4" THE PLANTING SOIL MIXTURE AS SPECIFIED IN SECTION E. THOROUGHLY WATER ALL PLANTS AFTER INSTALLATION.

15. TREE GUYING AND BRACING SHALL BE INSTALLED BY THE CONTRACTOR IN ACCORDANCE WITH THE PLANS TO INSURE STABILITY AND MAINTAIN TREES IN AN UPRIGHT POSITION. IF THE CONTRACTOR AND OWNER DECIDE TO WAIVE THE TREE GUYING AND BRACING, THE OWNER SHALL NOTIFY THE PROJECT LANDSCAPE ARCHITECT IN WRITING AND AGREE TO INDEMNIFY AND HOLD HARMLESS THE PROJECT LANDSCAPE ARCHITECT IN THE EVENT UNSUPPORTED TREES PLANTED UNDER THIS CONTRACT FALL AND DAMAGE PERSON OR PROPERTY.

16. ALL PLANT BEDS SHALL BE KEPT FREE OF NOXIOUS WEEDS UNTIL FINAL ACCEPTANCE OF WORK. IF DIRECTED BY THE OWNER, "ROUND-UP" SHALL BE APPLIED FOR WEED CONTROL BY QUALIFIED PERSONNEL TO ALL PLANTING AREAS IN SPOT APPLICATIONS PER MANUFACTURER'S PRECAUTIONS AND SPECIFICATIONS. PRIOR TO FINAL INSPECTION, TREAT ALL PLANTING BEDS WITH AN APPROVED PRE-EMERGENT HERBICIDE AT AN APPLICATION RATE RECOMMENDED BY THE MANUFACTURER. (AS ALLOWED BY JURISDICTIONAL AUTHORITY)

P. LAWN SODDING

1. THE WORK CONSISTS OF LAWN BED PREPARATION, SOIL PREPARATION, AND SODDING COMPLETE, IN STRICT ACCORDANCE WITH THE SPECIFICATIONS AND THE APPLICABLE DRAWINGS TO PRODUCE A TURF GRASS LAWN ACCEPTABLE TO THE OWNER.

2. ALL AREAS THAT ARE TO BE SODDED SHALL BE CLEARED OF ANY ROUGH GRASS, WEEDS, AND DEBRIS BY MEANS OF A SOD CUTTER TO A DEPTH OF THREE (3) INCHES, AND THE GROUND BROUGHT TO AN EVEN GRADE. THE ENTIRE SURFACE SHALL BE ROLLED WITH A ROLLER WEIGHING NOT MORE THAN ONE-HUNDRED (100) POUNDS PER FOOT OF WIDTH. DURING THE ROLLING, ALL DEPRESSIONS CAUSED BY SETTLEMENT SHALL BE FILLED WITH ADDITIONAL SOIL, AND THE SURFACE SHALL BE REGRADED AND ROLLED UNTIL PRESENTING A SMOOTH AND EVEN FINISH TO THE REQUIRED GRADE.

3. PREPARE LOOSE BED FOUR (4) INCHES DEEP. HAND RAKE UNTIL ALL BUMPS AND DEPRESSIONS ARE REMOVED. WET PREPARED AREA THOROUGHLY.

4. SODDING

a. THE CONTRACTOR SHALL SOD ALL AREAS THAT ARE NOT PAVED OR PLANTED AS DESIGNATED ON THE DRAWINGS WITHIN THE CONTRACT LIMITS, UNLESS SPECIFICALLY NOTED OTHERWISE.

b. THE SOD SHALL BE CERTIFIED TO MEET FLORIDA STATE PLANT BOARD SPECIFICATIONS, ABSOLUTELY TRUE TO VARIETAL TYPE, AND FREE FROM WEEDS, FUNGUS, INSECTS AND DISEASE OF ANY KIND.

c. SOD PANELS SHALL BE LAID TIGHTLY TOGETHER SO AS TO MAKE A SOLID SODDED LAWN AREA. SOD SHALL BE LAID UNIFORMLY AGAINST THE EDGES OF ALL CURBS AND OTHER HARDSCAPE ELEMENTS, PAVED AND PLANTED AREAS. ADJACENT TO BUILDINGS, A 24 INCH STONE MULCH STRIP SHALL BE PROVIDED. IMMEDIATELY FOLLOWING SOD LAYING, THE LAWN AREAS SHALL BE ROLLED WITH A LAWN ROLLER CUSTOMARILY USED FOR SUCH PURPOSES, AND THEN THOROUGHLY IRRIGATED. IF, IN THE OPINION OF THE OWNER, TOP-DRESSING IS NECESSARY AFTER ROLLING TO FILL THE VOIDS BETWEEN THE SOD PANELS AND TO EVEN OUT INCONSISTENCIES IN THE SOD, CLEAN SAND, AS APPROVED BY THE OWNER'S REPRESENTATIVE, SHALL BE UNIFORMLY SPREAD OVER THE ENTIRE SURFACE OF THE SOD AND THOROUGHLY WATERED IN. FERTILIZE INSTALLED SOD AS ALLOWED BY PROPERTY'S JURISDICTIONAL AUTHORITY.

5. DURING DELIVERY, PRIOR TO, AND DURING THE PLANTING OF THE LAWN AREAS, THE SOD PANELS SHALL AT ALL TIMES BE PROTECTED FROM EXCESSIVE DRYING AND UNNECESSARY EXPOSURE OF THE ROOTS TO THE SUN. ALL SOD SHALL BE STACKED SO AS NOT TO BE DAMAGED BY SWEATING OR EXCESSIVE HEAT AND MOISTURE.

6. LAWN MAINTENANCE

a. WITHIN THE CONTRACT LIMITS, THE CONTRACTOR SHALL PRODUCE A DENSE, WELL ESTABLISHED LAWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR AND RE-SODDING OF ALL ERODED, SUNKEN OR BARE SPOTS (LARGER THAN 12"X12") UNTIL CERTIFICATION OF ACCEPTANCE BY THE OWNER'S REPRESENTATIVE. REPAIRED SODDING SHALL BE ACCOMPLISHED AS IN THE ORIGINAL WORK (INCLUDING REGRADING IF NECESSARY).

b. CONTRACTOR RESPONSIBLE FOR ESTABLISHING AND MAINTAINING SOD/LAWN UNTIL ACCEPTANCE BY THE OWNER'S REPRESENTATIVE. PRIOR TO AND UPON ACCEPTANCE, CONTRACTOR TO PROVIDE WATERING/IRRIGATION SCHEDULE TO OWNER. OBSERVE ALL APPLICABLE WATERING RESTRICTIONS AS SET FORTH BY THE PROPERTY'S JURISDICTIONAL AUTHORITY.

Q. CLEANUP

UPON COMPLETION OF ALL PLANTING WORK AND BEFORE FINAL ACCEPTANCE, THE CONTRACTOR SHALL REMOVE ALL MATERIAL, EQUIPMENT, AND DEBRIS RESULTING FROM HIS WORK. ALL PAVED AREAS SHALL BE CLEANED AND THE SITE LEFT IN A NEAT AND ACCEPTABLE CONDITION AS APPROVED BY THE OWNER'S REPRESENTATIVE.

R. PLANT MATERIAL MAINTENANCE

ALL PLANTS AND PLANTING INCLUDED UNDER THIS CONTRACT SHALL BE MAINTAINED BY WATERING, CULTIVATING, SPRAYING, AND ALL OTHER OPERATIONS (SUCH AS RE-STAKING OR REPAIRING GUY SUPPORTS) NECESSARY TO INSURE A HEALTHY PLANT CONDITION BY THE CONTRACTOR UNTIL CERTIFICATION OF ACCEPTANCE BY THE OWNER'S REPRESENTATIVE.

S. FINAL INSPECTION AND ACCEPTANCE OF WORK

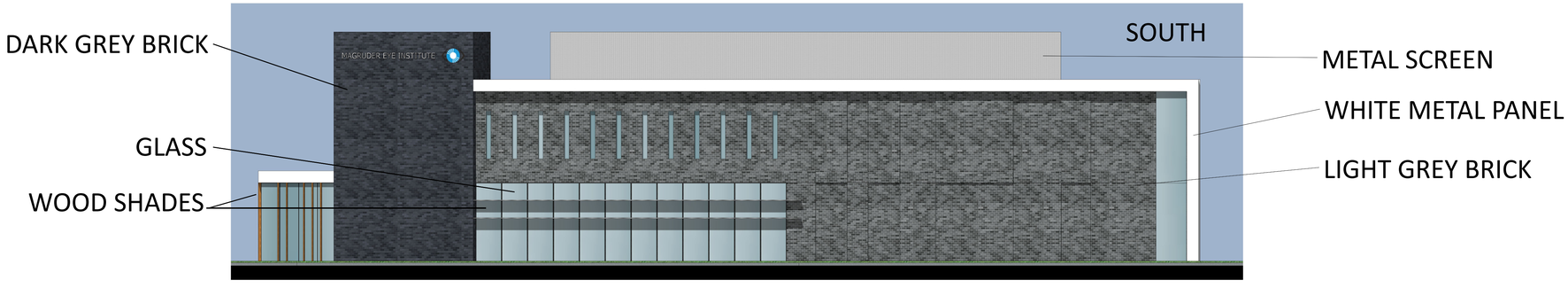
FINAL INSPECTION AT THE END OF THE WARRANTY PERIOD SHALL BE ON PLANTING, CONSTRUCTION AND ALL OTHER INCIDENTAL WORK PERTAINING TO THIS CONTRACT. ANY REPLACEMENT AT THIS TIME SHALL BE SUBJECT TO THE SAME ONE (1) YEAR WARRANTY (OR AS SPECIFIED BY THE LANDSCAPE ARCHITECT OR OWNER IN WRITING) BEGINNING WITH THE TIME OF REPLACEMENT AND ENDING WITH THE SAME INSPECTION AND ACCEPTANCE HEREIN DESCRIBED.

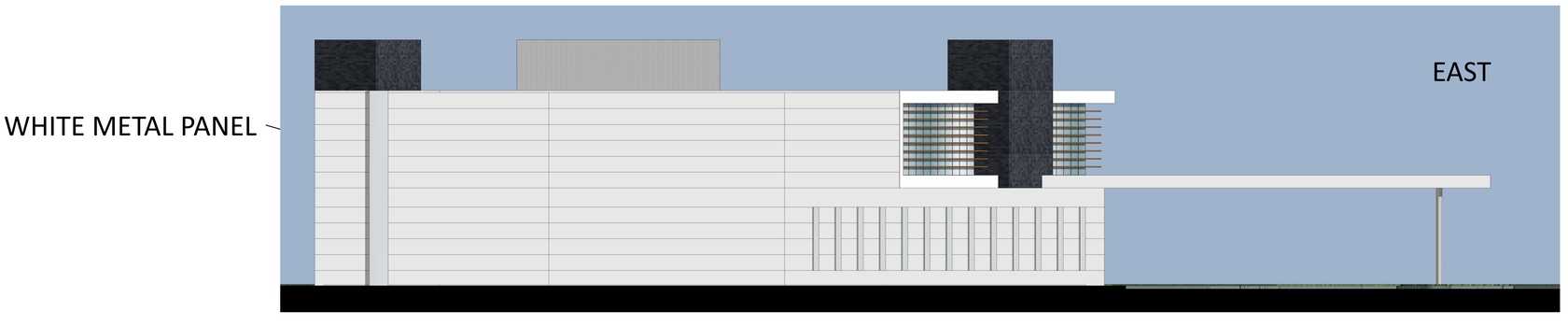
T. WARRANTY

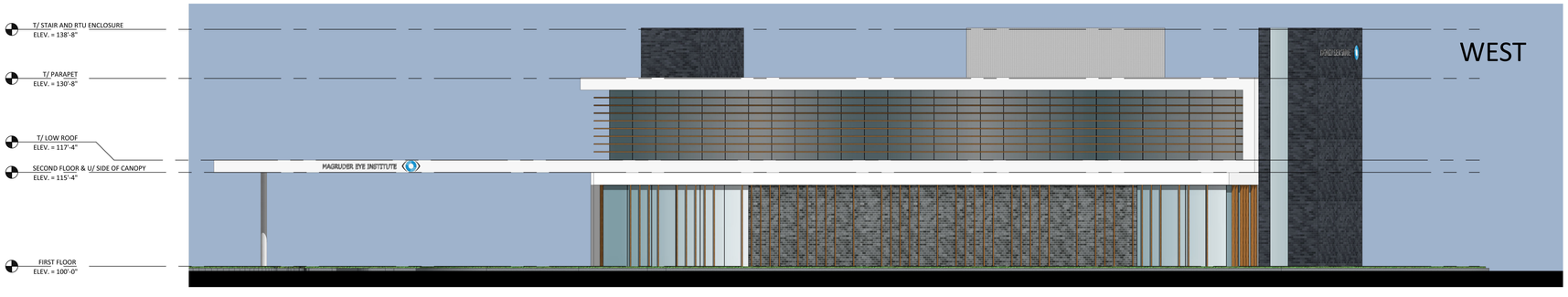
1. THE LIFE AND SATISFACTORY CONDITION OF ALL PLANT MATERIAL INSTALLED (INCLUDING SOD) BY THE LANDSCAPE CONTRACTOR SHALL BE WARRANTED BY THE CONTRACTOR FOR A MINIMUM OF ONE (1) CALENDAR YEAR COMMENCING AT THE TIME OF CERTIFICATION OF ACCEPTANCE BY THE OWNER'S REPRESENTATIVE.

2. ANY PLANT NOT FOUND IN A HEALTHY GROWING CONDITION AT THE END OF THE WARRANTY PERIOD SHALL BE REMOVED FROM THE SITE AND REPLACED AS SOON AS WEATHER CONDITIONS PERMIT. ALL REPLACEMENTS SHALL BE PLANTS OF THE SAME KIND AND SIZE AS SPECIFIED IN THE PLANT LIST. THEY SHALL BE FURNISHED PLANTED AND MULCHED AS SPECIFIED AT NO ADDITIONAL COST TO THE OWNER.

3. IN THE EVENT THE OWNER DOES NOT CONTRACT WITH THE CONTRACTOR FOR LANDSCAPE AND IRRIGATION MAINTENANCE, THE CONTRACTOR SHOULD VISIT THE PROJECT SITE PERIODICALLY DURING THE ONE (1) YEAR WARRANTY PERIOD TO EVALUATE MAINTENANCE PROCEDURES BEING PERFORMED BY THE OWNER. CONTRACTOR SHALL NOTIFY THE OWNER IN WRITING OF MAINTENANCE PROCEDURES OR CONDITIONS WHICH TH







ECKERT WORDELL

MAGRUDER EYE INSTITUTE

0 8

© ECKERT WORDELL LLC

02 NOVEMBER 2020

MAGRUDER EYE INSTITUTE

Traffic Impact Analysis

December 2020

Kimley»Horn

TRAFFIC IMPACT ANALYSIS

Magruder Eye Institute

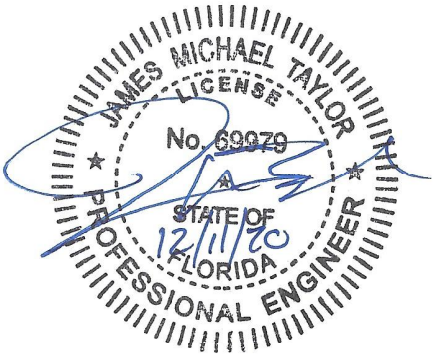
City of Winter Park, FL

Prepared for:

G. Brock Magruder, M.D., P.A.

Prepared by:

Kimley-Horn and Associates, Inc.



December 2020

James M. Taylor, P.E.

PE #69979

©Kimley-Horn and Associates, Inc. 2020

Kimley»Horn

Table of Contents

1.0 INTRODUCTION 1

1.1 Study Area 1

2.0 EXISTING CONDITIONS ANALYSIS 3

2.1 Existing Traffic Counts 3

2.2 Existing Intersection Conditions 3

3.0 PROJECT DEVELOPMENT 5

3.1 Site Access 5

3.2 Trip Generation 5

3.3 Trip Distribution 7

3.4 Trip Assignment 7

4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2021 11

4.1 Background Traffic 11

4.2 Background Intersection Analysis 11

5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2021 12

5.1 Buildout Traffic 12

5.2 Buildout Intersection Analysis 12

6.0 CONCLUSION 16

List of Figures

Figure 1: Project Location and Study Area 2
Figure 2: Project Distribution 8
Figure 3: AM Peak Hour Project Assignment 9
Figure 4: PM Peak Hour Project Assignment 10
Figure 5: AM Peak Hour Buildout (2021) Volumes 13
Figure 6: PM Peak Hour Buildout (2021) Volumes 14

List of Tables

Table 1: COVID-19 Adjustment Factor 3
Table 2: Existing Intersection Conditions (AM Peak Hour) 4
Table 3: Existing Intersection Conditions (PM Peak Hour) 4
Table 4: Magruder Eye Institute Trip Generation 6
Table 5: Background Intersection Conditions (AM Peak Hour) 11
Table 6: Background Intersection Conditions (PM Peak Hour) 11
Table 7: Buildout Intersection Conditions (AM Peak Hour) 15
Table 8: Buildout Intersection Conditions (PM Peak Hour) 15

Appendices

Appendix A: Conceptual Site Plan
Appendix B: Turning Movement Counts
Appendix C: Data from Orange County & FDOT's Florida Traffic Online (FTO)
Appendix D: Turning Movement Volume Worksheets
Appendix E: Synchro Outputs
Appendix F: CFRPM Model Plot

1.0 INTRODUCTION

Kimley-Horn has been retained by G. Brock Magruder, M.D., P.A., to analyze and document the traffic impacts associated with the development of Magruder Eye Institute, a proposed site redevelopment of several parcels generally located on the northeast corner of W Fairbanks Avenue and Cambridge Boulevard in the City of Winter Park, Florida.

Current zoning of the parcels to be redeveloped include Commercial (C-3) and Single Family Residential (R-1A). Proposed zoning is C-3, Office (O-1) and parking lot (PL). The site is composed of seven (7) separate parcels (Parcel ID #02-22-29-2996-01-010, 11-22-29-6260-05-010, 11-22-29-6260-05-020, 02-22-29-2996-01-040, 02-22-29-2996-01-050, 02-22-29-2996-01-069, and 02-22-29-2996-02-060. The project location and study area are shown in Figure 1.

Based on 2020 property records from the Orange County Property Appraiser, the site is currently occupied by seven (7) single family homes and 6,492 square feet of retail. The applicant is proposing to redevelop the site to consist of a 32,000 square foot medical office building and off-site parking. The total area of the site being redeveloped is approximately 2.01 acres. The conceptual site plan is provided in Appendix A.

1.1 STUDY AREA

The study area includes the project driveway and the intersection of W Fairbanks Avenue and Cambridge Boulevard, as shown in Figure 1.

Figure 1: Project Location and Study Area

December 2020
Project No.: 049610000

Kimley»Horn

© 2020 Kimley-Horn and Associates, Inc.
189 S Orange Ave, Suite 1000, Orlando, FL, 32801
Phone: (407)-898-1511

2.0 EXISTING CONDITIONS ANALYSIS

2.1 EXISTING TRAFFIC COUNTS

Turning movement counts (TMCs) were collected at the study intersection on Wednesday, November 4, 2020 during the AM (7:00AM – 9:00AM) and PM (4:00PM – 6:00PM) peak periods. Raw turning movement counts are provided in Appendix B.

Traffic counts were adjusted using the seasonal factor (SF) from FDOT's Florida Traffic Online (FTO) publication. To achieve pre-COVID-19 traffic conditions, an adjustment factor, which compares Year 2019 AADT along W Fairbanks Avenue to recently collected Year 2020 ADT, was also applied to the raw turning movement counts along W Fairbanks Avenue. AADT data provided by Orange County and seasonal factor data provided by FDOT are included in Appendix C.

Table 1 shows the COVID-19 adjustment factor and the parameters used to develop it. The adjusted turning movement volume worksheet for the study intersection can be found in Appendix D.

Table 1: COVID-19 Adjustment Factor

Year 2019 AADT [1]	32,220
Year 2020 Observed ADT	28,496
COVID 19 Adjustment Factor	1.13

[1] *AADT from Orange County Traffic Counts online database*

2.2 EXISTING INTERSECTION CONDITIONS

An intersection operational analysis was performed for existing (2020) conditions in the AM and PM peak hours using procedures outlined in the *Highway Capacity Manual,* 6th Edition (HCM 6) with *Synchro* (v10) software. Intersection level of service (LOS), maximum volume to capacity (v/c) ratios, and delay for the existing AM and PM peak hour conditions are provided in Tables 2 and 3, respectively. Synchro outputs are provided in Appendix E.

As shown in Tables 2 and 3, all study area intersection approaches are shown to operate with acceptable LOS and volume to capacity (v/c) ratios less than 1.0 during existing peak hour conditions with the exception of the stop-controlled approaches of Cambridge Boulevard, which operate at LOS F. This result is common when a minor street stop-controlled approach crosses a high-volume major street free-flow approach during peak period.

Table 2: Existing Intersection Conditions (AM Peak Hour)

Existing Condition - 2020					
Intersection	Control Type	Approach	AM Peak Hour		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
W Fairbanks Avenue & Cambridge Boulevard	Unsignalized (TWSC)	EB (L)	B	EBL	0.09
		WB (L)	B	WBL	0.00
		NB	F	NBL	0.12
		SB	F	SBL	1.40
		Overall	-	SBL	1.40

Table 3: Existing Intersection Conditions (PM Peak Hour)

Existing Condition - 2020					
Intersection	Control Type	Approach	PM Peak Hour		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
W Fairbanks Avenue & Cambridge Boulevard	Unsignalized (TWSC)	EB (L)	C	EBL	0.05
		WB (L)	B	WBL	0.00
		NB	F	NBL	0.26
		SB	F	SBL	0.92
		Overall	-	SBL	0.92

3.0 PROJECT DEVELOPMENT

The applicant is proposing to redevelop the site to consist of a 32,000 square foot medical office building and off-site parking. The total area of the site being redeveloped is approximately 2.01 acres. The latest industry standards were referenced to evaluate the amount of new external trips to be generated by the site at buildout.

3.1 SITE ACCESS

Access to the site is proposed via one (1) full-access driveway along Cambridge Avenue, as shown in the site plan.

3.2 TRIP GENERATION

Trip generation rates for the existing and proposed development scenarios were calculated using the 10th Edition of the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*. Land Use Codes (LUCs) used for the existing and proposed development scenarios include:

Existing Development
- LUC 210 – Single-Family Detached Housing
- LUC 820 – Shopping Center (Retail)

Proposed Redevelopment
- LUC 720 – Medical-Dental Office Building

Per City code requirement, Table 4 provides the Daily, AM peak hour, and PM peak hour trip generation summaries for the existing and proposed development scenarios to the net new vehicle trips anticipated to be added to the roadway network.

As shown in Table 4, the proposed redevelopment of the site is anticipated to generate 116 net new daily trips, 65 net new AM peak hour trips (56 inbound and 9 outbound), and 31 net new PM peak hour trips (-9 inbound and 40 outbound) to the external roadway network at buildout.

Table 4: Magruder Eye Institute Trip Generation

PROPOSED REDEVELOPMENT																					
Land Use	ITE LUC	Size	Units	Daily ITE Trip Rate[1]	AM ITE Trip Rate[1]	PM ITE Trip Rate[1]	Daily Trip Generation					AM Peak Hour Trip Generation					PM Peak Hour Trip Generation				
							Total	In[1]		Out[1]		Total	In[1]		Out[1]		Total	In[1]		Out[1]	
Medical-Dental Office Building	720	32.0	KSF	35.68	2.53	3.46	1,142	50%	571	50%	571	81	78%	63	22%	18	111	28%	31	72%	80
				New External Trips			1,142	571		571		81	63		18		111	31		80	
EXISTING DEVELOPMENT																					
Land Use	ITE LUC	Size	Units	Daily ITE Trip Rate[1]	AM ITE Trip Rate[1]	PM ITE Trip Rate[1]	Daily Trip Generation					AM Peak Hour Trip Generation					PM Peak Hour Trip Generation				
							Total	In[1]		Out[1]		Total	In[1]		Out[1]		Total	In[1]		Out[1]	
Single Family Detached Housing	210	7	DU	12.86	1.40	1.13	90	50%	45	50%	45	10	25%	3	75%	7	8	63%	5	37%	3
Retail	820	6.49	KSF	144.23	0.94	11.06	936	50%	468	50%	468	6	62%	4	38%	2	72	48%	35	52%	37
				External Trips			1,026	513		513		16	7		9		80	40		40	
NET NEW EXTERNAL TRIPS																					
							Daily Trip Generation					AM Peak Hour Trip Generation					PM Peak Hour Trip Generation				
							Total	In[1]		Out[1]		Total	In[1]		Out[1]		Total	In[1]		Out[1]	
				Net New External Trips			116	58		58		65	56		9		31	-9		40	

[1] Vehicle trip rates and directional splits per data and procedures outlined in ITE Trip Generation Manual, 10th Edition

3.3 TRIP DISTRIBUTION

Projected traffic demand of project trips on study roadways was derived with use of the latest adopted regional travel demand model. Land use data for the project was entered into a new traffic analysis zone (TAZ) within the Central Florida Regional Planning Model (CFRPM v6) model set and situated within the existing roadway network to appropriately represent project access. The model was used to assign trips for all trip purposes between allocated origin and destination pairs using project build-out year model data. Trip distribution for the project was extracted from the completed model assignment and reviewed for logic. The resulting CFRPM v6 model plot showing percent of daily project distribution is provided in Appendix F.

Figure 2 displays the anticipated trip distribution for the proposed Magruder Eye Institute and the existing development. Thirty percent (30%) of ingress and egress existing development traffic is assumed to access the south driveways east of Cambridge Boulevard.

3.4 TRIP ASSIGNMENT

Site distribution percentages were used to assign anticipated project trips to the study area intersection and driveway. Figures 3 and 4 show the anticipated net new project movements at the study area intersection and driveway during the AM and PM peak hours, respectively.

Figure 2: Project Distribution

December 2020
Project No.: 049610000

Kimley»Horn

© 2020 Kimley-Horn and Associates, Inc.
189 S Orange Ave, Suite 1000, Orlando, FL, 32801
Phone: (407)-898-1511

Figure 3: AM Peak Hour Project Assignment

December 2020
Project No.: 049610000

Kimley»Horn

© 2020 Kimley-Horn and Associates, Inc.
189 S Orange Ave, Suite 1000, Orlando, FL, 32801
Phone: (407)-898-1511

Figure 4: PM Peak Hour Project Assignment

December 2020
Project No.: 049610000

Kimley»Horn

© 2020 Kimley-Horn and Associates, Inc.
189 S Orange Ave, Suite 1000, Orlando, FL, 32801
Phone: (407)-898-1511

4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2021

4.1 BACKGROUND TRAFFIC

Traffic conditions were evaluated for Year 2021 background conditions. Background volumes at the study area intersections were derived by applying 2% annual growth to existing traffic counts. The adjusted turning movement volume worksheet for the study intersection can be found in Appendix D.

4.2 BACKGROUND INTERSECTION ANALYSIS

An intersection operational analysis was performed for Year 2021 background conditions in the AM and PM peak hours using procedures outlined in the *Highway Capacity Manual,* 6th Edition (HCM 6) with *Synchro* (v10) software. Intersection level of service (LOS), maximum volume to capacity (v/c) ratios, and delay for the background AM and PM peak hour conditions are provided in Tables 5 and 6, respectively. Synchro outputs are provided in Appendix E.

As shown in Tables 5 and 6, all study area intersection approaches are shown to operate with acceptable LOS and volume to capacity (v/c) ratios less than 1.0 during background peak hour conditions with the exception of the stop-controlled approaches of Cambridge Boulevard, which are expected to continue to operate at LOS F. This result is common when a minor street stop-controlled approach crosses a high-volume major street free-flow approach during peak periods.

Table 5: Background Intersection Conditions (AM Peak Hour)

Background Condition - 2021					
Intersection	Control Type	Approach	AM Peak Hour		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
W Fairbanks Avenue & Cambridge Boulevard	Unsignalized (TWSC)	EB (L)	B	EBL	0.10
		WB (L)	B	WBL	0.00
		NB	F	NBL	0.13
		SB	F	SBL	1.58
		Overall	-	SBL	1.58

Table 6: Background Intersection Conditions (PM Peak Hour)

Background Condition - 2021					
Intersection	Control Type	Approach	PM Peak Hour		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
W Fairbanks Avenue & Cambridge Boulevard	Unsignalized (TWSC)	EB (L)	C	EBL	0.06
		WB (L)	B	WBL	0.00
		NB	F	NBL	0.28
		SB	F	SBL	1.03
		Overall	-	SBL	1.03

5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2021

5.1 BUILDOUT TRAFFIC

Future traffic conditions for the proposed development were evaluated for Year 2021 conditions. Buildout volumes were developed by adding anticipated project trips to background volumes. Figures 5 and 6 illustrate turning movement volumes for AM and PM peak hour buildout conditions, respectively. Adjusted turning movement volume worksheets can be found in Appendix D.

5.2 BUILDOUT INTERSECTION ANALYSIS

An intersection operational analysis was performed for Year 2021 buildout conditions in the AM and PM peak hours using procedures outlined in the *Highway Capacity Manual,* 6th Edition (HCM 6) with *Synchro* (v10) software. Intersection level of service (LOS), maximum volume to capacity (v/c) ratios, and delay for the background AM and PM peak hour conditions are provided in Tables 7 and 8, respectively. Synchro outputs are provided in Appendix E.

As shown in Tables 7 and 8, all study area intersection and project driveway approaches are shown to operate with acceptable LOS and volume to capacity (v/c) ratios less than 1.0 during buildout peak hour conditions with the exception of the stop-controlled approaches of Cambridge Boulevard, which are expected to continue to operate at LOS F. This result is common when a minor street stop-controlled approach crosses a high-volume major street free-flow approach during peak periods.

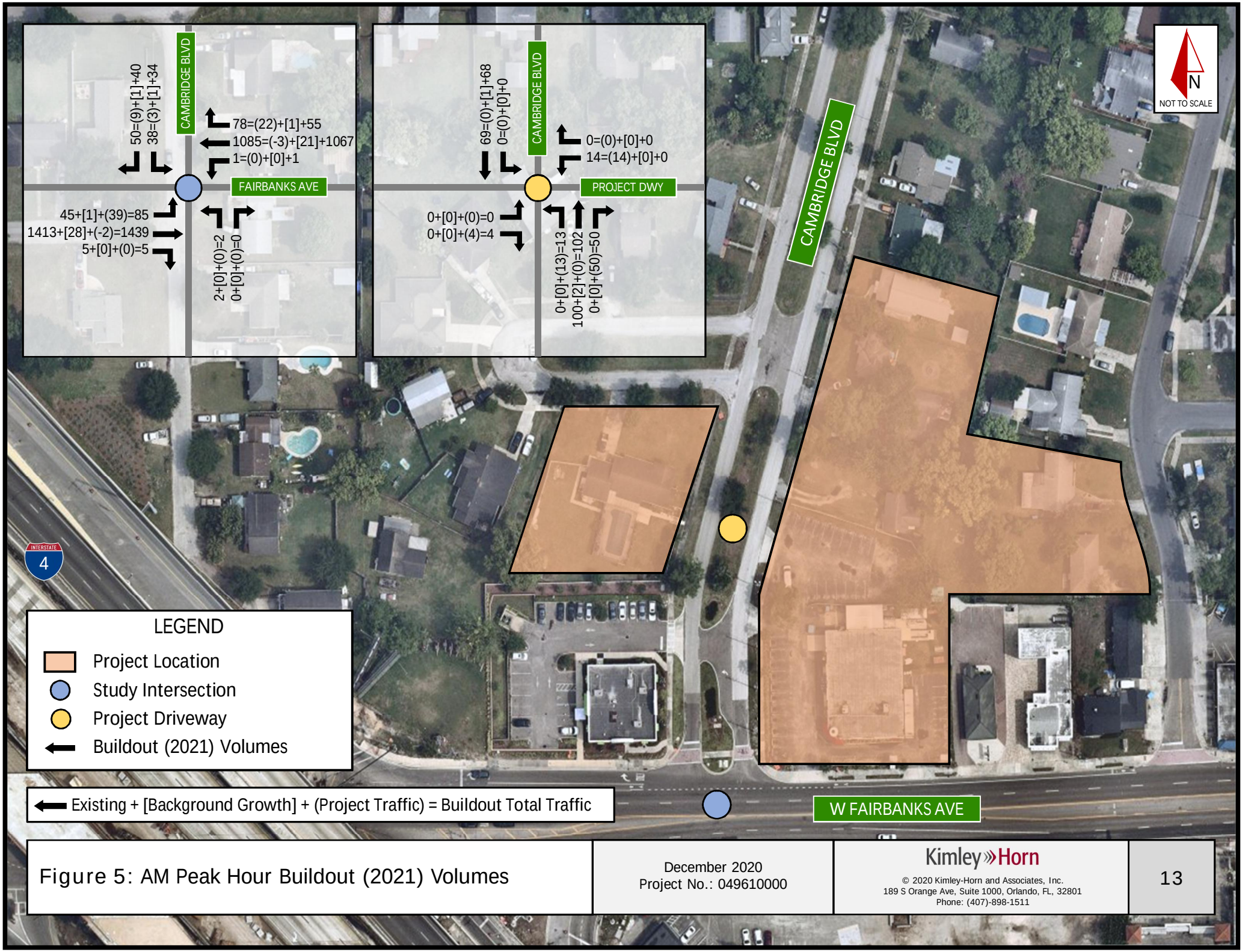


Figure 5: AM Peak Hour Buildout (2021) Volumes

December 2020
Project No.: 049610000

Kimley»Horn

© 2020 Kimley-Horn and Associates, Inc.
189 S Orange Ave, Suite 1000, Orlando, FL, 32801
Phone: (407)-898-1511

Figure 6: PM Peak Hour Buildout (2021) Volumes

December 2020
Project No.: 049610000

Kimley»Horn

© 2020 Kimley-Horn and Associates, Inc.
189 S Orange Ave, Suite 1000, Orlando, FL, 32801
Phone: (407)-898-1511

Table 7: Buildout Intersection Conditions (AM Peak Hour)

Buildout Condition - 2021					
Intersection	Control Type	Approach	AM Peak Hour		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
W Fairbanks Avenue & Cambridge Boulevard	Unsignalized (TWSC)	EB (L)	B	EBL	0.18
		WB (L)	B	WBL	0.00
		NB	F	NBL	0.17
		SB	F	SBL	2.10
		Overall	-	SBL	2.10
Cambridge Boulevard & Project Driveway	Unsignalized (TWSC)	EB	A	EBR	0.00
		WB	B	WBL	0.02
		NB (L)	A	NBL	0.01
		SB (L)	-	-	-
		Overall	-	WBL	0.02

Table 8: Buildout Intersection Conditions (PM Peak Hour)

Buildout Condition - 2021					
Intersection	Control Type	Approach	PM Peak Hour		
			Level of Service (*overall delay*)	Max V/C Movement	Max V/C Ratio
W Fairbanks Avenue & Cambridge Boulevard	Unsignalized (TWSC)	EB (L)	C	EBL	0.07
		WB (L)	B	WBL	0.00
		NB	F	NBL	0.34
		SB	F	SBL	2.02
		Overall	-	SBL	2.02
Cambridge Boulevard & Project Driveway	Unsignalized (TWSC)	EB	A	EBR	0.02
		WB	A	WBL	0.09
		NB (L)	A	NBL	0.00
		SB (L)	-	-	-
		Overall	-	WBL	0.09

6.0 CONCLUSION

This traffic impact analysis was performed to assess the transportation impacts of Magruder Eye Institute, a proposed site redevelopment of several parcels generally located on the northeast corner of W Fairbanks Avenue and Cambridge Boulevard in the City of Winter Park, Florida. The site is currently occupied by seven (7) single family homes and 6,492 square feet of retail. The redevelopment, proposed for buildout in 2021, will consist of a 32,000 square foot medical office building and off-site parking.

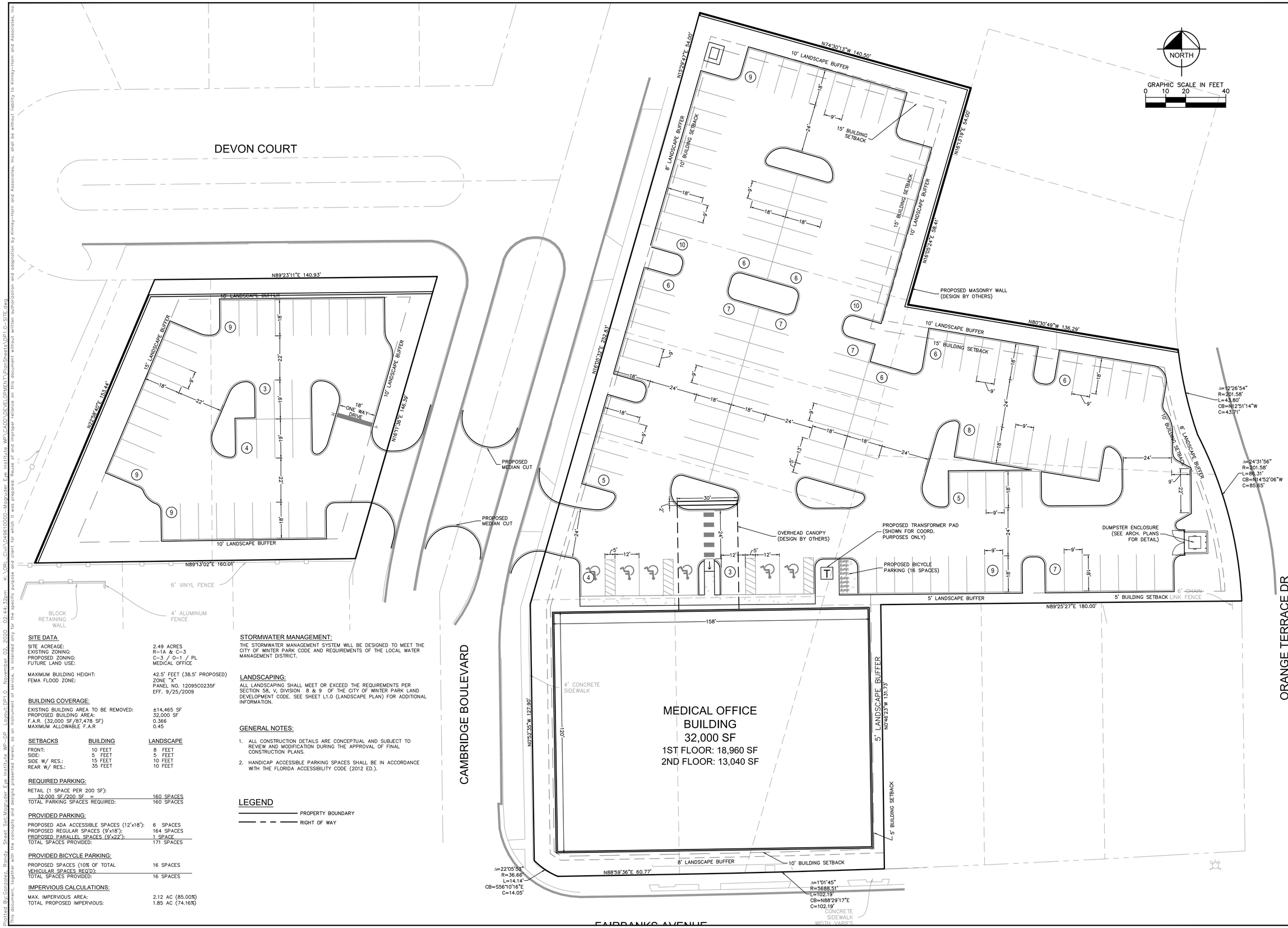
The proposed redevelopment of the site is anticipated to generate 116 net new daily trips, 65 net new AM peak hour trips (56 inbound and 9 outbound), and 31 net new PM peak hour trips (-9 inbound and 40 outbound) to the external roadway network at buildout. Access to the site is proposed via one (1) full-access driveway along Cambridge Avenue.

An operational analysis was performed at the project driveway and the intersection of W Fairbanks Avenue and Cambridge Boulevard for future background conditions (without project) and future buildout (with project) conditions. The analysis showed that the study area intersection and project driveway operate with acceptable LOS and volume to capacity (v/c) ratios less than 1.0 during background peak hour conditions with the exception of the stop-controlled approaches of Cambridge Boulevard, which are expected to continue to operate at LOS F. This result is common when a minor street stop-controlled approach crosses a high-volume major street free-flow approach during peak periods.

No new deficiencies are anticipated as a result of project traffic.

APPENDIX A
Conceptual Site Plan

Plotted By: Gonzalez, Randy Sheet Set: Magruder Eye Institute WP_DP Layout: DP1.0 November 02, 2020 02:44:32pm K:\ORL_Civil\049610000-Magruder Eye Institute WP\CADD\DEVELOPMENT\PlanSheets\DP1.0-SITE.dwg
This document, together with the concepts and designs presented herein, as an instrument of service, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



SITE DATA

SITE ACREAGE:	2.49 ACRES
EXISTING ZONING:	R-1A & C-3
PROPOSED ZONING:	C-3 / O-1 / PL
FUTURE LAND USE:	MEDICAL OFFICE
MAXIMUM BUILDING HEIGHT:	42.5' FEET (38.5' PROPOSED)
FEMA FLOOD ZONE:	ZONE "X" PANEL NO. 12095C0235F EFF. 9/25/2009

BUILDING COVERAGE:

EXISTING BUILDING AREA TO BE REMOVED:	±14,465 SF
PROPOSED BUILDING AREA:	32,000 SF
F.A.R. (32,000 SF/87,478 SF)	0.366
MAXIMUM ALLOWABLE F.A.R	0.45

SETBACKS

	BUILDING	LANDSCAPE
FRONT:	10 FEET	8 FEET
SIDE:	5 FEET	5 FEET
SIDE W/ RES.:	15 FEET	10 FEET
REAR W/ RES.:	35 FEET	10 FEET

REQUIRED PARKING:

RETAIL (1 SPACE PER 200 SF):	
32,000 SF/200 SF =	160 SPACES
TOTAL PARKING SPACES REQUIRED:	160 SPACES

PROVIDED PARKING:

PROPOSED ADA ACCESSIBLE SPACES (12'x18'):	6 SPACES
PROPOSED REGULAR SPACES (9'x18'):	164 SPACES
PROPOSED PARALLEL SPACES (9'x22'):	1 SPACE
TOTAL SPACES PROVIDED:	171 SPACES

PROVIDED BICYCLE PARKING:

PROPOSED SPACES (10% OF TOTAL VEHICULAR SPACES REQ'D):	16 SPACES
TOTAL SPACES PROVIDED:	16 SPACES

IMPERVIOUS CALCULATIONS:

MAX. IMPERVIOUS AREA:	2.12 AC (85.00%)
TOTAL PROPOSED IMPERVIOUS:	1.85 AC (74.16%)

STORMWATER MANAGEMENT:

THE STORMWATER MANAGEMENT SYSTEM WILL BE DESIGNED TO MEET THE CITY OF WINTER PARK CODE AND REQUIREMENTS OF THE LOCAL WATER MANAGEMENT DISTRICT.

LANDSCAPING:

ALL LANDSCAPING SHALL MEET OR EXCEED THE REQUIREMENTS PER SECTION 58, V, DIVISION 8 & 9 OF THE CITY OF WINTER PARK LAND DEVELOPMENT CODE. SEE SHEET L1.0 (LANDSCAPE PLAN) FOR ADDITIONAL INFORMATION.

GENERAL NOTES:

1. ALL CONSTRUCTION DETAILS ARE CONCEPTUAL AND SUBJECT TO REVIEW AND MODIFICATION DURING THE APPROVAL OF FINAL CONSTRUCTION PLANS.
2. HANDICAP ACCESSIBLE PARKING SPACES SHALL BE IN ACCORDANCE WITH THE FLORIDA ACCESSIBILITY CODE (2012 ED.).

LEGEND

PROPERTY BOUNDARY

RIGHT OF WAY

No.	REVISIONS	DATE	BY

Kimley»Horn

© 2020 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM CA 00000696

LICENSED PROFESSIONAL
JENNIFER J. STICKLER, P.E.
FLORIDA LICENSE NUMBER
76118
DATE: 10/28/2020

KHA PROJECT 049610000
DATE 11/02/2020
SCALE AS SHOWN
DESIGNED BY JJS
DRAWN BY EGS
CHECKED BY RMG

SITE PLAN

MAGRUDER EYE INSTITUTE WP

WINTER PARK, FLORIDA

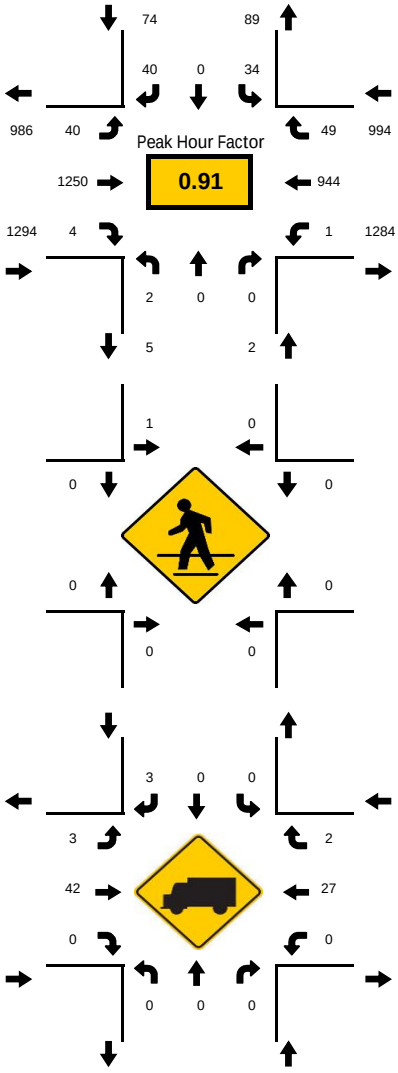
SHEET NUMBER
DP1.0

APPENDIX B
Turning Movement Counts

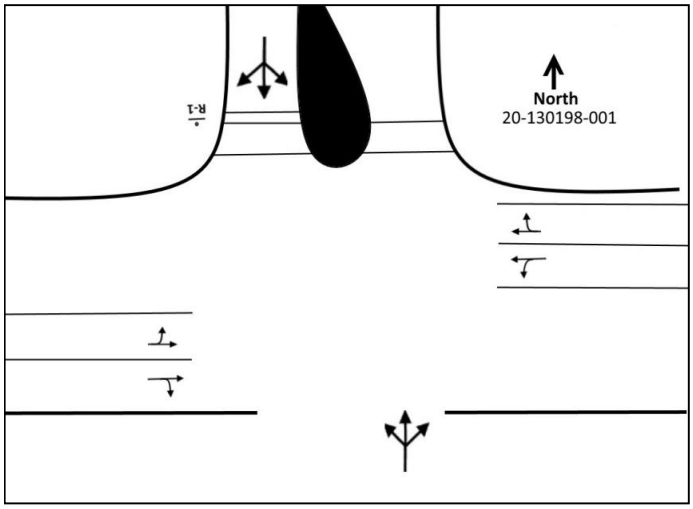
LOCATION: Cambridge Blvd & W Fairbanks Ave
CITY/STATE: Winter Park, FL

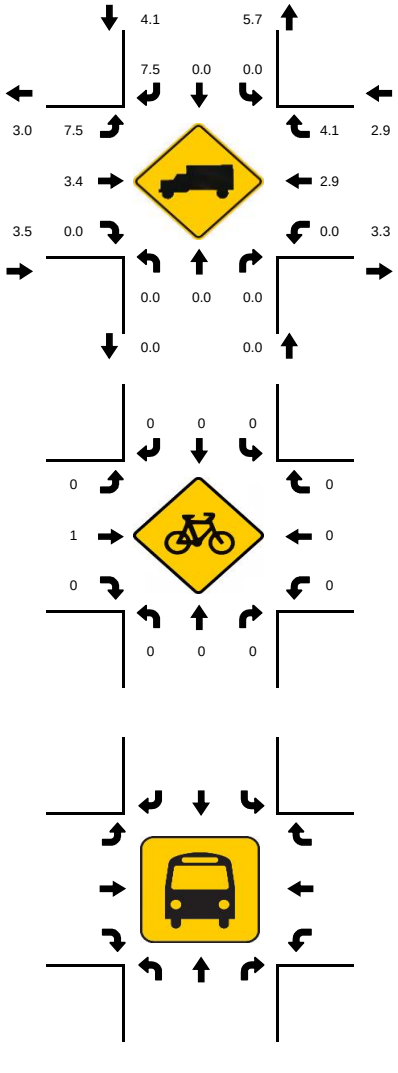
PROJECT ID: 20-130198-001
DATE: 11/04/2020

Peak-Hour: 08:00 AM - 09:00 AM
Peak 15-Minute: 08:45 AM - 09:00 AM



National Data & Surveying Services





15-Min Count Period Beginning At	Cambridge Blvd Northbound					Cambridge Blvd Southbound					W Fairbanks Ave Eastbound					W Fairbanks Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	0	0	0	0		3	0	7	0		6	191	0	0		0	183	4	0		394	2114
07:15 AM	0	0	0	0		3	0	5	0		4	260	0	1		0	234	6	0		513	2271
07:30 AM	0	0	0	0		5	0	8	0		7	312	0	0		0	236	10	0		578	2347
07:45 AM	0	0	0	0		3	0	7	0		11	350	0	0		1	246	11	0		629	2347
08:00 AM	0	0	0	0		7	0	3	0		9	272	3	0		0	243	14	0		551	2364
08:15 AM	0	0	0	0		12	0	17	0		10	299	1	0		1	236	13	0		589	1813
08:30 AM	2	0	0	0		10	0	17	0		13	320	0	2		0	205	9	0		578	1224
08:45 AM	0	0	0	0		5	0	3	0		6	359	0	0		0	260	13	0		646	646
Peak 15-Min Flowrates	**Northbound**					**Southbound**					**Eastbound**					**Westbound**					**Total**	
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
All Vehicles	8	0	0	0		48	0	68	0		52	1436	12	8		4	1040	56	0		**2732**	
Heavy Trucks	0	0	0			0	0	12			8	64	0			0	32	4			**120**	
Pedestrians		0					4					0					0				**4**	
Bicycles	0	0	0			0	0	0			0	4	0			0	0	0			**4**	
Railroad																						
Stopped Buses																						

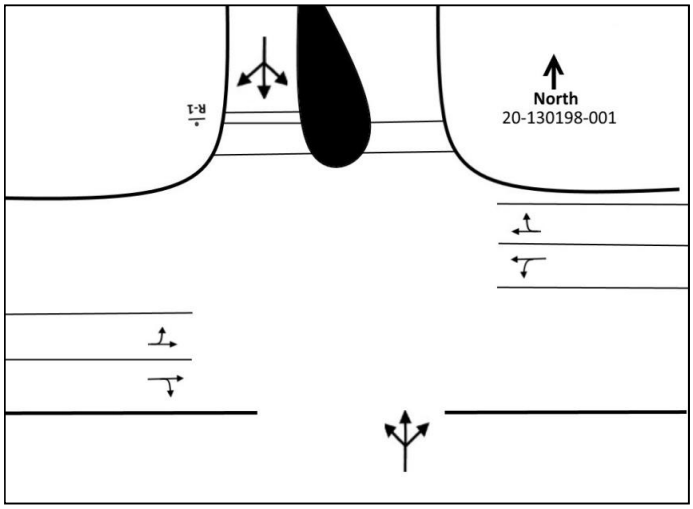
LOCATION: Cambridge Blvd & W Fairbanks Ave
CITY/STATE: Winter Park, FL

PROJECT ID: 20-130198-001
DATE: 11/04/2020

Peak-Hour: 04:30 PM - 05:30 PM
Peak 15-Minute: 05:15 PM - 05:30 PM

Peak Hour Factor: **0.95**

National Data & Surveying Services



15-Min Count Period Beginning At	Cambridge Blvd Northbound					Cambridge Blvd Southbound					W Fairbanks Ave Eastbound					W Fairbanks Ave Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	0	0	1	0		6	0	7	0		9	218	0	0		0	307	14	0		562	2396
04:15 PM	1	0	0	0		7	1	7	0		7	254	2	0		0	336	8	0		623	2495
04:30 PM	1	0	0	0		4	0	7	0		5	246	0	0		0	336	10	0		609	2541
04:45 PM	0	0	0	0		5	0	10	0		3	263	1	0		0	313	7	0		602	2503
05:00 PM	1	0	0	0		3	0	6	0		7	250	1	0		1	379	13	0		661	2460
05:15 PM	4	0	2	0		3	0	5	0		2	290	1	0		0	342	20	0		669	1799
05:30 PM	0	0	0	0		3	0	1	0		7	241	0	1		0	309	9	0		571	1130
05:45 PM	0	0	0	0		7	0	3	0		5	240	0	0		0	291	13	0		559	559

Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	
All Vehicles	16	0	8	0		20	0	40	0		28	1160	4	0		4	1516	80	0		**2876**
Heavy Trucks	0	0	0			0	0	0			0	20	0			0	12	8			**40**
Pedestrians		4					4					0					0				**8**
Bicycles	0	0	0			0	0	0			0	0	0			0	4	0			**4**
Railroad																					
Stopped Buses																					

Prepared by National Data & Surveying Services

VOLUME

W Fairbanks Ave 1,000' W/O Orlando Ave

Day: Wednesday
Date: 11/4/2020

City: Winter Park
Project #: FL20_130199_001

DAILY TOTALS	NB	SB	EB	WB	Total
	0	0	13,906	14,590	28,496

AM Period	NB	SB	EB		WB		TOTAL		PM Period	NB	SB	EB		WB		TOTAL	
00:00			24		14		38		12:00			239		248		487	
00:15			13		17		30		12:15			255		283		538	
00:30			11		9		20		12:30			295		259		554	
00:45			10	58	16	56	26	114	12:45			263	1052	258	1048	521	2100
01:00			10		14		24		13:00			250		254		504	
01:15			2		7		9		13:15			253		271		524	
01:30			8		7		15		13:30			234		232		466	
01:45			5	25	6	34	11	59	13:45			213	950	278	1035	491	1985
02:00			5		15		20		14:00			234		276		510	
02:15			7		15		22		14:15			249		290		539	
02:30			7		2		9		14:30			243		264		507	
02:45			6	25	1	33	7	58	14:45			205	931	287	1117	492	2048
03:00			7		8		15		15:00			226		228		454	
03:15			5		11		16		15:15			204		279		483	
03:30			7		13		20		15:30			242		268		510	
03:45			15	34	8	40	23	74	15:45			243	915	273	1048	516	1963
04:00			7		11		18		16:00			219		262		481	
04:15			9		5		14		16:15			271		249		520	
04:30			14		16		30		16:30			260		278		538	
04:45			17	47	14	46	31	93	16:45			291	1041	236	1025	527	2066
05:00			19		23		42		17:00			264		269		533	
05:15			37		29		66		17:15			307		268		575	
05:30			63		48		111		17:30			261		233		494	
05:45			53	172	63	163	116	335	17:45			222	1054	252	1022	474	2076
06:00			90		63		153		18:00			194		242		436	
06:15			142		93		235		18:15			242		244		486	
06:30			173		129		302		18:30			231		194		425	
06:45			201	606	160	445	361	1051	18:45			165	832	200	880	365	1712
07:00			166		180		346		19:00			162		179		341	
07:15			223		235		458		19:15			136		219		355	
07:30			255		229		484		19:30			115		178		293	
07:45			273	917	265	909	538	1826	19:45			98	511	147	723	245	1234
08:00			268		225		493		20:00			116		165		281	
08:15			279		209		488		20:15			109		141		250	
08:30			284		219		503		20:30			81		148		229	
08:45			295	1126	231	884	526	2010	20:45			78	384	128	582	206	966
09:00			252		199		451		21:00			59		129		188	
09:15			220		218		438		21:15			53		129		182	
09:30			239		214		453		21:30			43		124		167	
09:45			243	954	234	865	477	1819	21:45			60	215	115	497	175	712
10:00			164		205		369		22:00			57		97		154	
10:15			206		193		399		22:15			62		67		129	
10:30			249		190		439		22:30			47		57		104	
10:45			200	819	221	809	421	1628	22:45			46	212	53	274	99	486
11:00			196		224		420		23:00			30		54		84	
11:15			203		193		396		23:15			36		40		76	
11:30			262		242		504		23:30			24		31		55	
11:45			239	900	241	900	480	1800	23:45			36	126	30	155	66	281
TOTALS				5683		5184		10867	TOTALS				8223		9406		17629
SPLIT %				52.3%		47.7%		38.1%	SPLIT %				46.6%		53.4%		61.9%

DAILY TOTALS	NB	SB	EB	WB	Total
	0	0	13,906	14,590	28,496

AM Peak Hour			08:00	11:45	11:45	PM Peak Hour		16:45	14:00	16:30
AM Pk Volume			1126	1031	2059	PM Pk Volume		1123	1117	2173
Pk Hr Factor			0.954	0.911	0.929	Pk Hr Factor		0.914	0.963	0.945
7 - 9 Volume	0	0	2043	1793	3836	4 - 6 Volume	0	2095	2047	4142
7 - 9 Peak Hour			08:00	07:15	07:45	4 - 6 Peak Hour		16:45	16:30	16:30
7 - 9 Pk Volume	0	0	1126	954	2022	4 - 6 Pk Volume	0	1123	1051	2173
Pk Hr Factor	0.000	0.000	0.954	0.900	0.940	Pk Hr Factor	0.000	0.914	0.945	0.945

APPENDIX C

Data from Orange County and FDOT's Florida Traffic Online (FTO)

2019 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL

CATEGORY: 7500 ORANGE COUNTYWIDE

MOCF: 0.98

WEEK	DATES	SF	PSCF
1	01/01/2019 - 01/05/2019	1.02	1.04
2	01/06/2019 - 01/12/2019	1.03	1.05
3	01/13/2019 - 01/19/2019	1.04	1.06
4	01/20/2019 - 01/26/2019	1.03	1.05
5	01/27/2019 - 02/02/2019	1.02	1.04
6	02/03/2019 - 02/09/2019	1.00	1.02
7	02/10/2019 - 02/16/2019	0.99	1.01
8	02/17/2019 - 02/23/2019	0.99	1.01
* 9	02/24/2019 - 03/02/2019	0.98	1.00
*10	03/03/2019 - 03/09/2019	0.98	1.00
*11	03/10/2019 - 03/16/2019	0.97	0.99
*12	03/17/2019 - 03/23/2019	0.97	0.99
*13	03/24/2019 - 03/30/2019	0.97	0.99
*14	03/31/2019 - 04/06/2019	0.97	0.99
*15	04/07/2019 - 04/13/2019	0.97	0.99
*16	04/14/2019 - 04/20/2019	0.97	0.99
*17	04/21/2019 - 04/27/2019	0.98	1.00
*18	04/28/2019 - 05/04/2019	0.98	1.00
*19	05/05/2019 - 05/11/2019	0.98	1.00
*20	05/12/2019 - 05/18/2019	0.98	1.00
*21	05/19/2019 - 05/25/2019	0.99	1.01
22	05/26/2019 - 06/01/2019	0.99	1.01
23	06/02/2019 - 06/08/2019	1.00	1.02
24	06/09/2019 - 06/15/2019	1.00	1.02
25	06/16/2019 - 06/22/2019	1.01	1.03
26	06/23/2019 - 06/29/2019	1.01	1.03
27	06/30/2019 - 07/06/2019	1.01	1.03
28	07/07/2019 - 07/13/2019	1.02	1.04
29	07/14/2019 - 07/20/2019	1.02	1.04
30	07/21/2019 - 07/27/2019	1.01	1.03
31	07/28/2019 - 08/03/2019	1.01	1.03
32	08/04/2019 - 08/10/2019	1.00	1.02
33	08/11/2019 - 08/17/2019	0.99	1.01
34	08/18/2019 - 08/24/2019	1.01	1.03
35	08/25/2019 - 08/31/2019	1.02	1.04
36	09/01/2019 - 09/07/2019	1.03	1.05
37	09/08/2019 - 09/14/2019	1.04	1.06
38	09/15/2019 - 09/21/2019	1.05	1.07
39	09/22/2019 - 09/28/2019	1.04	1.06
40	09/29/2019 - 10/05/2019	1.03	1.05
41	10/06/2019 - 10/12/2019	1.01	1.03
42	10/13/2019 - 10/19/2019	1.00	1.02
43	10/20/2019 - 10/26/2019	1.00	1.02
44	10/27/2019 - 11/02/2019	1.00	1.02
45	11/03/2019 - 11/09/2019	1.00	1.02
46	11/10/2019 - 11/16/2019	1.00	1.02
47	11/17/2019 - 11/23/2019	1.01	1.03
48	11/24/2019 - 11/30/2019	1.01	1.03
49	12/01/2019 - 12/07/2019	1.01	1.03
50	12/08/2019 - 12/14/2019	1.01	1.03
51	12/15/2019 - 12/21/2019	1.02	1.04
52	12/22/2019 - 12/28/2019	1.03	1.05
53	12/29/2019 - 12/31/2019	1.04	1.06

* PEAK SEASON

Roadway Count Summary

Start Date	12-Nov-19	Start Time	00:00
Stop Date	13-Nov-19	Stop Time	24:00
County	Orange	Station ID	435
Location	Fairbanks Av : Interstate 4 to Orlando Av (0.27 Miles W. of Orlando Av)		

12-Nov-19 — Eastbound Volume for Lane 1

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	23	12	8	8	15	37	107	193	279	262	255	219
30	23	21	10	9	14	52	142	243	317	270	240	246
45	24	14	1	10	20	49	155	262	297	262	251	255
00	24	17	12	14	30	80	218	269	293	280	264	236
Hr Total	94	64	31	41	79	218	622	967	1186	1074	1010	956

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	244	250	225	257	253	308	262	177	113	107	72	38
30	281	233	237	225	253	318	272	175	105	107	63	40
45	246	275	220	268	267	308	240	151	124	88	55	31
00	251	238	240	256	282	280	231	138	88	73	42	29
Hr Total	1022	996	922	1006	1055	1214	1005	641	430	375	232	138

24 Hour Total	15,378				
AM Peak Hour Begins	8:00	AM Peak Volume	1,186	AM Peak Hour Factor	0.94
PM Peak Hour Begins	16:45	PM Peak Volume	1,216	PM Peak Hour Factor	0.96

12-Nov-19 — Westbound Volume for Lane 2

End Time	00	01	02	03	04	05	6	07	08	09	10	11
15	41	23	12	6	15	26	101	243	251	241	199	266
30	37	15	17	10	18	40	106	285	244	223	207	243
45	27	13	20	16	19	78	185	259	205	201	218	254
00	17	9	16	13	30	76	176	249	230	248	238	256
Hr Total	122	60	65	45	82	220	568	1036	930	913	862	1019

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	257	270	274	308	310	316	283	222	203	156	134	80
30	293	287	317	315	286	279	273	228	185	170	76	68
45	271	275	295	294	269	265	255	194	188	131	86	49
00	310	266	299	325	302	287	214	204	171	108	59	48
Hr Total	1131	1098	1185	1242	1167	1147	1025	848	747	565	355	245

24 Hour Total	16,677				
AM Peak Hour Begins	12:00	AM Peak Volume	1,131	AM Peak Hour Factor	0.91
PM Peak Hour Begins	15:15	PM Peak Volume	1,244	PM Peak Hour Factor	0.96

12-Nov-19 — Total Volume for All Lanes

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	64	35	20	14	30	63	208	436	530	503	454	485
30	60	36	27	19	32	92	248	528	561	493	447	489
45	51	27	21	26	39	127	340	521	502	463	469	509
00	41	26	28	27	60	156	394	518	523	528	502	492
Hr Total	216	124	96	86	161	438	1190	2003	2116	1987	1872	1975

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	501	520	499	565	563	624	545	399	316	263	206	118
30	574	520	554	540	539	597	545	403	290	277	139	108
45	517	550	515	562	536	573	495	345	312	219	141	80
00	561	504	539	581	584	567	445	342	259	181	101	77
Hr Total	2153	2094	2107	2248	2222	2361	2030	1489	1177	940	587	383

24 Hour Total	32,055				
AM Peak Hour Begins	12:00	AM Peak Volume	2,153	AM Peak Hour Factor	0.94
PM Peak Hour Begins	16:45	PM Peak Volume	2,378	PM Peak Hour Factor	0.95

Roadway Count Summary

Start Date	13-Nov-19	Start Time	00:00
Stop Date	14-Nov-19	Stop Time	24:00
County	Orange	Station ID	435
Location	Fairbanks Av : Interstate 4 to Orlando Av (0.27 Miles W. of Orlando Av)		

13-Nov-19 Eastbound Volume for Lane 1

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	25	21	9	3	19	27	113	178	294	264	240	207
30	9	12	7	8	8	54	147	215	302	268	238	251
45	16	12	6	3	19	68	180	254	291	301	245	270
00	12	8	7	10	28	85	213	296	331	319	237	282
Hr Total	62	53	29	24	74	234	653	943	1218	1152	960	1010

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	230	256	274	241	254	337	277	260	119	103	84	52
30	258	257	289	245	243	286	291	213	121	108	71	43
45	260	267	264	244	240	290	278	171	104	96	64	47
00	253	233	249	246	282	287	287	148	124	86	43	35
Hr Total	1001	1013	1076	976	1019	1200	1133	792	468	393	262	177

24 Hour Total	15,922				
AM Peak Hour Begins	8:00	AM Peak Volume	1,218	AM Peak Hour Factor	0.92
PM Peak Hour Begins	17:00	PM Peak Volume	1,200	PM Peak Hour Factor	0.89

13-Nov-19 Westbound Volume for Lane 2

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	37	17	16	9	9	44	88	247	224	219	224	278
30	22	21	14	8	16	47	132	285	260	239	238	271
45	20	22	18	15	24	63	138	266	209	188	237	250
00	16	16	7	10	22	71	182	275	230	230	222	283
Hr Total	95	76	55	42	71	225	540	1073	923	876	921	1082

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	261	286	314	262	282	299	228	194	199	172	128	78
30	276	264	286	322	318	314	279	201	165	197	149	78
45	252	291	309	297	309	261	213	206	140	145	104	73
00	271	264	264	287	315	244	204	170	163	151	83	46
Hr Total	1060	1105	1173	1168	1224	1118	924	771	667	665	464	275

24 Hour Total	16,593				
AM Peak Hour Begins	11:00	AM Peak Volume	1,082	AM Peak Hour Factor	0.96
PM Peak Hour Begins	16:15	PM Peak Volume	1,241	PM Peak Hour Factor	0.98

13-Nov-19 Total Volume for All Lanes

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	62	38	25	12	28	71	201	425	518	483	464	485
30	31	33	21	16	24	101	279	500	562	507	476	522
45	36	34	24	18	43	131	318	520	500	489	482	520
00	28	24	14	20	50	156	395	571	561	549	459	565
Hr Total	157	129	84	66	145	459	1193	2016	2141	2028	1881	2092

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	491	542	588	503	536	636	505	454	318	275	212	130
30	534	521	575	567	561	600	570	414	286	305	220	121
45	512	558	573	541	549	551	491	377	244	241	168	120
00	524	497	513	533	597	531	491	318	287	237	126	81
Hr Total	2061	2118	2249	2144	2243	2318	2057	1563	1135	1058	726	452

24 Hour Total	32,515				
AM Peak Hour Begins	7:30	AM Peak Volume	2,171	AM Peak Hour Factor	0.95
PM Peak Hour Begins	16:45	PM Peak Volume	2,384	PM Peak Hour Factor	0.94

Roadway Count Summary

Start Date	14-Nov-19	Start Time	00:00
Stop Date	15-Nov-19	Stop Time	24:00
County	Orange	Station ID	435
Location	Fairbanks Av : Interstate 4 to Orlando Av (0.27 Miles W. of Orlando Av)		

14-Nov-19 — Eastbound Volume for Lane 1

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	26	9	14	10	8	33	96	156	283	252	248	229
30	14	17	5	10	18	52	149	210	275	291	234	228
45	22	17	13	12	15	55	182	276	291	273	270	255
00	13	21	17	14	23	85	187	275	308	302	218	264
Hr Total	75	64	49	46	64	225	614	917	1157	1118	970	976

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	241	246	219	237	261	304	287	223	117	188	47	30
30	264	230	254	244	273	308	299	210	148	141	60	26
45	233	245	215	258	268	275	261	188	122	119	37	27
00	248	250	264	272	307	295	241	178	91	88	30	23
Hr Total	986	971	952	1011	1109	1182	1088	799	478	536	174	106

24 Hour Total	15,667				
AM Peak Hour Begins	8:00	AM Peak Volume	1,157	AM Peak Hour Factor	0.94
PM Peak Hour Begins	16:45	PM Peak Volume	1,194	PM Peak Hour Factor	0.97

14-Nov-19 — Westbound Volume for Lane 2

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	34	22	18	9	18	34	91	271	255	214	220	237
30	36	20	19	10	15	39	122	286	215	240	223	246
45	29	9	16	21	20	63	160	237	223	231	195	256
00	27	17	15	22	26	70	186	253	266	260	212	257
Hr Total	126	68	68	62	79	206	559	1047	959	945	850	996

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	215	298	238	306	272	303	236	207	212	198	135	79
30	287	264	291	273	258	299	240	203	209	220	103	75
45	245	283	280	312	274	289	205	197	193	155	92	54
00	281	250	267	250	322	229	199	187	198	143	88	45
Hr Total	1028	1095	1076	1141	1126	1120	880	794	812	716	418	253

24 Hour Total	16,424				
AM Peak Hour Begins	7:00	AM Peak Volume	1,047	AM Peak Hour Factor	0.92
PM Peak Hour Begins	16:45	PM Peak Volume	1,213	PM Peak Hour Factor	0.94

14-Nov-19 — Total Volume for All Lanes

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	60	31	32	19	26	67	187	427	538	466	468	466
30	50	37	24	20	33	91	271	496	490	531	457	474
45	51	26	29	33	35	118	342	513	514	504	465	511
00	40	38	32	36	49	155	373	528	574	562	430	521
Hr Total	201	132	117	108	143	431	1173	1964	2116	2063	1820	1972

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	456	544	457	543	533	607	523	430	329	386	182	109
30	551	494	545	517	531	607	539	413	357	361	163	101
45	478	528	495	570	542	564	466	385	315	274	129	81
00	529	500	531	522	629	524	440	365	289	231	118	68
Hr Total	2014	2066	2028	2152	2235	2302	1968	1593	1290	1252	592	359

24 Hour Total	32,091				
AM Peak Hour Begins	8:00	AM Peak Volume	2,116	AM Peak Hour Factor	0.92
PM Peak Hour Begins	16:45	PM Peak Volume	2,407	PM Peak Hour Factor	0.96

Roadway Count Summary

Start Date	12-Nov-19	Start Time	00:00
Stop Date	14-Nov-19	Stop Time	24:00
County	Orange	Station ID	435
Location	Fairbanks Av : Interstate 4 to Orlando Av (0.27 Miles W. of Orlando Av)		

12-Nov-19 — Eastbound Volume for Lane 1

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	25	14	10	7	14	32	105	176	285	259	248	218
30	15	17	7	9	13	53	146	223	298	276	237	242
45	21	14	7	8	18	57	172	264	293	279	255	260
00	16	15	12	13	27	83	206	280	311	300	240	261
Hr Total	77	60	36	37	72	226	630	942	1187	1115	980	981

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	238	251	239	245	256	316	275	220	116	133	68	40
30	268	240	260	238	256	304	287	199	125	119	65	36
45	246	262	233	257	258	291	260	170	117	101	52	35
00	251	240	251	258	290	287	253	155	101	82	38	29
Hr Total	1003	993	983	998	1061	1199	1075	744	459	435	223	140

24 Hour Total	15,656				
AM Peak Hour Begins	8:00	AM Peak Volume	1,187	AM Peak Hour Factor	0.96
PM Peak Hour Begins	16:45	PM Peak Volume	1,202	PM Peak Hour Factor	0.95

12-Nov-19 — Westbound Volume for Lane 2

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	37	21	15	8	14	35	93	254	243	225	214	260
30	32	19	17	9	16	42	120	285	240	234	223	253
45	25	15	18	17	21	68	161	254	212	207	217	253
00	20	14	13	15	26	72	181	259	242	246	224	265
Hr Total	114	68	63	50	77	217	556	1052	937	911	878	1032

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	244	285	275	292	288	306	249	208	205	175	132	79
30	285	272	298	303	287	297	264	211	186	196	109	74
45	256	283	295	301	284	272	224	199	174	144	94	59
00	287	260	277	287	313	253	206	187	177	134	77	46
Hr Total	1073	1099	1145	1184	1172	1128	943	804	742	649	412	258

24 Hour Total	16,565				
AM Peak Hour Begins	12:00	AM Peak Volume	1,073	AM Peak Hour Factor	0.93
PM Peak Hour Begins	16:30	PM Peak Volume	1,200	PM Peak Hour Factor	0.96

12-Nov-19 — Total Volume for All Lanes

End Time	00	01	02	03	04	05	06	07	08	09	10	11
15	62	35	26	15	28	67	199	429	529	484	462	479
30	47	35	24	18	30	95	266	508	538	510	460	495
45	46	29	25	26	39	125	333	518	505	485	472	513
00	36	29	25	28	53	156	387	539	553	546	464	526
Hr Total	191	128	99	87	150	443	1185	1994	2124	2026	1858	2013

End Time	12	13	14	15	16	17	18	19	20	21	22	23
15	483	535	515	537	544	622	524	428	321	308	200	119
30	553	512	558	541	544	601	551	410	311	314	174	110
45	502	545	528	558	542	563	484	369	290	245	146	94
00	538	500	528	545	603	541	459	342	278	216	115	75
Hr Total	2076	2093	2128	2181	2233	2327	2018	1548	1201	1083	635	398

24 Hour Total	32,220				
AM Peak Hour Begins	8:00	AM Peak Volume	2,124	AM Peak Hour Factor	0.96
PM Peak Hour Begins	16:45	PM Peak Volume	2,390	PM Peak Hour Factor	0.96

APPENDIX D
Turning Movement Volume Worksheet

INTERSECTION VOLUME DEVELOPMENT SHEET

Cambridge Blvd & **W Fairbanks Ave**

AM Peak Hour Factor: 0.91
PM Peak Hour Factor: 0.95

Weekday AM Peak Hour	**Cambridge Blvd**						**W Fairbanks Ave**					
	Northbound			Southbound			Eastbound			Westbound		
08:00 AM - 09:00 AM	L	T	R	L	T	R	L	T	R	L	T	R
TMC	2	0	0	34	0	40	40	1,250	4	1	944	49
Seasonal Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Heavy Vehicle	0%	-	-	0%	-	8%	8%	3%	0%	0%	3%	4%
COVID-19 Adjustment Factor	-	-	-	-	-	-	1.13	1.13	1.13	1.13	1.13	1.13
Existing Volume (2020)	**2**	**0**	**0**	**34**	**0**	**40**	**45**	**1,413**	**5**	**1**	**1,067**	**55**
Growth Factor	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Numbers of Years	1	1	1	1	1	1	1	1	1	1	1	1
Background (2021)	**2**	**0**	**0**	**35**	**0**	**41**	**46**	**1,441**	**5**	**1**	**1,088**	**56**
Project Trips												
Existing Development Traffic												
Distribution [1]				*35%*		*35%*	*35%*	*30%*			*30%*	
Direction	*N/A*	*N/A*	*N/A*	**OUT**	*N/A*	**OUT**	**IN**	**IN**	*N/A*	*N/A*	**OUT**	*N/A*
Trips	*0*	*0*	*0*	*3*	*0*	*3*	*2*	*2*	*0*	*0*	*3*	*0*
Project Redevelopment Traffic												
Project Distribution				*35%*		*65%*	*65%*					*35%*
Direction	*N/A*	*N/A*	*N/A*	**OUT**	*N/A*	**OUT**	**IN**	*N/A*	*N/A*	*N/A*	*N/A*	**IN**
Project Trips	*0*	*0*	*0*	*6*	*0*	*12*	*41*	*0*	*0*	*0*	*0*	*22*
Net New Project Trips	0	0	0	3	0	9	39	-2	0	0	-3	22
Project Buildout	**2**	**0**	**0**	**38**	**0**	**50**	**85**	**1,439**	**5**	**1**	**1,085**	**78**

Weekday PM Peak Hour	**Cambridge Blvd**						**W Fairbanks Ave**					
	Northbound			Southbound			Eastbound			Westbound		
04:30 PM - 05:30 PM	L	T	R	L	T	R	L	T	R	L	T	R
TMC	6	0	2	15	0	28	17	1,049	3	1	1,370	50
Seasonal Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Heavy Vehicle	0%	-	0%	0%	-	0%	0%	1%	0%	0%	1%	4%
COVID-19 Adjustment Factor	-	-	-	-	-	-	1.13	1.13	1.13	1.13	1.13	1.13
Existing Volume (2020)	**6**	**0**	**2**	**15**	**0**	**28**	**19**	**1,186**	**3**	**1**	**1,549**	**57**
Growth Factor	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Numbers of Years	1	1	1	1	1	1	1	1	1	1	1	1
Background (2021)	**6**	**0**	**2**	**15**	**0**	**29**	**19**	**1,210**	**3**	**1**	**1,580**	**58**
Project Trips												
Existing Development Traffic												
Distribution [1]				*35%*		*35%*	*35%*	*30%*			*30%*	
Direction	*N/A*	*N/A*	*N/A*	**OUT**	*N/A*	**OUT**	**IN**	**IN**	*N/A*	*N/A*	**OUT**	*N/A*
Trips	*0*	*0*	*0*	*14*	*0*	*14*	*14*	*12*	*0*	*0*	*12*	*0*
Project Redevelopment Traffic												
Project Distribution				*35%*		*65%*	*65%*					*35%*
Direction	*N/A*	*N/A*	*N/A*	**OUT**	*N/A*	**OUT**	**IN**	*N/A*	*N/A*	*N/A*	*N/A*	**IN**
Project Trips	*0*	*0*	*0*	*28*	*0*	*52*	*20*	*0*	*0*	*0*	*0*	*11*
Net New Project Trips	0	0	0	14	0	38	6	-12	0	0	-12	11
Project Buildout	**6**	**0**	**2**	**29**	**0**	**67**	**25**	**1,198**	**3**	**1**	**1,568**	**69**

1. Existing development traffic distribution assumes 30% of vehicles use the south driveway to/from Fairbanks Avenue, and the remaining 35% uses Cambridge Boulevard.

INTERSECTION VOLUME DEVELOPMENT SHEET

Cambridge Blvd & **Project Driveway**

Weekday AM Peak Hour	Cambridge Blvd						Project Driveway					
	Northbound			Southbound			Eastbound			Westbound		
08:00 AM - 09:00 AM	L	T	R	L	T	R	L	T	R	L	T	R
Adjusted TMCs	0	100	0	0	68	0	0	0	0	0	0	0
Heavy Vehicle	-	12%	-	-	8%	-	-	-	-	-	-	-
Existing Volume (2020)	**0**	**100**	**0**	**0**	**68**	**0**	**0**	**0**	**0**	**0**	**0**	**0**
Growth Factor	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Numbers of Years	1	1	1	1	1	1	1	1	1	1	1	1
Background (2021)	**0**	**102**	**0**	**0**	**69**	**0**	**0**	**0**	**0**	**0**	**0**	**0**
Project Assignment	20%		80%						20%	80%		
Direction	**IN**	N/A	**IN**	N/A	N/A	N/A	N/A	N/A	**OUT**	**OUT**	N/A	N/A
Project Trips	13	0	50	0	0	0	0	0	4	14	0	0
Project Buildout	**13**	**102**	**50**	**0**	**69**	**0**	**0**	**0**	**4**	**14**	**0**	**0**

Weekday PM Peak Hour	Cambridge Blvd						Project Driveway					
	Northbound			Southbound			Eastbound			Westbound		
04:30 PM - 05:30 PM	L	T	R	L	T	R	L	T	R	L	T	R
Adjusted TMCs	0	76	0	0	15	0	0	0	0	0	0	0
Seasonal Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Heavy Vehicle	-	4.00%	-	-	0.00%	-	-	-	-	-	-	-
Existing Volume (2020)	**0**	**76**	**0**	**0**	**15**	**0**	**0**	**0**	**0**	**0**	**0**	**0**
Growth Factor	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Numbers of Years	1	1	1	1	1	1	1	1	1	1	1	1
Background (2021)	**0**	**78**	**0**	**0**	**15**	**0**	**0**	**0**	**0**	**0**	**0**	**0**
Project Assignment	20%		80%						20%	80%		
Direction	**IN**	N/A	**IN**	N/A	N/A	N/A	N/A	N/A	**OUT**	**OUT**	N/A	N/A
Project Trips	6	0	25	0	0	0	0	0	16	64	0	0
Project Buildout	**6**	**78**	**25**	**0**	**15**	**0**	**0**	**0**	**16**	**64**	**0**	**0**

APPENDIX E
Synchro Outputs

Intersection	
Int Delay, s/veh	10.8

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	45	1413	5	1	1067	55	2	0	0	34	0	40
Future Vol, veh/h	45	1413	5	1	1067	55	2	0	0	34	0	40
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	-	100	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	91	91	91	91	91	91	91	91	91	91	91	91
Heavy Vehicles, %	8	3	2	2	3	4	2	2	2	2	2	8
Mvmt Flow	49	1553	5	1	1173	60	2	0	0	37	0	44

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1233	0	0	1558	0	0	2243	2889	779	2080	2861	617
Stage 1	-	-	-	-	-	-	1654	1654	-	1205	1205	-
Stage 2	-	-	-	-	-	-	589	1235	-	875	1656	-
Critical Hdwy	4.26	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	7.06
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.28	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.38
Pot Cap-1 Maneuver	529	-	-	421	-	-	23	16	339	~ 31	16	418
Stage 1	-	-	-	-	-	-	102	154	-	195	255	-
Stage 2	-	-	-	-	-	-	461	247	-	310	154	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	529	-	-	421	-	-	19	14	339	~ 29	14	418
Mov Cap-2 Maneuver	-	-	-	-	-	-	19	14	-	~ 29	14	-
Stage 1	-	-	-	-	-	-	93	140	-	177	254	-
Stage 2	-	-	-	-	-	-	412	247	-	281	140	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.4	0	217.9	$ 375.1
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	19	529	-	-	421	-	-	58
HCM Lane V/C Ratio	0.116	0.093	-	-	0.003	-	-	1.402
HCM Control Delay (s)	217.9	12.5	-	-	13.6	-	-	$ 375.1
HCM Lane LOS	F	B	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.3	0.3	-	-	0	-	-	7.2

Notes
~: Volume exceeds capacity $: Delay exceeds 300s +: Computation Not Defined *: All major volume in platoon

Intersection	
Int Delay, s/veh	4.1

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	19	1186	3	1	1549	57	6	0	2	15	0	28
Future Vol, veh/h	19	1186	3	1	1549	57	6	0	2	15	0	28
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	-	100	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	2	2	2	2	2	4	2	2	2	2	2	2
Mvmt Flow	20	1248	3	1	1631	60	6	0	2	16	0	29

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1691	0	0	1251	0	0	2108	2983	626	2327	2954	846
Stage 1	-	-	-	-	-	-	1290	1290	-	1663	1663	-
Stage 2	-	-	-	-	-	-	818	1693	-	664	1291	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	374	-	-	552	-	-	29	14	427	20	14	306
Stage 1	-	-	-	-	-	-	173	232	-	101	152	-
Stage 2	-	-	-	-	-	-	336	147	-	416	232	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	374	-	-	552	-	-	25	13	427	19	13	306
Mov Cap-2 Maneuver	-	-	-	-	-	-	25	13	-	19	13	-
Stage 1	-	-	-	-	-	-	164	220	-	96	152	-
Stage 2	-	-	-	-	-	-	303	147	-	392	220	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.2	0	148	236.9
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	33	374	-	-	552	-	-	49
HCM Lane V/C Ratio	0.255	0.053	-	-	0.002	-	-	0.924
HCM Control Delay (s)	148	15.2	-	-	11.5	-	-	236.9
HCM Lane LOS	F	C	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.8	0.2	-	-	0	-	-	3.9

Intersection	
Int Delay, s/veh	13.2

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	46	1441	5	1	1088	56	2	0	0	35	0	41
Future Vol, veh/h	46	1441	5	1	1088	56	2	0	0	35	0	41
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	-	100	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	91	91	91	91	91	91	91	91	91	91	91	91
Heavy Vehicles, %	8	3	2	2	3	4	2	2	2	2	2	8
Mvmt Flow	51	1584	5	1	1196	62	2	0	0	38	0	45

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1258	0	0	1589	0	0	2289	2949	795	2123	2920	629
Stage 1	-	-	-	-	-	-	1689	1689	-	1229	1229	-
Stage 2	-	-	-	-	-	-	600	1260	-	894	1691	-
Critical Hdwy	4.26	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	7.06
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.28	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.38
Pot Cap-1 Maneuver	517	-	-	409	-	-	21	14	330	~ 28	15	411
Stage 1	-	-	-	-	-	-	97	148	-	188	248	-
Stage 2	-	-	-	-	-	-	455	240	-	302	148	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	517	-	-	409	-	-	17	13	330	~ 26	13	411
Mov Cap-2 Maneuver	-	-	-	-	-	-	17	13	-	~ 26	13	-
Stage 1	-	-	-	-	-	-	87	133	-	169	248	-
Stage 2	-	-	-	-	-	-	404	240	-	272	133	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.4	0	246	$ 457.3
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	17	517	-	-	409	-	-	53
HCM Lane V/C Ratio	0.129	0.098	-	-	0.003	-	-	1.576
HCM Control Delay (s)	246	12.7	-	-	13.8	-	-	$ 457.3
HCM Lane LOS	F	B	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.4	0.3	-	-	0	-	-	7.8

Notes			
~: Volume exceeds capacity	$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

Intersection	
Int Delay, s/veh	4.8

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	19	1210	3	1	1580	58	6	0	2	15	0	29
Future Vol, veh/h	19	1210	3	1	1580	58	6	0	2	15	0	29
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	-	100	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	2	2	2	2	2	4	2	2	2	2	2	2
Mvmt Flow	20	1274	3	1	1663	61	6	0	2	16	0	31

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1724	0	0	1277	0	0	2150	3042	639	2373	3013	862
Stage 1	-	-	-	-	-	-	1316	1316	-	1696	1696	-
Stage 2	-	-	-	-	-	-	834	1726	-	677	1317	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	363	-	-	540	-	-	27	13	419	18	13	298
Stage 1	-	-	-	-	-	-	166	226	-	96	147	-
Stage 2	-	-	-	-	-	-	329	142	-	409	225	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	363	-	-	540	-	-	23	12	419	17	12	298
Mov Cap-2 Maneuver	-	-	-	-	-	-	23	12	-	17	12	-
Stage 1	-	-	-	-	-	-	157	214	-	91	147	-
Stage 2	-	-	-	-	-	-	295	142	-	385	213	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.2	0	166.5	284.2
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	30	363	-	-	540	-	-	45
HCM Lane V/C Ratio	0.281	0.055	-	-	0.002	-	-	1.029
HCM Control Delay (s)	166.5	15.5	-	-	11.7	-	-	284.2
HCM Lane LOS	F	C	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.9	0.2	-	-	0	-	-	4.3

Intersection	
Int Delay, s/veh	22.8

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	85	1439	5	1	1085	78	2	0	0	38	0	50
Future Vol, veh/h	85	1439	5	1	1085	78	2	0	0	38	0	50
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	-	100	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	91	91	91	91	91	91	91	91	91	91	91	91
Heavy Vehicles, %	8	3	2	2	3	4	2	2	2	2	2	8
Mvmt Flow	93	1581	5	1	1192	86	2	0	0	42	0	55

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1278	0	0	1586	0	0	2368	3050	793	2214	3009	639
Stage 1	-	-	-	-	-	-	1770	1770	-	1237	1237	-
Stage 2	-	-	-	-	-	-	598	1280	-	977	1772	-
Critical Hdwy	4.26	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	7.06
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.28	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.38
Pot Cap-1 Maneuver	508	-	-	410	-	-	18	12	331	~ 24	13	405
Stage 1	-	-	-	-	-	-	86	135	-	186	246	-
Stage 2	-	-	-	-	-	-	456	235	-	269	135	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	508	-	-	410	-	-	13	10	331	~ 21	11	405
Mov Cap-2 Maneuver	-	-	-	-	-	-	13	10	-	~ 21	11	-
Stage 1	-	-	-	-	-	-	70	110	-	152	246	-
Stage 2	-	-	-	-	-	-	393	235	-	220	110	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.8	0	$ 331.7	$ 699.4
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	13	508	-	-	410	-	-	46
HCM Lane V/C Ratio	0.169	0.184	-	-	0.003	-	-	2.102
HCM Control Delay (s)	$ 331.7	13.7	-	-	13.8	-	-	$ 699.4
HCM Lane LOS	F	B	-	-	B	-	-	F
HCM 95th %tile Q(veh)	0.5	0.7	-	-	0	-	-	10

Notes			
~: Volume exceeds capacity	$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

Intersection	
Int Delay, s/veh	1.1

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	0	0	4	14	0	0	13	102	50	0	69	0
Future Vol, veh/h	0	0	4	14	0	0	13	102	50	0	69	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	12	2	2	8	2
Mvmt Flow	0	0	4	15	0	0	14	111	54	0	75	0

Major/Minor	Minor2			Minor1			Major1			Major2		
Conflicting Flow All	241	268	75	243	241	138	75	0	0	165	0	0
Stage 1	75	75	-	166	166	-	-	-	-	-	-	-
Stage 2	166	193	-	77	75	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	713	638	986	711	660	910	1524	-	-	1413	-	0
Stage 1	934	833	-	836	761	-	-	-	-	-	-	0
Stage 2	836	741	-	932	833	-	-	-	-	-	-	0
Platoon blocked, %								-	-		-	
Mov Cap-1 Maneuver	707	632	986	702	653	910	1524	-	-	1413	-	-
Mov Cap-2 Maneuver	707	632	-	702	653	-	-	-	-	-	-	-
Stage 1	925	833	-	828	753	-	-	-	-	-	-	-
Stage 2	828	734	-	928	833	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	8.7	10.2	0.6	0
HCM LOS	A	B		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT
Capacity (veh/h)	1524	-	-	986	702	1413	-
HCM Lane V/C Ratio	0.009	-	-	0.004	0.022	-	-
HCM Control Delay (s)	7.4	-	-	8.7	10.2	0	-
HCM Lane LOS	A	-	-	A	B	A	-
HCM 95th %tile Q(veh)	0	-	-	0	0.1	0	-

Intersection	
Int Delay, s/veh	21.7

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	25	1198	3	1	1568	69	6	0	2	29	0	67
Future Vol, veh/h	25	1198	3	1	1568	69	6	0	2	29	0	67
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	100	-	-	100	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	95	95	95	95	95	95	95	95	95	95	95	95
Heavy Vehicles, %	2	2	2	2	2	4	2	2	2	2	2	2
Mvmt Flow	26	1261	3	1	1651	73	6	0	2	31	0	71

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1724	0	0	1264	0	0	2143	3041	632	2373	3006	862
Stage 1	-	-	-	-	-	-	1315	1315	-	1690	1690	-
Stage 2	-	-	-	-	-	-	828	1726	-	683	1316	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.54	6.54	6.94	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.54	5.54	-	6.54	5.54	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.52	4.02	3.32	3.52	4.02	3.32
Pot Cap-1 Maneuver	363	-	-	546	-	-	27	13	423	~ 18	13	298
Stage 1	-	-	-	-	-	-	167	226	-	97	148	-
Stage 2	-	-	-	-	-	-	332	142	-	405	226	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	363	-	-	546	-	-	19	12	423	~ 17	12	298
Mov Cap-2 Maneuver	-	-	-	-	-	-	19	12	-	~ 17	12	-
Stage 1	-	-	-	-	-	-	155	210	-	90	148	-
Stage 2	-	-	-	-	-	-	253	142	-	374	210	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.3	0	209.8	$ 650.6
HCM LOS			F	F

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	25	363	-	-	546	-	-	50
HCM Lane V/C Ratio	0.337	0.072	-	-	0.002	-	-	2.021
HCM Control Delay (s)	209.8	15.7	-	-	11.6	-	-	$ 650.6
HCM Lane LOS	F	C	-	-	B	-	-	F
HCM 95th %tile Q(veh)	1	0.2	-	-	0	-	-	10.1

Notes			
~: Volume exceeds capacity	$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

Intersection	
Int Delay, s/veh	3.9

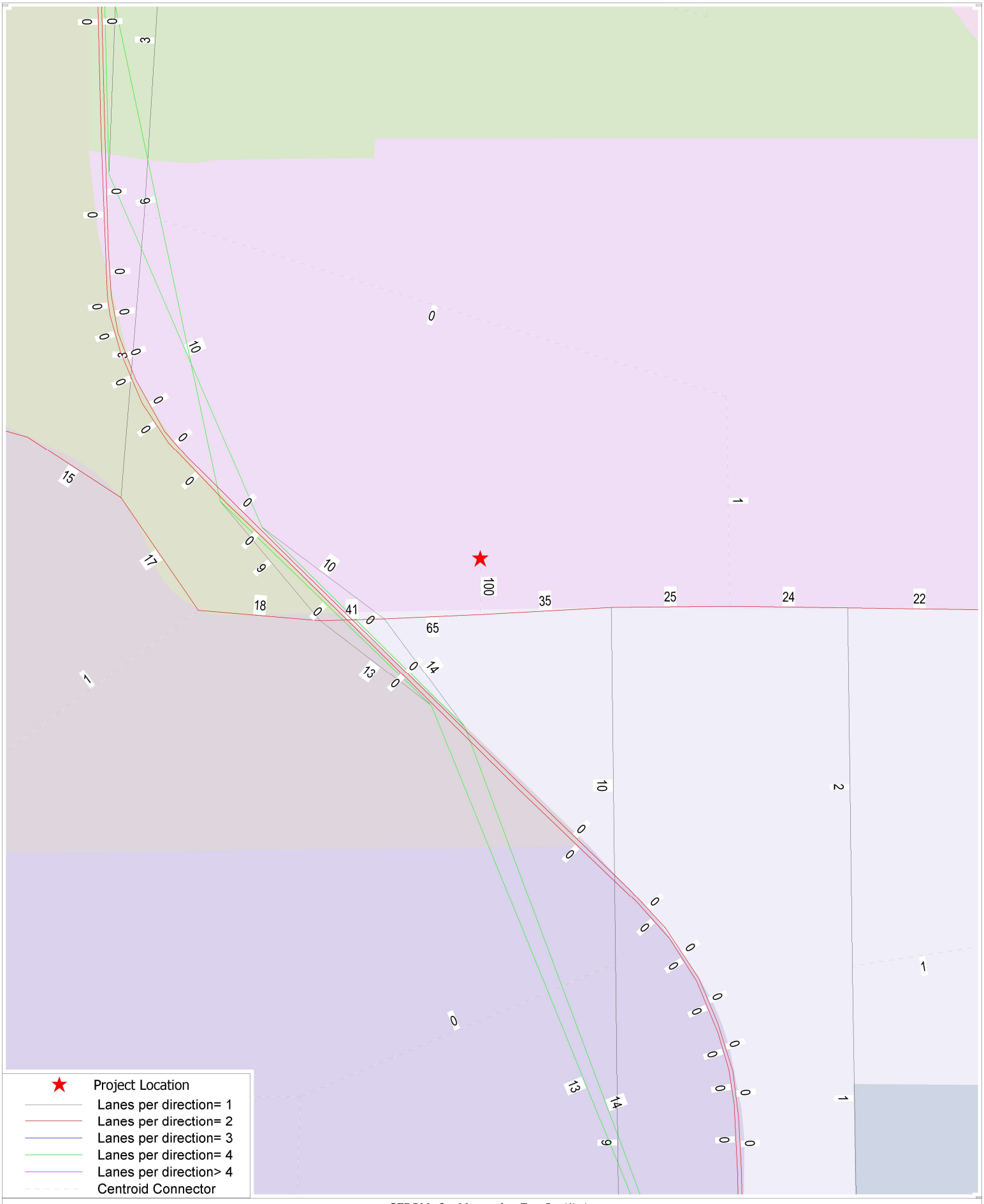
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↰	
Traffic Vol, veh/h	0	0	16	64	0	0	6	78	25	0	15	0
Future Vol, veh/h	0	0	16	64	0	0	6	78	25	0	15	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	4	2	2	2	2
Mvmt Flow	0	0	17	70	0	0	7	85	27	0	16	0

Major/Minor	Minor2			Minor1			Major1			Major2		
Conflicting Flow All	129	142	16	138	129	99	16	0	0	112	0	0
Stage 1	16	16	-	113	113	-	-	-	-	-	-	-
Stage 2	113	126	-	25	16	-	-	-	-	-	-	-
Critical Hdwy	7.12	6.52	6.22	7.12	6.52	6.22	4.12	-	-	4.12	-	-
Critical Hdwy Stg 1	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.12	5.52	-	6.12	5.52	-	-	-	-	-	-	-
Follow-up Hdwy	3.518	4.018	3.318	3.518	4.018	3.318	2.218	-	-	2.218	-	-
Pot Cap-1 Maneuver	844	749	1063	833	762	957	1602	-	-	1478	-	0
Stage 1	1004	882	-	892	802	-	-	-	-	-	-	0
Stage 2	892	792	-	993	882	-	-	-	-	-	-	0
Platoon blocked, %								-	-		-	
Mov Cap-1 Maneuver	841	745	1063	816	758	957	1602	-	-	1478	-	-
Mov Cap-2 Maneuver	841	745	-	816	758	-	-	-	-	-	-	-
Stage 1	999	882	-	888	798	-	-	-	-	-	-	-
Stage 2	888	788	-	977	882	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	8.4	9.8	0.4	0
HCM LOS	A	A		

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT
Capacity (veh/h)	1602	-	-	1063	816	1478	-
HCM Lane V/C Ratio	0.004	-	-	0.016	0.085	-	-
HCM Control Delay (s)	7.3	0	-	8.4	9.8	0	-
HCM Lane LOS	A	A	-	A	A	A	-
HCM 95th %tile Q(veh)	0	-	-	0.1	0.3	0	-

APPENDIX F
CFRPM Model Plot



CFRPMv6 - Magruder Eye Institute
2020 Base Run
C:\FSUTMS\D5\CFRPMV61_Daily\Base\CF_2020\MagruderEye_20\Output\HWYLOAD_C20.NET 10/29/2020

cube

(Licensed to Kimley Horn and Associates Inc)

Planning & TRANSPORTATION

401 South Park Avenue • Winter Park, Florida 32789

407-599-3324 • planning@cityofwinterpark.org
cityofwinterpark.org

Planning & Zoning Board
Staff Report for January 5, 2021 Meeting

ZTA# 20-08 Request of the City of Winter Park for: An Ordinance amending Article III, Zoning of Chapter 58 of the Land Development Code providing regulations electric vehicle charging stations in multi-family and non-residential parking lots and parking garages.

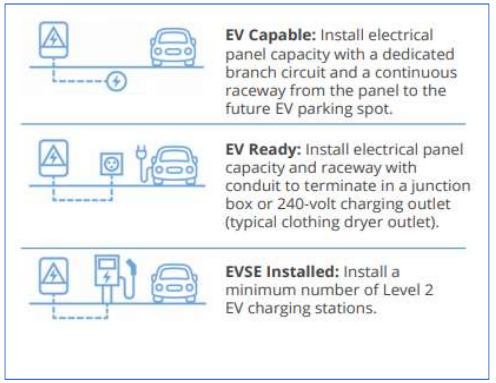
Background:

This public hearing is to consider an Ordinance to amend the City's parking regulations in order to require electric vehicle charging stations and readiness in parking lots and parking garages.

1. The Ordinance requires that for all multi-family residential projects with individual unit garages or an underground parking garage, electric vehicle readiness and parking space requirements be determined as part of the Conditional Use approval process.
2. The Ordinance requires that all non-residential projects install one electric car charging station per every 20 required off-street parking spaces.
3. The Ordinance requires that for future EV charging readiness, all non-residential properties projects with surface parking or parking structures, must provide the electrical capacity and buried conduit/raceway so that in the future, 10% of the spaces in the parking lot or parking garage can be converted to EV charging stations.
4. In implementing an electric charging station, the location of at least one of the EV charging stations is required to be adjacent to the handicapped parking space(s) so that a handicapped individual can have access to the charging station from within the handicapped parking space.

Staff recommendation is for approval.

	Single Family, Duplex			Multifamily			Commercial		
	EV Capable	EV Ready	EVSE Installed	EV Capable	EV Ready	EVSE Installed	EV Capable	EV Ready	EVSE Installed
Surfside, FL					20+ units % not specified			20+ units % not specified	
Jupiter, FL									1 rapid charger at any new fueling station
Miami Beach, FL					20+ units 2% of req. spaces	20+ units 2% of req. spaces			
Hollywood, FL	1 space			min. 1 space			min. 1 space		
Atlanta, GA	1 space			20% of req. spaces			20% or req. spaces		
Boca Raton, FL				50+ units min. 1 space		50+ parking spaces req.	50+ units min. 1 space		50+ parking spaces 2% of req. spaces
Miami Dade County, FL					10+ spaces 10% of req. spaces			10+ spaces 10% of req spaces	
Coral Gables, FL				20+spaces 15% or req. spaces	20+ spaces 3% of req. spaces	20+ parking spaces 2% of req. spaces	20+ spaces 15% of req. spaces	20+ spaces 3% of req. spaces	20+ parking spaces 2% of req. spaces
Boynton Beach, FL						2/50 units			2/50,000 sq. ft
Winter Park, FL *proposed*				conditional use		conditional use	10%		1/20 req. spaces
Orlando, FL *proposed*				20% of req. spaces		250+ spaces, 2% of req. spaces	20% of req. spaces		250+ spaces 2% of req. spaces
Orange County, FL *proposed*				20% of req. spaces		250+ spaces, 2% of req. spaces	20% of req. spaces		250+ spaces 2% of req. spaces



Date Passed	Jurisdiction	Ordinance	Code Section(s)	Definitions	Exemptions	EV Charging Station Infrastructure Requirement (does not require charging station be installed)			EV Charging Station Installation Requirement	
						Single Family Residential	Multi-family Residential	Commercial	Multi-family Residential	Commercial
03/11/14	Surfside, FL	14-1617	Zoning, Definitions, Regulated Uses, Off-street Parking Requirements	Electric Vehicle Charging Level; Electric Vehicle Charging Station			All new multifamily or hotel development with **20 or more units** shall provide **access** to **220 volt** capability **throughout** the garage to offer charging opportunities to residents and guests **as needed**.	All new multifamily or hotel development with 20 or more units shall provide **access** to 220 volt capability **throughout** the garage to offer charging opportunities to residents and guests **as needed**.		
02/17/15	Jupiter, FL	44-14	Zoning, Supplementary District Regulations, Regulations for Automotive Fueling Stations	Electric vehicle charging station						At least **one electric vehicle rapid charging station** containing a CHAdeMO and Combined Charging Standard (CCS) handle (minimum of 50 kw) shall be **installed and maintained** at any automotive fueling station with **a convenience store or other commercial use.**
1/13/16	Miami Beach, FL	2016-3988	Land Development Regulations, Off-street Parking	Electric Vehicle Charging Level, Electric Vehicle Charging Station, Electric Vehicle Parking Space, Electric Vehicle	Single-family		Any residential multifamily or hotel development with **20 or more units** shall install and **provide access** to electrical power supply rated at **240 volts** or greater for **two (2) percent** of the required off-street parking spaces. Fee in Lieu Option: $8,000 per EV space not provided (in addition to $40,000 per parking space fee in lieu of providing parking for new construction).	Any residential multifamily or hotel development with **20 or more units** shall install and **provide access** to electrical power supply rated at **240 volts** or greater for **two (2) percent** of the required off-street parking spaces. Fee in Lieu Option: $8,000 per EV space not provided (in addition to $40,000 per parking space fee in lieu of providing parking for new construction).		
01/20/17	Hollywood, FL	O-2016-02	Land Usage, Buildings, Mandatory Green Building Practices			New commercial or residential structure to construct the **infrastructure necessary** for future installation of **an (at least 1) electric vehicle-charging station** within be included in such project. **Minimally**, the following shall be installed: an **empty three-quarter-inch raceway from the branch circuit panel board to a location** in the garage or a designated parking area, with a **two-gang junction box with a blank plate**; **or a fully functional electric vehicle-charging station**.	New commercial or residential structure to construct the **infrastructure necessary** for future installation of **an (at least 1) electric vehicle-charging station** within be included in such project. **Minimally**, the following shall be installed: an **empty three-quarter-inch raceway from the branch circuit panel board to a locatio**n in the garage or a designated parking area, with a **two-gang junction box with a blank plate**; **or a fully functional electric vehicle-charging station**.	New commercial or residential structure to construct the **infrastructure necessary** for future installation of **an (at least 1) electric vehicle-charging station** within be included in such project. **Minimally**, the following shall be installed: an **empty three-quarter-inch raceway from the branch circuit panel board to a locatio**n in the garage or a designated parking area, with a **two-gang junction box with a blank plate**; **or a fully functional electric vehicle-charging station**.		
11/20/17	Atlanta, GA	17-0-1654	Electrical Code Amendments	Electric Vehicle, Electric Vehicle Supply Equipment (EVSE), EVSE Infrastructure, PHEV	Single-family units without designated parking space on premise	All new single-family dwellings, two-family dwellings and townhomes required to provide **electrical capacity for 40-ampere 240-volt branch circuit**. Absent an attached or detached garage, provide an underground electrical conduit between the dwelling and designated parking space, which is continuous from the branch circuit / feeder panel, raceway shall not be less than 1" (one inch) in size and must include a pull rope or line, with the raceway sealed and	New Multifamily and Commercial, ratio of EV parking Spaces to Non-Electrical parking spaces shall be **1:5 (20%)**. Must include **raceway** (>1" in size) with pull rope/line, sealed and labeled for future use. **Electrical equipment room must have a dedicated space** for the future installation of EV Supply Equipment and be labelled "Future EV Charging Equipment & Panels"	New Multifamily and Commercial, ratio of EV parking Spaces to Non-Electrical parking spaces shall be **1:5 (20%)**. Must include **raceway** (>1" in size) with pull rope/line, sealed and labeled for future use. **Electrical equipment room must have a dedicated space** for the future installation of EV Supply Equipment and be labelled "Future EV Charging Equipment & Panels"		
12/17/17	Boca Raton, FL	5420	Zoning, Off-street Parking	Electric vehicle, Electric vehicle charging level, Electric vehicle parking space, Electric vehicle charging station	Single-family		Multifamily residential developments with ≥ 50 units must install & **provide access (at least 1)** to electrical power supply rated at ≥ 240 volts for all off-street parking facilities to allow the installation of additional EV parking spaces in the future.	Hotels, apartment hotels and motels with ≥ 50 rooms must install & **provide access (at least 1)** to electrical power supply rated at ≥ 240 volts in all off-street parking facilities to allow the installation of additional EV parking spaces in the future.	When there is an expansion requiring more than 50 parking spaces, changes of use, or new multifamily or commercial where **50 or more parking spaces required**, a **minimum of two (2) percent** of the required off-street parking spaces shall have **Level-2** charging station.	When there is an expansion requiring more than 50 parking spaces, changes of use, or new multifamily or commercial where 50 or more parking spaces required, a **minimum of two (2%)** of the required off-street parking spaces shall have **Level-2** charging station.
03/05/19	Miami Dade County, FL	19-17	Zoning, Off-street Parking	Electric Vehicle, Electric Vehicle Supply Equipment, EVSE Space, EVSE-Ready Space	Single-family, duplex, townhouses, church or religious use		If **≥ 10 parking spaces req**uired, **min. of 10%** of parking spaces must have **full circuitry installed** in accordance with the FBC and ready for the charger to be connected; increases to **20% in Jan. 2022**	If **≥ 10 parking spaces req**uired, **min. of 10%** of parking spaces must have **full circuitry installed** in accordance with the FBC and ready for the charger to be connected; increases to **20% in Jan. 2022**		
03/26/19	Coral Gables, FL	2019-19	Zoning, Development Standards	Electric Vehicle Supply Equipment (EVSE) infrastructure, EV-Ready, EV Capable	Single-family, duplexes, and townhouses		If **≥ 20 parking spaces required**, min. of **3% of spaces** must be EV-Ready by including **40-Amps on an independent 240-volt AC circuit for every EV spac**e; AND min. of **15% of spaces** must be EV-Capable by having **raceway and electric capacity** sized to accommodate 60A or 40A breakers for each parking space. If calculation results in fractional space, rounded	If **≥ 20 parking spaces required**, min. of **3% of spaces** must be EV-Ready by including **40-Amps on an independent 240-volt AC circuit for every EV spac**e; AND min. of **15% of spaces** must be EV-Capable by having **raceway and electric capacity** sized to accommodate 60A or 40A breakers for each parking space. If calculation results in fractional space, rounded up	When **20 or more** parking spaces required, min. of **two (2) percent** of spaces (min. 1 space) must provide a **Level-2** EV charging. If calculation results in fractional space, rounded up to next whole number.	When **20 or more** parking spaces required, min. of **two (2) percent** of spaces (min. 1 space) must provide a **Level-2** EV charging. If calculation results in fractional space, rounded up to next whole number.

09/05/19	Boynton Beach, FL	19-027	Land Development Regulations, Site Development Standards, Sustainable Development Standards		Single-family, duplexes				Provide **Level 2** charging station capable of **servicing two (2) parking spaces** for **every fifty (50) dwelling units**	**Provide Level 2** charging station capable of **servicing two (2) parking spaces** per **every fifty thousand (50,000) square feet** of non-residential development.
	Winter Park, FL		Buildings, Amendments to the Florida Building Code; Zoning, Off-street Parking	Charging, Electric Vehicle, Electric vehicle charging level, Electric vehicle charging station infrastructure, electric vehicle parking space			Readiness requirements for new multi-family residential projects **that require a Conditional Use**, shall be determined as part of the **Conditional Use approval process**.	Provide the **electrical capacity** and **buried raceway** necessary to accommodate the future hardwire installation, at the minimum, a **Level-2** vehicle charging station, for a minimum ratio of **10%** of the total required parking spaces	Electrical vehicle parking requirements for new multi-family residential projects **that require a Conditional Use**, shall be determined as part of the **Conditional Use approval process**.	Provide **one** (1) electric vehicle parking space equipped with an **Level-2** electrical vehicle charging station **per every 20** required off-street parking spaces

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY OF WINTER PARK, FLORIDA AMENDING CHAPTER 58 "LAND DEVELOPMENT CODE", ARTICLE III, "ZONING REGULATIONS" SUBSECTION 58-86 "OFF-STREET PARKING AND LOADING REGULATIONS" SO AS TO ADD REGULATIONS FOR ELECTRIC VEHICLE CHARGING INFRASTRUCTURE IN MULTI-FAMILY AND NON-RESIDENTIAL PARKING LOTS AND PARKING GARAGES, PROVIDING FOR VESTING, CONFLICTS, CODIFICATION, SEVERABILITY, AND AN EFFECTIVE DATE.

WHEREAS, the City of Winter Park's Off-Street Parking and Loading Regulations portion of the Land Development Code does not currently contain provisions for off-street parking facility requirements for electric vehicles;

WHEREAS, according to the U.S. Department of Energy, the benefits of electric vehicles include improved air quality, reduction of carbon emissions, quieter and more livable streets, and decreased dependency on fossil fuels;

WHEREAS, the City of Winter Park is located within an urban area which is subject to ever increasing levels of carbon emissions generated by fossil fuel powered engines which over time will result in mandatory vehicle emissions testing and potential loss of Federal transportation dollars due to inability to meet healthy air quality levels (See attached document entitled "Orange County FACT SHEET, Transportation-Critical Air Quality Considerations, November, 2019). The City of Winter Park houses within its boundaries one of only two air quality monitoring stations within Orange County that capture samples for measurement needed for local compliance with Federal regulations.

WHEREAS, a significant number of industry stakeholders are urging electric utilities to support the buildout of electric vehicle infrastructure to aid the development of the electric vehicle usage which in turn will decarbonize the transportation sector, promote energy independence, and increase electric retail sales resulting in a net benefit to all stakeholders;

WHEREAS, Florida ranks within the top five states nationally for sales of electric vehicles;

WHEREAS, the City should continue its support of plug-in electric vehicles and its efforts in constructing electric vehicle and plug-in hybrid electric vehicle charging infrastructure as this further supports the City's Sustainability Action Plan;

WHEREAS, the proposed amendment is consistent with the City's Vision of Winter Park as the city of arts and culture, cherishing its traditional scale and charm while building a healthy and sustainable future for all generations;

WHEREAS, the Planning and Zoning Board, after notice and public hearing, has considered the proposed amendments to the Off-Street Parking and Loading Regulations portion of the Land Development Code, more specifically described herein, and submitted its recommendation to the City Commission;

WHEREAS, the City Commission, after notice and public hearing, has considered the proposed amendments to the Off-Street Parking and Loading Regulations portion of the Land Development Code, the recommendations of the Planning and Zoning Board and all public comments;

WHEREAS, the proposed amendment to the Off-Street Parking and Loading Regulations portion of the Land Development Code is consistent with the City of Winter Park Comprehensive Plan;

WHEREAS, a companion technical amendment ordinance is proposed in Chapter 22, Buildings and Building Regulation, Section 22-28, Sub-section 2703 covering National Electrical Code criteria for installation along with Chapter 553 Statutory justification;

WHEREAS, the portions of Chapter 58, Land Development Code, Article III, Zoning Regulations that are to be amended and modified as described in each section and amended to read as shown herein.

NOW THEREFORE, BE IT ENACTED BY THE CITY COMMISSION OF THE CITY OF WINTER PARK:

SECTION 1. That Chapter 58 "Land Development Code", Article III "Zoning" of the Code of Ordinances is hereby amended and modified within Section 58-86 "Off-street Parking and Loading Regulations", adding subsection (5) "Electric Vehicle Charging Station Infrastructure and Electric Vehicle Parking Space Requirements" in the "Zoning" Article of the Land Development Code to read as follows:

Sec. 58-86. Off-Street Parking and Loading Regulations.

(5) *Electric Vehicle Charging Station Infrastructure and Electric Vehicle Parking Space Requirements.*

a) *Intent and purpose.* The intent of this section is to facilitate and encourage the use of electric vehicles and to expedite the establishment of a convenient, cost-effective electric vehicle infrastructure that will also accommodate future technology advancements.

b) *Definitions.* For the purposes of this section, the following definitions shall apply:

1. *Charging* means that an electric vehicle is parked at an electric vehicle charging station and is connected to the battery charging station equipment and is actively charging at some point during the charging session.

2. *Electric vehicle* means any motor vehicle registered to operate on public roadways that operates either partially or exclusively on electric energy. Electric vehicles (EVs) include: (a) Battery-powered electric vehicles; (b) Plug-in hybrid electric

vehicles; (c) electric motorcycles; and (d) Fuel cell vehicles.

3. *Electric vehicle charging level* means the standardized indicators of electrical force, or voltage, amps and kilowatts by which an electric vehicle's batteries are recharged. EV recharging equipment is commonly known as Electric Vehicle Service Equipment (EVSE) and can output either Alternating Current (AC) or Direct Current (DC). EVSE are technically not chargers, they are power supply units, the charger is onboard the vehicle. The onboard charger helps manage the charging session and converts an AC input to DC to charge the vehicle's batteries; if a DC input is supplied the charger passes the power directly to the batteries. The terms Level1 (L1), Level2 (L2), and Level3 (L3) are the most common charging levels; L3 is also referred to DCFC or DC Fast Charging, and include the following specifications:
    a) Level-1 is considered slow charging. Voltage including the range from 0 through 120.
    b) Level-2 is considered medium charging. Voltage is greater than 120, up to 240.
    c) Level-3 is considered fast or rapid charging, is also referred to DCFC or DC Fast Charging. Voltage is greater than 240.

4. *Electric vehicle charging station* means battery charging station equipment that has as its primary purpose the transfer of electric energy (by conductive or inductive means) to a battery or other energy storage device in an electric vehicle.

5. *Electric vehicle charging station infrastructure* means conduit/wiring, structures, machinery, and equipment necessary and integral to support an electric vehicle, including battery charging stations and rapid charging stations.

6. *Electric vehicle parking space* means any off-street parking space that is equipped with an electric vehicle charging station that is exclusively for use by electric vehicles.

c) *Readiness requirements for multi-family residential and non-residential properties.* In order to proactively plan for and accommodate the anticipated future growth in market demand for electric vehicles, all new development shall provide electric vehicle charging station infrastructure per this section. The infrastructure shall be installed per Technical Amendments to the Florida Building Code found in Chapter 22, Section 2703 of the City of Winter Park Code of Ordinances..

    1. Readiness requirements for new multi-family residential projects that require a Conditional Use, shall be determined as part of the Conditional Use approval process.

    2. Non-residential properties (such as commercial, office, institutional or industrial uses, or any combination thereof) with surface parking or parking structures, shall provide the electrical capacity and buried raceway necessary to accommodate the future hardwire installation, at the minimum, a Level-2 vehicle charging station, for a minimum ratio of 10% of the total required parking spaces. Electric vehicle

parking spaces provided in excess of the required electric vehicle parking spaces shall be counted toward meeting the readiness requirement. Required electric vehicle parking spaces shall not count toward meeting the readiness requirement

d) *Electric vehicle parking space requirement for multi-family residential and non-residential properties.* Electrical vehicle parking requirements for new multi-family residential projects that require a Conditional Use, shall be determined as part of the Conditional Use approval process. All non-residential properties (such as commercial, office, institutional or industrial uses, or any combination thereof) are required to provide one electric vehicle parking space equipped with an electrical vehicle charging station per every 20 required off-street parking spaces (such spaces shall be counted toward meeting the overall parking requirement and any EV parking spaces in excess of the required electric vehicle parking spaces shall count toward meeting the readiness requirement; required electric vehicle parking spaces shall not count toward meeting the readiness requirement) in accordance with the following standards:

1. Minimum standards. Electric vehicle parking spaces shall, at a minimum, be equipped with an electric vehicle charging station rated at electric vehicle charging Level 2.

2. Fees. Nothing herein shall prohibit the charging of a fee for the use of an electric vehicle charging station by a resident, guest, invitee or employee.

3. ADA Accessible Spaces. A minimum of one (1) electric vehicle parking space must be located adjacent to a required accessible parking space such that the electric vehicle charging station can be shared between an accessible parking space and electric vehicle parking space. A minimum five (5) feet wide accessway must be provided by the accessible electric vehicle parking space if the accessway is not already provided as part of the planned accessible parking space.

4. Lighting. Site lighting shall be provided where an electric vehicle charging station is installed.

5. Equipment Standards and Protection. Battery charging station outlets and connector devices shall be no less than 36 inches and no higher than 48 inches from the surface where mounted. Equipment mounted on pedestals, lighting posts, bollards, or other devices shall be designed and located as to not impede pedestrian travel or create trip hazards on sidewalks. Adequate battery charging station protection, such as concrete-filled steel bollards, shall be used. Curbing may be used in lieu of bollards, if the battery charging station is setback a minimum of 24 inches from the face of the curb.

6. Signage. (1) Information shall be posted identifying voltage and amperage levels and any time of use, fees, or safety information related to the electric vehicle charging station. (2) Each electric vehicle charging station space shall be posted with signage indicating the space is only for electric vehicle charging purposes. For

purposes of this subsection, "charging" means that an electric vehicle is parked at an electric vehicle charging station and is connected to the battery charging station equipment and is actively charging at some point during the charging session. (3) Restrictions shall be included on the signage, if removal provisions are to be enforced by the property owner pursuant to state statutes.

7. Maintenance. Electric vehicle charging stations shall be maintained in good condition in all respects, including the functioning of the equipment, by the property owner. Removal of any required EV charging stations is prohibited except for repair or replacement of equipment. A phone number or other contact information shall be provided on the equipment for reporting when the equipment is not functioning or other problems are encountered.

e) *Exception*. Where the installation of one or more electric vehicle parking spaces, and/or the installation of electric vehicle charging station infrastructure to allow for the future installation of electric vehicle charging stations, are required by this article, an exception may be granted by the Building Official through the site plan approval process only where it is demonstrated that the extension of the electrical power supply to the effected location is physically impractical. Financial impracticality is not a valid criterion for granting such an exception.

SECTION 2. VESTING. In order to not adversely affect development projects that may be in process and for which expenditures have been made in reliance upon the existing code provisions, the City will allow such development or building permit applications to be subject to the parking code existing prior to the adoption of this Ordinance, provided such development projects or permits have been submitted prior to the effective date of this Ordinance.

SECTION 3. SEVERABILITY. If any Section or portion of a Section of this Ordinance proves to be invalid, unlawful, or unconstitutional, it shall not be held to invalidate or impair the validity, force, or effect of the remainder of this Ordinance.

SECTION 4. CODIFICATION. It is the intention of the City Commission of the City of Winter Park, Florida, and it is hereby ordained that the provisions of this Ordinance shall become and be made a part of the Code of Ordinance of the City of Winter Park, Florida, except for Sections 6-10;

SECTION 5. CONFLICTS. All Ordinances or parts of Ordinances in conflict with any of the provisions of this Ordinance are hereby repealed.

SECTION 6. EFFECTIVE DATE. This Ordinance shall become effective immediately upon its passage and adoption.

ADOPTED at a regular meeting of the City Commission of the City of Winter Park, Florida, held in City Hall, Winter Park, on this _____ day of _____, 2020.

Steve Leary, Mayor

ATTEST:

_____

City Clerk